

Historic Effects Report Honolulu High-Capacity Transit Corridor Project

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Prepared for:
City and County of Honolulu

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HISTORIC EFFECTS REPORT

1.0 Summary

The City and County of Honolulu Department of Transportation Services, in coordination with the U.S. Department of Transportation Federal Transit Administration, is evaluating the potential impacts of proposed high-capacity transit service on O‘ahu (the Honolulu High-Capacity Transit Corridor Project). The proposed transit service would consist of a primarily grade-separated, fixed guideway rail transit system. The 30-mile system was originally proposed to extend between the City of Kapolei and the University of Hawai‘i at Mānoa (UH Mānoa) with a connection to Waikīkī. However, existing funding is inadequate to construct the full 30-mile system; therefore, a fundable 20-mile section of the Project between the East Kapolei area and Ala Moana Center in Honolulu is currently being studied and proposed for initial construction. This 20-mile section, known as “the Project,” was the focus of the technical environmental studies summarized in the Draft Environmental Impact Statement. Future planned extensions would serve West Kapolei, Waikīkī, and UH Mānoa.

The purpose of this report is to document compliance with Section 106 of the National Historic Preservation Act of 1966 (as amended) which requires any Federal agency having direct or indirect jurisdiction over a Federal or federally assisted undertaking to consider the effect of that undertaking on any district, site, building, structure, or object that is included in or eligible for inclusion in the National Register of Historic Places (NRHP).

This document follows regulations and guidelines set forth in the Code of Federal Regulations (CFR) in “Section 106 Regulations, Protection of Historic Properties” (36 CFR 800) for assessing effects the proposed undertaking may have on NRHP-listed and NRHP-eligible properties identified within the Project’s Area of Potential Effect (APE). Preliminary effects assessments made in the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008) and Draft Environmental Impact Statement are superseded by the findings in this report.

In this report, potential project effects to the 81 identified NRHP-listed and eligible properties within the Project’s APE are evaluated using the criteria of adverse effect outlined in 36 CFR 800.5. These criteria establish that an adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the NRHP in a manner that would diminish the integrity of the property’s location, design, setting, materials, workmanship, feeling, and association.

Of the 81 identified NRHP-listed and eligible historic properties to which the criteria of adverse effect were applied, 22 properties were determined to be adversely affected by implementation of the Project. Of the remaining 59 historic properties, implementation of the Project would have no adverse effect on 51 properties and no effect on 8 properties.

2.0 Project Description

The Project will include the construction and operation of a primarily grade-separated fixed guideway transit system between East Kapolei and Ala Moana Center (Airport Alternative as shown in Figures 1, 2, 3, and 4). All parts of the system will either be elevated in existing transportation right-of-way or in exclusive right-of-way or easement. Steel-wheel-on-steel-rail transit technology has been selected through a comparative process based on the ability of various transit technologies to cost-effectively meet project requirements.

The Project will begin by following North-South Road and other future roadways to Farrington Highway. Proposed station locations and other project features in this area are shown in Figure 1. The guideway will follow Farrington Highway Koko Head on an elevated structure and continue along Kamehameha Highway to the vicinity of Aloha Stadium (Figure 2).

The Project will continue along Kamehameha Highway makai past Aloha Stadium to Nimitz Highway and turn makai onto Aolele Street and then follow Aolele Street Koko Head to reconnect to Nimitz Highway near Moanalua Stream and continue to the Middle Street Transit Center (Figure 3). Stations will be constructed at Aloha Stadium, Pearl Harbor Naval Base, Honolulu International Airport, and Lagoon Drive.

Koko Head of Middle Street, the guideway will follow Dillingham Boulevard to the vicinity of Ka'aahi Street and then turn Koko Head to connect to Nimitz Highway in the vicinity of Iwilei Road.

The alignment will follow Nimitz Highway Koko Head to Halekauwila Street, and then proceed along Halekauwila Street past Ward Avenue where it will transition to Queen Street and Kona Street. Property on the mauka side of Waimanu Street will be acquired to allow the alignment to cross over to Kona Street. The guideway will run above Kona Street through Ala Moana Center.

Planned extensions would connect at both ends of the corridor. At the Wai'anae end of the corridor, the alignment would follow Kapolei Parkway to Wākea Street and then turn makai to Saratoga Avenue. The guideway would continue on future extensions of Saratoga Avenue and North-South Road. At the Koko Head end of the corridor, the alignment would veer mauka from Ala Moana Center to follow Kapi'olani Boulevard to University Avenue, where it would again turn mauka to follow University Avenue over the H-1 Freeway to a proposed terminal facility in UH Mānoa's Lower Campus. A branch line with a transfer point at Ala Moana Center or the Hawai'i Convention Center into Waikīkī would follow Kalākaua Avenue to Kūhiō Avenue to end near Kapahulu Avenue (Figure 4).

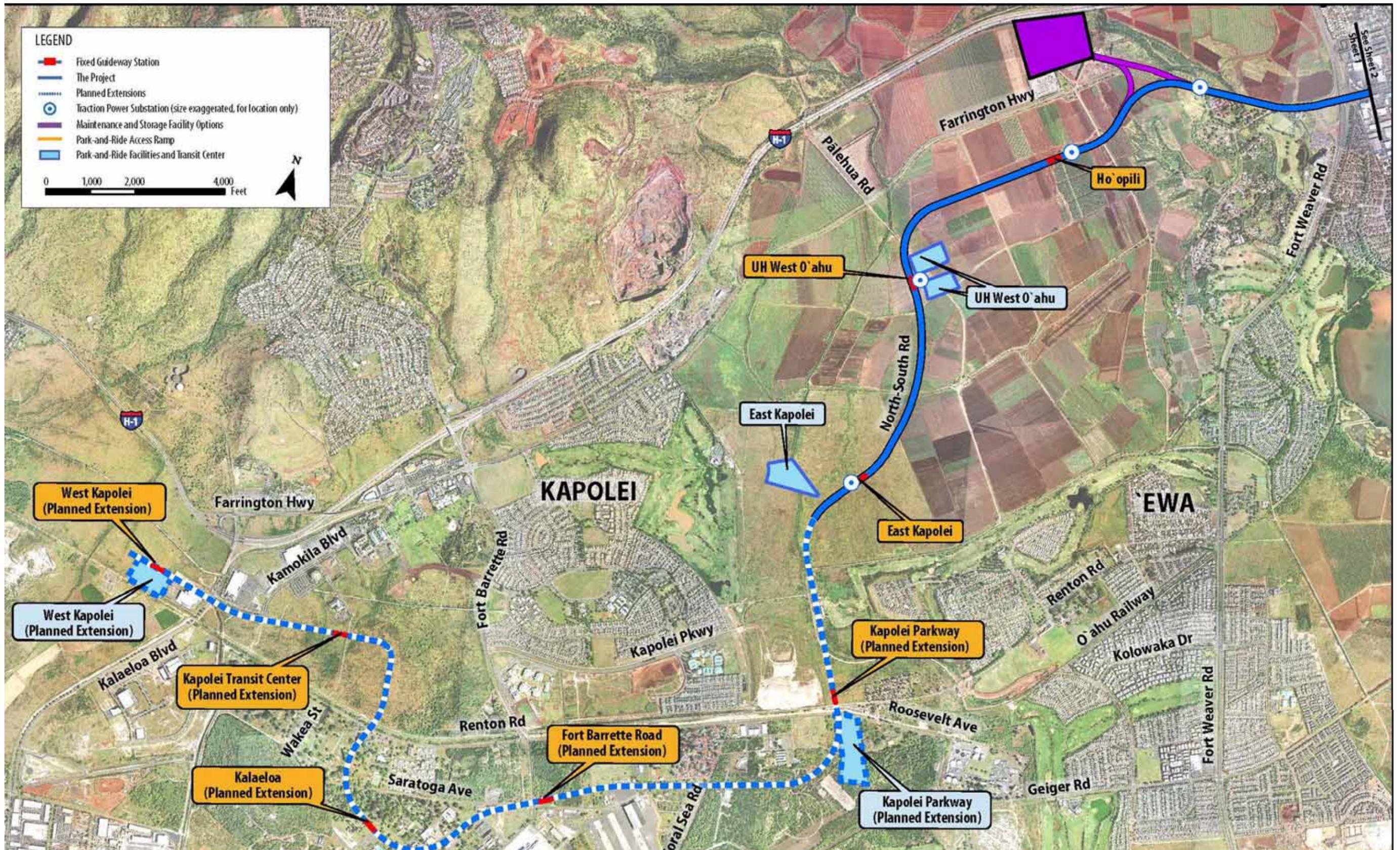


Figure 1. Project Features (Kapolei to Fort Weaver Road)



Figure 2. Project Features (Fort Weaver Road to Aloha Stadium)



Figure 3. Project Features (Aloha Stadium to Kalihi)

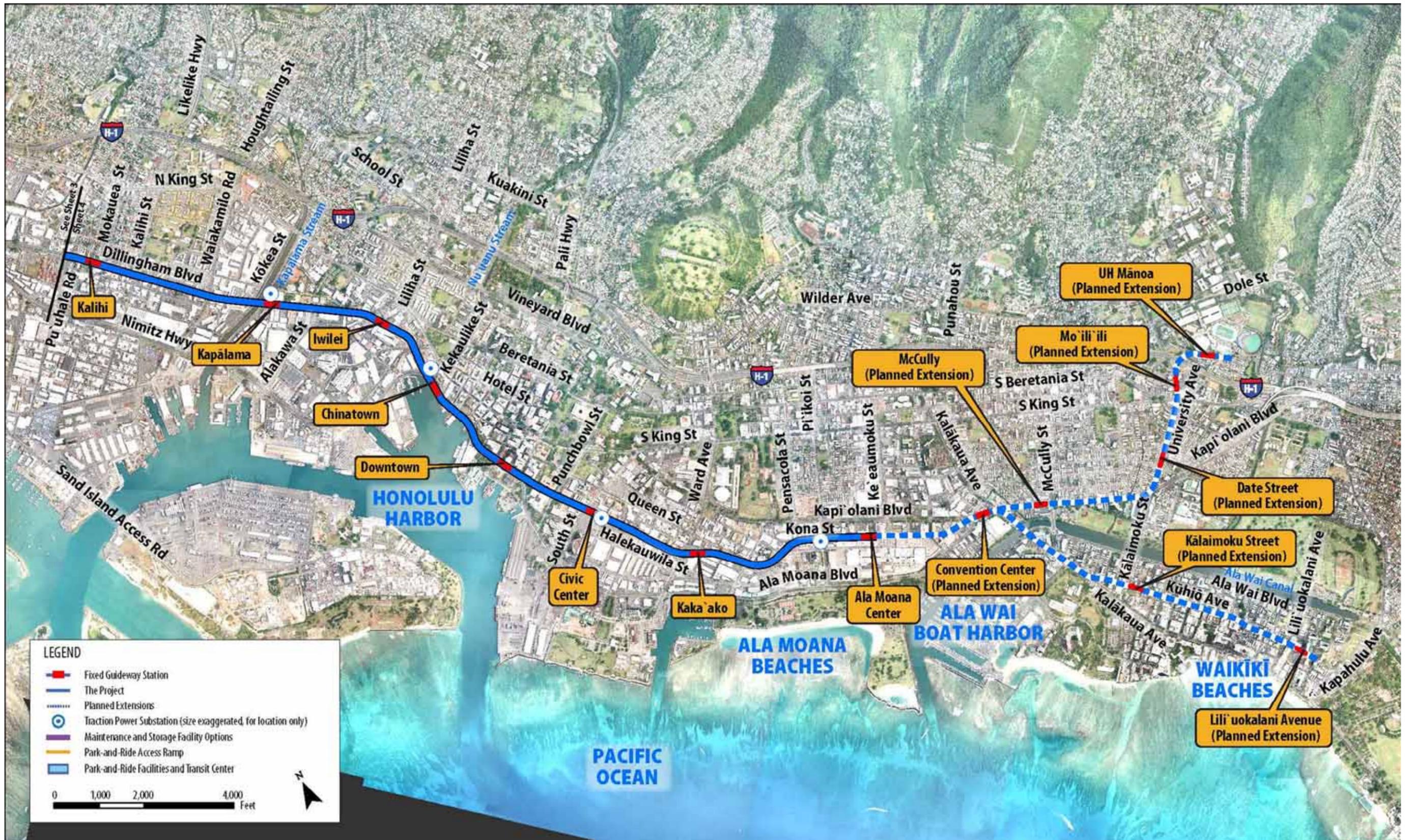


Figure 4. Project Features (Kalihi to UH Mānoa)

The total guideway length for this alternative will be approximately 20 miles and include 21 stations. The eventual guideway length, including planned extensions, for this alternative would be approximately 29 miles and include 33 stations.

In addition to the guideway and transit stations, the Project will require the construction of supporting facilities, including a maintenance and storage facility, transit centers, park-and-ride lots, and traction power substations. The maintenance and storage facility will either be located between North-South Road and Fort Weaver Road or near Leeward Community College (Figure 1 and Figure 2). Some bus service will be reconfigured to transport riders on local buses to nearby fixed guideway transit stations. To support this system, the bus fleet will be expanded.

3.0 Effects Assessment Methodology and Background

Section 106 of the National Historic Preservation Act requires that Federal agencies consider the effects of their undertakings on historic properties and afford the Advisory Council on Historic Preservation a reasonable opportunity to comment on such undertakings. Procedures for carrying out the requirements of Section 106 are outlined in 36 CFR 800. These procedures establish guidelines for identifying participants in the Section 106 process, identifying historic properties that may be affected by the proposed undertaking, assessing effects the undertaking may have on historic properties, and resolving adverse effects a project may have on historic properties.

3.1 *Previous Development and Description of Area of Potential Effect*

The Area of Potential Effect, or APE, is the geographic area within which an undertaking may directly or indirectly alter the character or use of historic properties. The APE for the Project was established in December 2007 in consultation with the Hawai'i State Historic Preservation Division (SHPD) and documented in the August 2008 *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008). The APE is defined as "generally one tax map key (TMK) parcel deep from the project alignments, but larger around stations and in a few other instances." All parcels adjacent to the guideway alignments and all associated facilities are included in the APE. Associated facilities include park-and-ride lots, traction power substations, and the maintenance and storage yard. The APE around transit stations is defined "to include entire blocks (or to extend 500 feet, where blocks are not discernible)." Furthermore, where the guideway is proposed to be constructed through historic districts, the APE is defined "to include entire blocks or a depth of 500 feet." Composite project location maps, showing plans, profiles, and historic property locations, are attached to this report as Appendix A. Relevant typical sections of the guideway are included as Appendix B, and relevant typical sections and plans of transit stations are included as Appendix C.

3.2 Continuing Section 106 Consultation

After completion of the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008), the document was circulated to the SHPD and consulting parties via transmittals dated August 25, 2008. Comments from the SHPD were received via letters dated September 26 and October 3, 2008. These letters expressed various concerns regarding the preliminary evaluations of effect to historic resources, additional properties that should be reevaluated for potential NRHP-eligibility, and possible mitigation measures. Additional comments were received from the Historic Hawai'i Foundation dated September 15, 2008, the Hawai'i Community Development Authority (for Kaka'ako and Kalaeloa) dated September 16, 2008, AIA Honolulu dated September 17, 2008, and the Royal Order of Kamehameha dated October 6, 2008. Among concerns addressed were possible adverse effects to additional historic properties, including the Chinatown Historic District, Aloha Tower, and various bridges. Impacts to the setting of historic properties were a common concern in correspondence from consulting parties.

Additional correspondence received included a letter from the National Trust for Historic Preservation dated October 6, 2008, requesting consulting party status and also expressing concern regarding the preliminary evaluation of adverse effects to historic properties. A letter from the U.S. Navy dated November 12, 2008, was received and expressed concern regarding determinations of eligibility that did not correspond to findings made under previous Section 106 evaluations: the nationwide Programmatic Memorandum of Agreement for World War II Temporary Buildings; the Programmatic Agreement Regarding Navy Undertakings in Hawai'i; and the Navy's Cold War Building Inventory. A letter from the National Park Service (NPS) dated January 6, 2009, was received in which the NPS accepted consulting party status and indicated concerns regarding potential project impacts to the Pearl Harbor National Historic Landmark and historic resources associated with the World War II Valor in the Pacific National Monument. Consulting parties also contacted and expressed concern to the Advisory Council on Historic Preservation. As a result of this contact, the Advisory Council on Historic Preservation transmitted a letter dated January 27, 2009, which expressed additional concern regarding the evaluation of effect determinations and their appropriate documentation.

The "Notice of Availability" for the Project's Draft Environmental Impact Statement (Draft EIS) was published in the *Federal Register* on November 21, 2008. Section 4.15 of the Draft EIS included a summation of Section 106 coordination and findings regarding historic properties as documented in the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008). Comments on the Draft EIS pertaining to Section 4.15 and to the Section 106 process were received via letters from AIA Honolulu dated December 8, 2008; the Historic Hawai'i Foundation dated December 10, 2008; Hawai'i Department of Land and Natural Resources dated February 2, 2009; and the National Park Service dated February 6, 2009. Comments generally expressed continued concerns regarding preliminary effects methodology and determinations and also the evaluation of potential impacts to the Pearl Harbor National Historic Landmark.

Additional consultation meetings and site visits have also been conducted. A meeting with the SHPD, National Trust for Historic Preservation, Historic Hawai'i Foundation, FTA, and the project team occurred on December 17, 2008, to discuss reconsideration of preliminary effects determined for various resources. A field review encompassing the alignment of the Project was conducted with SHPD staff and the Historic Hawai'i Foundation on January 9, 2009. Referenced correspondence is located in Appendix D.

3.3 Identification of Historic Properties

NRHP-listed and NRHP-eligible historic properties located within the APE were identified, documented, and evaluated in the August 2008 *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008). This report was transmitted to the SHPD and all consulting parties for review. The report outlined the methodology employed to locate and research potential historic properties within the APE for NRHP evaluation. Of note, all properties identified as constructed or developed by or before 1968 were evaluated based on their future age of at least 50 years in 2018, the Project's target construction completion date.

Properties were previously listed or determined eligible for listing in the NRHP by applying the Criteria for Evaluation found in 36 CFR 60. These criteria indicate that the quality of significance in American history, architecture, archeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and:

- (a) that are associated with events that have made a significant contribution to the broad patterns of our history; or
- (b) that are associated with the lives of persons significant in our past; or
- (c) that embody distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- (d) that have yielded, or may be likely to yield, information important in prehistory or history. Thus, properties are listed in the NRHP or determined eligible for listing if they are shown to be significant under one or more criteria and if they also retain relevant aspects of integrity related to that criterion.

The National Register guidelines also contain Criteria Considerations that allow properties that normally would not be considered eligible to be listed. The property types that the considerations address include cemeteries, churches, resources that have been moved, and resources that have attained significance within the past 50 years. This Project contains resources that were found to be eligible under these considerations. Because the SHPD concurred that these resources were eligible, effects to them were assessed as well.

Of the 499 properties identified as constructed or developed by or before 1968 and located within the APE of the Project (excluding the Salt Lake Alternative), 9 were listed in the NRHP and 74 were determined to be eligible for listing in the NRHP. Two NRHP-eligible properties, AC Electric and the Tsumoto shophouse, have since been demolished. Survey forms for the extant 81 NRHP-listed and NRHP-eligible properties are located in Appendix E. The remaining surveyed properties were determined not eligible for listing in the NRHP. These findings were coordinated with the SHPD by submittal of the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008) in August 2008 and in following correspondence between the SHPD, the Honolulu Department of Transportation, and the Federal Transit Administration (see correspondence in Appendix D).

Because the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008) organized documentation of historic properties by “station sectors,” this report will follow the same general sequence. Station sectors in the 2008 report were defined to include “properties in the blocks (or within a 500-foot radius) around each [transit] station and the parcels abutting the transit line on either side of the station.” Boundaries for station sectors were chosen to be approximately halfway between stations and to follow logical division lines where applicable, such as at streams, roads, or “notable” parcels (e.g., the ‘Aiea Cemetery). Numbers assigned to each station sector in that report are duplicated in this report. These station sector numbers are not sequential as a result of the removal of certain station locations, the presence of no historic properties within some station sectors, and because the determination does not include some areas that were included in early planning efforts.

Historic properties, including NRHP-listed and NRHP-eligible properties, are identified by the station sectors in Table 1. Full property names as initially shown in the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008), including capitalization and punctuation, are maintained to ensure consistency. Tax map key (TMK) parcel number, property type, and NRHP status and relevant criteria for evaluation are also included.

Table 1: Identified NRHP-listed and NRHP-eligible Historic Properties by Station Sectors

Property Name	TMK Parcel No.	Property Type	NRHP Status	Criteria
Station Sectors 6 through 8: East Kapolei, UH West O'ahu, and Ho'opili from Kalo'i Gulch to Fort Weaver Road/Kunia Road				
Hono'uli'uli Stream Bridge	N/A	Bridge	Eligible	A & C
Station Sector 9: West Loch from Fort Weaver Road /Kunia Road to Waikele Stream				
Lum-Terahira Three-Story Apartments	94039082	Apartment building	Eligible	C
West O'ahu Christian Church/former American Security Bank (round plan)	94027127	Former commercial	Eligible	C
Tanaka-Ishihara House	94025008	House	Eligible	C
Station Sector 10: Waipahu Transit Center from Waikele Stream to Waipi'o Point Access Road /Kahualii Street				
Waikele Stream Bridge east-bound span and Bridge over OR&L spur	N/A	Bridge pair	Eligible	A & C
Ohara & Okahara Two-Story Apartments	94019021, 94019020	Apartment buildings	Eligible	C
Codera-Carvalho Two-Story Apartments/Waipahu Hale	94017043	Apartment building	Eligible	C
Waipahu Hawaii Stake, Church of Jesus Christ of Latter-Day Saints	94036071	Church building	Eligible	A
Station Sector 11: Leeward Community College from Waipi'o Point Access Road/Kahualii Street to the H-1 Freeway (at Farrington Highway overpass)				
Watercress of Hawai'i	96003026	Farm	Eligible	A
Station Sector 12: Pearl Highlands from the H-1 Freeway (at Farrington Highway overpass) to 'Ewa side of Hawaiian Electric Company (HECO) Waiau				
Waiawa Stream Bridge 1932 (west-bound lanes)	N/A	Bridge	Eligible	A & C
Waiawa Separation Bridge	N/A	Bridge	Eligible	A & C
Waiawa Stream Bridge 1952 (east-bound lanes)	N/A	Bridge	Eligible	A & C
Waiawa Booster Pump Station	96003045	Pump station	Eligible	A & C

Property Name	TMK Parcel No.	Property Type	NRHP Status	Criteria
Station Sector 13: Pearlridge from 'Ewa side of HECO Waiiau to 'Ewa side of Aiea Cemetery				
HECO Waiiau Plant	98003010, 98004003	Power plant	Eligible	A
Nishi Service	98006024	Commercial	Eligible	C
Waimalu Stream Bridge	N/A	Bridge	Eligible	A
Waimalu Shopping Center	98022074, 98022081	Commercial	Eligible	A & C
Sumida Watercress Farm	98016046, 98016047	Farm	Eligible	A & C
Kalauao Spring Bridge	N/A	Bridge	Eligible	A
Kalauao Stream Bridge	N/A	Bridge	Eligible	A
Forty Niner Saimin	98018042	Commercial	Eligible	A & C
Akiona House (Quonset)	98018041	House	Eligible	C
Station Sector 14: Aloha Stadium from 'Ewa side of 'Aiea Cemetery to road to CINCPAC Landing				
'Aiea Cemetery/Honolulu Plantation Cemetery	99012006, 99012001	Cemetery	Eligible	A & D
Station Sector 35: Richardson Recreation Center from road to CINCPAC Landing to Arizona Street/Hālawa Drive Intersection with Kamehameha Highway				
United States Naval Base Pearl Harbor National Historic Landmark	Various	Historic district	NHL	
Bombproof Switch Station – Fac. B-6	99003038	Military utility building	Eligible	A & C
Richardson Recreation Center Pool Complex (Swimming Pool – Fac. S-21; Recreation Facility – Fac. 1; Bath House/Locker Room – Fac. 2; Handball Court – Fac. S-20)	99003029	Recreation center	Eligible	A
Kamehameha Highway Bridge over Hālawa Stream (mauka span)	N/A	Bridge	Eligible	A
Commander-in-Chief Pacific Fleet (CINCPACFLT) Headquarters – Fac. 250 National Historic Landmark	99002004	Military office building	NHL	
Fuel Oil Pump-out Pump House - Fac. S-386	99001001	Pump station	Eligible	C

Property Name	TMK Parcel No.	Property Type	NRHP Status	Criteria
Station Sector 36: Pearl Harbor Naval Base (Airport Alternative) from Arizona Street/Hālawā Drive to Valkenburgh Street				
Publications Printing Office and Plant – Fac. 550/District Printing Plant	99001008	Military office building	Eligible	A
Potential Makalapa Navy Housing Historic District	99002004	Historic district	Eligible	A & C
Navy Upper Tank Farm (fuel storage)	99001008	Fuel storage tanks	Eligible	A
Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society - Fac. 1514	99001008	Military office building	Eligible	C
Potential Little Makalapa Navy Housing Historic District	99002004	Historic district	Eligible	A & C
Navy WWII splinterproof shelter - Fac. S-51	99001008	Bomb shelter	Eligible	A & C
Rehab Center/Former Navy Fire Station - Fac. 199	99001008	Former fire station	Eligible	A & C
Station Sector 38: Lagoon Drive (Airport Alternative) from Paiea Street to Middle Street				
Hawai'i Employers Council	11016004	Office building	Eligible	A & C
Station Sector 19: Middle Street from Middle Street to Laumaka Street/'Ewa side of OCCC				
Gaspro Store	12013007	Commercial	Eligible	C
Foremost Dairy	12013006	Dairy complex	Eligible	A & C
Station Sector 20: Kalihi from Laumaka Street/'Ewa side of OCCC to Waiakamilo Road				
Lava Rock Curbs	N/A	Roadway curbing	Eligible	A & C
Pu'uhale Market	12012014	Commercial	Eligible	A
Afuso House	12009017	House	Eligible	A & C
Higa Four-plex	12009017	House	Eligible	A & C
Teixeira House	12009018	House	Eligible	A & C
Pang Craftsman-Style House	12009060	House	Eligible	A & C
10 Courtyard Houses	12002113	Multiple houses	Eligible	A & C
Duarte House	12002108	House	Eligible	A & C
Boulevard Saimin	15029060	Commercial	Eligible	A
Station Sector 21: Kapālama from Waiakamilo Road to Akepo Lane				
True Kamani Trees	N/A	Street tree planting	Eligible	A & C
Kapālama Canal Bridge	N/A	Bridge	Eligible	A & C
Six Quonset Huts	15015008	Quonset buildings	Eligible	A & C

Property Name	TMK Parcel No.	Property Type	NRHP Status	Criteria
Station Sector 22: Iwilei from Akepo Lane to Iwilei Road				
Institute for Human Services/Tamura Building	15007033	Office building	Eligible	C
Tong Fat Co.	15007003	Commercial	Eligible	A & C
Wood Tenement Buildings behind Tong Fat Co.	15007003	Multiple houses	Eligible	A & C
O'ahu Railway & Land Co. Terminal Building	15007001, 15007002	Railway terminal	Eligible	A & C
O'ahu Railway & Land Co. Office & Document Storage Building	15007001, 15007002	Office building	Eligible	A & C
Former filling station on OR&L property	15007001	Commercial	Eligible	A
O'ahu Railway & Land Co. basalt paving blocks	15007001, 15007002	Street pavers	Eligible	A, C, & D
Station Sector 23: Chinatown from Iwilei Road to Queen Street				
Nu'uuanu Stream Bridge	N/A	Bridge	Eligible	A & C
Chinatown Historic District	Various	Historic district	Listed	
Harbor retaining wall of coral blocks from Honolulu Fort	21001056	Harbor wall	Eligible	D
Merchant Street Historic District (incl. Walter Murray Gibson Building/ Honolulu Police Station)	21002024, 21002057	Office building	Listed	
Station Sector 24: Downtown from Queen Street to Punchbowl Street				
DOT Harbors Division	21001005	Office building	Eligible	A
Pier 10/11	21001001	Maritime terminal	Eligible	A & C
Walker Park	N/A	Park	Eligible	A & C
Irwin Park	21013007	Park	Eligible	A, B, & C
Aloha Tower	21001013	Tower	Listed	
Dillingham Transportation Building	21014003	Office building	Listed	
HECO Downtown Plant & Leslie A. Hicks Building	21014006	Power plant	Eligible	A
Hawai'i Capital Historic District (incl. Attorney General's Office/Hale 'Auhau)	21026022	Office building	Listed	
Station Sector 25: Civic Center from Punchbowl Street to Cooke Street				
Department of Transportation	21031012	Office building	Eligible	C
Royal Brewery/The Honolulu Brewing & Malting Co.	21031021	Industrial	Listed	
[Old] Kaka'ako Fire Station	21031018	Fire station	Listed	
Kamaka Ukulele	21030014	Industrial	Eligible	A
Mother Waldron Playground	21051005, 21051006	Park	Eligible	A & C

Property Name	TMK Parcel No.	Property Type	NRHP Status	Criteria
Station Sector 26: Kaka'ako from Cooke Street to Waimanu Street				
Fuji Sake Brewing Company	21052008	Industrial	Eligible	C
Ching Market and House	21050049	Commercial	Eligible	A & C
American Saving Bank/ Liberty Bank – Queen-Ward Branch	21050052	Commercial	Eligible	C
Station Sector 27: Ala Moana Center from Waimanu Street to Mahukona Street				
Pacific Development Office Building	23007029	Office building	Eligible	C
Hawaiian Life Building	23039023	Office building	Eligible	C
Ala Moana Building	23039001	Office building	Eligible	C

3.4 Assessing Effects to Historic Properties

To determine if any identified historic properties would be affected by implementation of the Project, architectural historians reviewed documentation for all identified eligible or listed properties, reviewed current project plans, and made field visits to each eligible or listed property. Site visits were completed in January 2009. Additional photographs of relevant views were taken, and notes addressing each aspect of integrity for each property and potential project impacts were made on forms specifically developed for the Project.

Each historic property within the APE was evaluated in the field within its current context and setting, with regards to its identified historic significance and level of retention of historic integrity, and in relation to changes to the property or within its vicinity that the Project would or may cause. During the field visits, project plans and typical sections were used to evaluate effects. The most up-to date versions of project information were used to assess effects and make determinations. Effects assessments were based on the criteria of adverse effect as defined in 36 CFR 800.5 “Assessment of adverse effects.” According to this portion of the regulations, the criteria of adverse effect are defined as follows:

An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property’s location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property’s eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance, or be cumulative.

Examples of adverse effects are identified in this part and include, but are not limited to, the following:

- Physical destruction of or damage to all or part of the property
- Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of

- handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR 68) and applicable guidelines
- Removal of the property from its historic location
 - Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance
 - Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features
 - Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization
 - Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance

National Register of Historic Places Bulletins do not address assessments of effects, as effects evaluations are related to the Section 106 process and not the Section 110 process in which the National Register guidance is more commonly used. However, crucial information on integrity assessments (used for eligibility determinations) proved helpful as reminders of what each aspect of integrity entails and how each aspect relates to the select National Register criteria for eligibility. As described above, retention of relevant aspects of integrity is critical to a property's significance under the NRHP Criteria for Evaluation. The National Register Bulletin *How to Apply the National Register Criteria for Evaluation* (NPS 1997) identifies the aspects of integrity and describes their relevance to the NRHP Criteria for Evaluation. The seven aspects of integrity are described in the bulletin as follows:

Location is the place where the historic property was constructed or the place where the historic event occurred. The relationship between the property and its location is often important to understanding why the property was created or why something happened. The actual location of a historic property, complemented by its setting, is particularly important in recapturing the sense of historic events and persons.

Design is the combination of elements that create the form, plan, space, structure, and style of a property. It results from conscious decisions made during the original conception and planning of a property (or its significant alteration) and applies to activities as diverse as community planning, engineering, architecture, and landscape architecture. Design includes such elements as organization of space, proportion, scale, technology, ornamentation, and materials. A property's design reflects historic functions and technologies as well as aesthetics. It includes such considerations as the structural system; massing; arrangement of spaces; pattern of fenestration; textures and colors of surface materials; type, amount, and style of ornamental detailing; and arrangement and type of plantings in a designed landscape.

Design can also apply to districts, whether they are important primarily for historic association, architectural value, information potential, or a

combination thereof. For districts significant primarily for historic association or architectural value, design concerns more than just the individual buildings or structures located within the boundaries. It also applies to the way in which buildings, sites, or structures are related.

Setting is the physical environment of a historic property. Whereas location refers to the specific place where a property was built or an event occurred, setting refers to the *character* of the place in which the property played its historical role. It involves *how*, not just *where*, the property is situated and its relationship to surrounding features and open space. Setting often reflects the basic physical conditions under which a property was built and the functions it was intended to serve. In addition, the way in which a property is positioned in its environment can reflect the designer's concept of nature and aesthetic preferences.

The physical features that constitute the setting of a historic property can be either natural or manmade, including such elements as: topographic features (a gorge or the crest of a hill); vegetation; simple manmade features (paths or fences); and relationships between buildings and other features or open space. These features and their relationships should be examined not only within the exact boundaries of the property, but also between the property and its *surroundings*. This is particularly important for districts.

Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property. The choice and combination of materials reveal the preferences of those who created the property and indicate the availability of particular types of materials and technologies. Indigenous materials are often the focus of regional building traditions and thereby help define an area's sense of time and place. A property must retain the key exterior materials dating from the period of its historic significance. If the property has been rehabilitated, the historic materials and significant features must have been preserved.

Workmanship is the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory. It is the evidence of artisans' labor and skill in constructing or altering a building, structure, object, or site. Workmanship can apply to the property as a whole or to its individual components. It can be expressed in vernacular methods of construction and plain finishes or in highly sophisticated configurations and ornamental detailing. It can be based on common traditions or innovative period techniques. Workmanship is important because it can furnish evidence of the technology of a craft, illustrate the aesthetic principles of a historic or prehistoric period, and reveal individual, local, regional, or national applications of both technological practices and aesthetic principles.

Feeling is a property's expression of the aesthetic or historic sense of a particular period of time. It results from the presence of physical features that, taken together, convey the property's historic character.

Association is the direct link between an important historic event or person and a historic property. A property retains association if it is the place where the event or activity occurred and is sufficiently intact to convey that relationship to an observer. Like feeling, association requires the presence of physical features that convey a property's historic character.

According to guidance found in *How to Apply the National Register Criteria for Evaluation*, different aspects of integrity may be more or less relevant dependent on why a specific historic property was listed in or determined eligible for listing in the NRHP. For example, a property that is significant for its historic association (Criteria A or B) is eligible if it retains the essential physical features that made up its character or appearance during the period of its association with the important event, historical pattern, or person(s). A property determined eligible under Criteria A or B ideally might retain some features of all aspects of integrity, although aspects such as design and workmanship might not be as important.

A property important for illustrating a particular architectural style or construction technique (Criterion C) must retain most of the physical features that constitute that style or technique. A property that has lost some historic materials or details can be eligible if it retains the majority of the features that illustrate its style in terms of the massing, spatial relationships, proportion, pattern of windows and doors, texture of materials, and ornamentation. The property is not eligible, however, if it retains some basic features conveying massing but has lost the majority of the features that once characterized its style. A property significant under Criterion C must retain those physical features that characterize the type, period, or method of construction that the property represents. Retention of design, workmanship, and materials will usually be more important than location, setting, feeling, and association. Location and setting will be important, however, for those properties whose design is a reflection of their immediate environment (such as designed landscapes and bridges).

For properties eligible under Criterion D, including archaeological sites and standing structures studied for their information potential, less attention is given to their overall condition than if they were being considered under Criteria A, B, or C. For properties eligible under Criterion D, integrity is based upon the property's potential to yield specific data that address important research questions.

For a historic district to retain integrity as a whole, the majority of the components that make up the district's historic character must possess integrity even if they are individually undistinguished. In addition, the relationships among the district's components must be substantially unchanged since the period of significance.

Using the criteria of adverse effect established in 36 CFR 800.5(a)(1) and guidance found in the National Register Bulletin *How to Apply the National Register Criteria for Evaluation*, each historic property was evaluated to determine if implementation of the Project would alter any historically significant characteristics or features of a historic property by diminishing relevant aspects of that property's historic integrity. This point is particularly important to understand. For some eligible or listed resources within the Project's APE, certain aspects of integrity are not critical to the

reasons that a property was determined to be eligible for listing. For example, regarding potential effects to a historic property determined NRHP-eligible under Criterion C as an example of a type of design, retention of integrity of design, materials, and workmanship would be of greater priority than retention of integrity of setting. In other cases, select aspects of integrity are currently and substantially compromised. These changes may have been made prior to determinations of eligibility or since these determinations were made.

Because of common misunderstandings regarding the application of the criteria of adverse effects to historic properties, it is necessary to clearly state that just because the project may be visible from a historic resource, this does not necessarily constitute an adverse effect. Factors considered for resources that fall into this category include proximity of the guideway to the resource; the significance of viewsheds as called out in prior documentation (including earlier documentation and more recent updates, some of which were completed as part of this Project); and the overall importance of integrity of setting to the resource's determination of eligibility. Prior documentation for each historic property was reviewed to determine under which Criteria for Evaluation a property was deemed eligible for the NRHP, which historic characteristics and features of a property qualified it for NRHP eligibility, and which areas of integrity were most relevant to the eligibility determination and to what degree the property retains them. Because most properties within the APE were recently evaluated and the SHPD concurred with the findings and deemed the documentation to be of an appropriate level of effort, the information on integrity provided on these forms was considered heavily when completing determinations of effect.

As part of the current effects assessment, facts presented in the previous documentation and determinations were noted. Generally, the prior determinations of eligibility and National Register nominations, which were completed by multiple preparers over a number of years from the 1960s until as recently as 2008, did not include detailed integrity assessments, or did not specifically address each aspect of integrity. Therefore, the the current level of integrity for each resource was evaluated because of either a lack of acceptable prior documentation or changes to the resources and surrounding areas in the years since the initial documentation was completed and submitted for review.

Because potential changes to setting were the area where consulting parties had expressed the highest level of concern, particular attention was paid to this aspect of integrity while reviewing prior documentation and assessing integrity in the field. In the previous documentation identifying historic properties in the APE, the historic properties' setting is generally not discussed. Therefore, during the current assessment of effects, information available for each property was reviewed to determine if the setting within and/or outside of the historic boundary, as well as viewsheds to and from historic properties, was historically significant and contributed to the property's eligibility. Using the same information, a determination was made regarding which aspects of integrity were most critical to a historic property's NRHP eligibility. Of note, over the course of the evaluation, it was determined that the majority of historic properties identified within the APE were not associated with

and/or did not retain historic setting. Therefore, when integrity of setting was determined to not be critical to character-defining features and/or National Register eligibility (regardless of whether the individual aspects of integrity were specifically called out in prior documentation) or when integrity of setting was no longer retained, introduction of the rail guideway in a portion of a historic property's setting or viewshed generally resulted in a No Adverse Effect determination.

Noise and vibration analyses were also conducted for properties along the transit corridor. The methodology and findings of this analysis were included in the *Honolulu High-Capacity Transit Corridor Project Noise and Vibration Technical Report* (RTD 2008). The transit vehicles, guideway, and stations have been designed to eliminate or significantly reduce noise impacts to all properties along the transit corridor. Noise-reducing design features include solid 3-foot-high parapet walls on both sides of the guideway and wheel skirts on the vehicles. As a result of these features, severe noise effects were avoided along the corridor and few moderate effects were identified. No noise or vibration effects were identified for historic properties in the project corridor.

After site visits, plan review, and additional documentation for each property, the findings of the assessment of effects have been included in this report. Each property is individually evaluated, and in the case of Pearl Harbor, the National Historic Landmark district and each potentially contributing resource to the district was evaluated to provide an additional layer of assessment to ensure that no adverse effect would occur as a result of the Project. For each historic property, a brief description of the property is offered (see the individual eligibility forms in Appendix E for more detailed descriptions); a description of the Project's plans in the vicinity of each resource is included; and potential project impacts to the aspects of integrity for each property are described. Each assessment includes relevant photographs (with the exception of Pearl Harbor resources, which were not permitted to be photographed because of security reasons, and for Watercress of Hawai'i, because adequate access to the property was not available) and a map with an aerial view of the historic property showing project activity in its vicinity. Additional maps are located in Appendix A and show the project location in its entirety and all project activity and construction, including the rail guideway, station locations, parking, power substations, design profiles, and additional relevant project information. Typical sections in the vicinity of historic properties are referenced in the text and located in Appendix B. Where applicable, transit stations located in the vicinity of historic properties are also referenced, and station plans are included in Appendix C.

For each historic property, a finding has been made regarding the Project's potential to affect each aspect of integrity. While only one aspect of integrity for a single property may receive a No Adverse Effect or Adverse Effect determination, while all other aspects receive No Effect determinations, the property's effect determination will correspond with the effect determination that accounts for the greatest impacts to the overall property. Following guidelines set forth in 36 CFR 800 and supported by information on integrity set forth in the National Register Bulletin *How to Apply the*

National Register Criteria for Evaluation, the following findings were used to assess project effects to historic properties:

- **No Effect:** Per 36 CFR 800.4(d)(1), an undertaking may be deemed to have no effect to historic properties present in the APE, and a finding of “No historic properties affected” may be determined for an undertaking. This finding indicates that an undertaking would not alter any aspects of integrity for any historic properties. This rationale has been used to assess effects to individual historic properties within the APE for the guideway alignment. If project implementation would not alter a specific aspect of integrity for an individual historic property, then the finding for that aspect of integrity is “No Effect.” If no aspect of integrity for an individual historic property is altered, then the finding for the property itself is “No Effect.”
- **No Adverse Effect:** Per 36 CFR 800.5(b), an undertaking may be determined to have “no adverse effect” to historic properties if the undertaking’s effects do not meet the criteria of adverse effect as described above. If project implementation would alter a specific aspect of integrity for an individual historic property but the effect would not alter a characteristic that qualifies that property for inclusion in the NRHP in a manner that diminishes the significant aspect of integrity, then the finding for that aspect of integrity is “No Adverse Effect.” If a finding of “No Adverse Effect” is determined for any aspect of integrity for an individual historic property, but no effects are determined to be adverse, then the finding for the property itself is “No Adverse Effect.”
- **Adverse Effect:** An adverse effect is determined if the undertaking would alter a characteristic that qualifies that property for inclusion in the NRHP in a manner that diminishes the significant aspect(s) of integrity. If after applying the criteria of adverse effect to an individual historic property and an adverse effect is determined for one or more aspects of integrity, then the finding for the property itself is “Adverse Effect.”

3.5 Indirect and Cumulative Effects to Historic Properties

Based on the type, scope, and magnitude of the Project, potential indirect and cumulative effects to historic properties within the project corridor were considered and assessed.

Predicting land use changes caused by project implementation from those that would occur regardless may be difficult or impossible given historic and recent real estate market conditions on O’ahu and in Honolulu. Nonetheless, implementation of the Project would likely indirectly and/or cumulatively alter select historic and non-historic properties along the transit corridor as surrounding communities adapt to its presence. Historic properties identified in this report may be affected by such changes. These effects could be beneficial and include the continued use, reuse, or rehabilitation of historic properties spurred by economic redevelopment resulting from the Project. Alternatively, these effects could be adverse and alter the characteristics of historic properties that qualify them for inclusion in the NRHP. For

example, adverse effects related to land use change could include alterations in use of, physical alterations to, and the removal of historic properties.

Because the scope and magnitude of the Project, which traverses a substantial portion of O'ahu's coastline, is unprecedented in state history, and because the Project's full future effects cannot be known, this document presumes additional, unidentifiable adverse effects to historic properties in the project APE. These presumed adverse effects cannot be adequately documented, but their likelihood shall inform the discussion of appropriate mitigation measures stipulated in a forthcoming Memorandum of Agreement to address adverse effects to historic properties resulting from implementation of the Project.

4.0 Avoidance Alternatives, Planning to Minimize Harm, and Mitigation

During the Alternatives Analysis phase of the Project, planners and design staff were made aware of the presence of potential historic properties within corridors under consideration for the introduction of transit. Preliminary screening, including background research and windshield surveys, was used to determine the likely presence of NRHP-listed and NRHP-eligible historic properties. This screening effort indicated that the Project, as described in this document, would likely have the least impact to historic properties compared to the other transit corridor alternatives considered.

Efforts have been made to avoid physical impacts to historic properties or encroachments into their NRHP boundaries. For environmental and functional considerations, the guideway alignment has been proposed to be located within the existing right-of-way of roadway alignments wherever possible.

Per 36 CFR 800.6, findings of adverse effect to historic properties require that efforts to resolve such effects by developing and evaluating alternatives or modifications to the undertaking that could avoid, minimize, or mitigate adverse effects must be undertaken. As described above, the presence of historic properties and potential impacts to them were included in the criteria used during the Alternatives Analysis and selection of the final project alignment. Furthermore, project planners and designers developed the project alignment in a manner to reduce and minimize potential direct effects to historic properties wherever possible. Efforts to place the alignment in a way that affected the fewest number of resources possible, considered rare or unique historic resources, and/or minimized the type of effect to each resource were strongly considered and implemented wherever possible.

Although these measures minimized potential harm to historic properties by reducing the number of properties affected and the magnitude of potential effects to them, findings of adverse effect to individual historic properties within the APE for this undertaking have been made. Therefore, the Project as a whole is considered to have an adverse effect on historic properties. Additional coordination with the SHPD and consulting parties will be undertaken to develop appropriate mitigation measures to address the adverse effects to historic properties identified in this

document. Such efforts will be documented under separate cover in a Memorandum of Agreement.

5.0 Assessment of Effects to Historic Properties

Potential project effects to each historic property are addressed in the following sections.

5.1 Hono‘uli‘uli Stream Bridge

08 Ho‘opili Station sector—TMK not applicable
Farrington Highway over Hono‘uli‘uli Stream

Property description

The Hono‘uli‘uli Stream Bridge is a reinforced-concrete T-beam bridge constructed in 1939. The property was determined eligible for the NRHP under Criterion A for its association with the history of government road development in southwestern O‘ahu and under Criterion C as a good example of a concrete T-beam bridge from the late 1930s.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure immediately mauka of the Hono‘uli‘uli Stream Bridge (see Figure 5 and Drawings HP004a and HP005 in Appendix A). The makai edge of the guideway would overhang the mauka edge of the bridge. The guideway’s design in this area would correspond to the typical section illustrated in Figure B-2 in Appendix B and would be elevated approximately 40 feet above grade and the bridge.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Hono‘uli‘uli Stream Bridge.

Location: No Effect. The Project would not cause the property’s removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not result in the bridge being removed or physically altered. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. Construction of the guideway would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: Adverse Effect. This property retains its historically rural setting. Although a modern building complex is located makai of this structure, it is partially screened from the historic property by intervening vegetation and thus constitutes a small portion of the property’s viewshed and setting. Mauka views to mountains are intact and also constitute a significant element within the property’s intact historic setting. The Project would introduce an elevated, fixed guideway rail structure immediately mauka of the bridge. The guideway would be elevated approximately 40 feet above grade and the historic bridge, and a portion of the guideway’s makai edge would span a portion of the bridge’s mauka edge. The guideway’s construction immediately mauka of—and spanning portions of—the bridge would introduce a physical and visual element out of character with the property’s historic setting, both within and outside

of the NRHP boundary. The modern guideway would become the prominent element in the property's setting, would partially obscure mauka views from and to the bridge, and would generally obscure the historic appearance of the bridge. Although setting was not specifically identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, the guideway's placement in the property's setting would alter the intact historic setting in a manner that significantly diminishes its integrity in this area. Therefore, the Project would have an adverse effect to the property's integrity of setting. Because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be physically altered. Careful consideration was given to the placement of columns to avoid affecting the resource directly. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter the property's historic setting. Furthermore, the guideway's placement immediately adjacent to and partially over the historic bridge would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter the property's historic setting. Furthermore, the guideway's placement immediately adjacent to and partially over the historic bridge would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

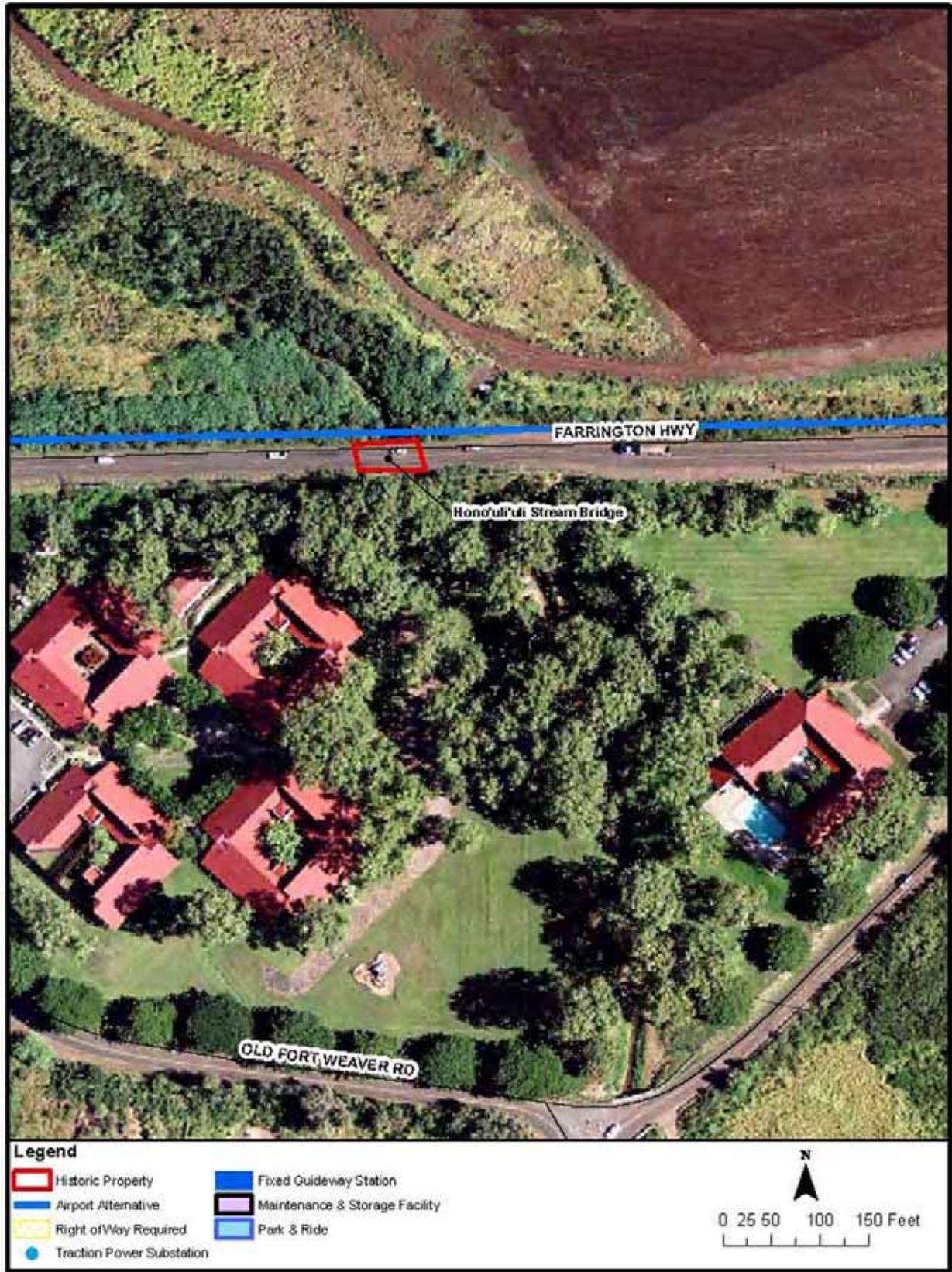


Figure 5. Hono'uli'uli Stream Bridge



Figure 6. Hono‘uli‘uli Stream Bridge, facing mauka/‘Ewa to bridge



Figure 7. Hono‘uli‘uli Stream Bridge, facing makai/‘Ewa to bridge and non-historic property

5.2 Lum-Terahira Three-story Apartments

09 West Loch Station sector—TMK 94039082

94-143 Pupukahi Street

Property description

The Lum-Terahira Three-story Apartments building was constructed in 1965. The property was determined eligible for the NRHP under Criterion C because it embodies the distinctive characteristics of a 1960s walk-up apartment building. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway just mauka of the Lum-Terahira Three-story Apartments building (see Figure 8 and TMK as shown on Drawing HP006 in Appendix A). The makai edge of the guideway would be approximately 40 feet from the mauka edge of the NRHP boundary and approximately 70 feet from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-3 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Lum-Terahira Three-story Apartments.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. The property is surrounded on all sides by both contemporary and non-historic commercial and residential properties, including a non-historic, high-rise apartment building immediately Koko Head. The building is also oriented 'Ewa to Pupukahi Street; the elevation facing heavily travelled Farrington Highway and the Project area is secondary. Properties and features located within the property's setting and viewshed do not contribute to the historic significance of this property. Furthermore, no historically significant viewsheds to or from this property were

identified. The Project would introduce an elevated, fixed guideway rail structure mauka of the property and within the center of the adjacent highway alignment that would be visible from the mauka portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-to-late-20th-century apartment building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-to-late-20th-century apartment building. Therefore, the Project would have no effect to the property's integrity of association.

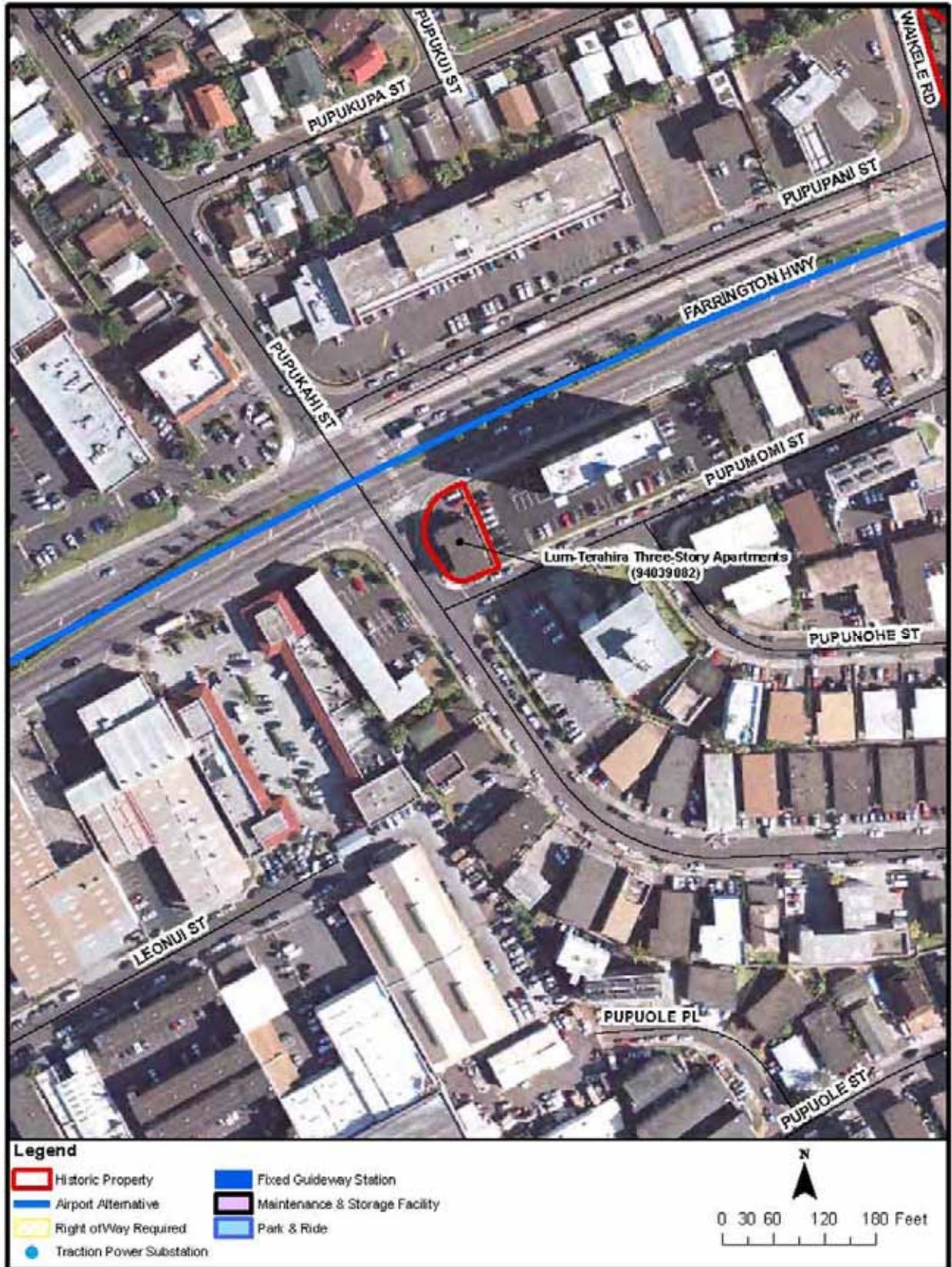


Figure 8. Lum-Terahira Three-Story Apartments



Figure 9. Lum-Terahira Three-Story Apartments, facing mauka to Farrington Highway from just makai of property



Figure 10. Lum-Terahira Three-Story Apartments, facing mauka to Farrington Highway from property

5.3 West O‘ahu Christian Church/former American Security Bank (round plan)

09 West Loch Station sector—TMK 94027127
94-420 Farrington Highway

Property description

The West O‘ahu Christian Church/former American Security Bank (round plan) building is a single-story, circular plan building originally constructed as a bank in 1962. The property was determined eligible for the NRHP under Criterion C as a good example of circular-plan architecture used in Hawai‘i in the 1960s and an expression of the State’s “up-to-date architectural repertoire.” Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway just makai of the West O‘ahu Christian Church/former American Security Bank (round plan) building (see Figure 11 and TMK as shown on Drawing HP006 in Appendix A). The mauka edge of the guideway would be approximately 40 feet from the makai edge of the NRHP boundary and the building. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-3 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to West O‘ahu Christian Church/former American Security Bank (round plan).

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting. The property is surrounded on all sides by both contemporary and non-historic commercial, residential, and institutional properties. Although the property’s setting was not initially identified as a historically significant feature of the property, architectural historians have determined that the property’s mauka viewshed to the

'Ewa Plain and surrounding mountain ranges is an integral component of the property and building's design. The mauka elevation of the building is fenestrated with multiple large windows, presumably to accommodate mountain views; in contrast, the makai elevation is fenestrated with few and small windows, possibly to reduce or eliminate visibility of the adjacent heavily travelled Farrington Highway. Because the property was determined eligible for the NRHP under Criterion C for the building's design, the mauka viewshed to the 'Ewa Plain and surrounding mountains is a historically significant and character-defining feature of the property's setting. This portion of the property's setting is intact.

The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent Farrington Highway alignment that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the property's makai setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. The property's mauka viewshed from the building is a historically significant and character-defining feature of the property, whereas the makai viewshed and setting are neither intact nor evaluated as historically significant. The mauka viewshed from the building would not be altered. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-to-late-20th-century circular building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-to-late-20th-century circular building. Therefore, the Project would have no effect to the property's integrity of association.



**Figure 11. West O'ahu Christian Church/
former American Security Bank (round plan)**



Figure 12. West O'ahu Christian Church, facing makai to property from Waikele Road



Figure 13. West O'ahu Christian Church, facing 'Ewa from property along Farrington Highway

5.4 Tanaka-Ishihara House

09 West Loch Station sector—TMK 94025008
94-256 Farrington Highway

Property description

The Tanaka-Ishihara House is a single-story vernacular house constructed in 1956. The property was determined eligible for the NRHP under Criterion C because it embodies the distinctive characteristics of post-World War II, single-wall, vernacular houses and is representative of the evolution of utilitarian housing in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway just makai of the Tanaka-Ishihara House (see Figure 14 and TMK as shown on Drawing HP006 in Appendix A). The mauka edge of the structure would be approximately 40 feet from the makai edge of the NRHP boundary and approximately 50 feet from the house. The guideway's design in this area would correspond to the typical section illustrated as Figure B-4 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Tanaka-Ishihara House.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting. The property is surrounded by both contemporary and non-historic commercial and residential properties. Although the property's setting was not identified as a historically significant feature of the property during the Assessment of Effects evaluation, architectural historians determined that the property's mauka viewshed to the 'Ewa Plain and surrounding mountain ranges may be a noteworthy feature of the house's design. The house is elevated in the rear and appears to have unobstructed views mauka. Makai views from the house to heavily travelled Farrington Highway do not appear to have any historic significance and are also obstructed by the attached carport. Because the property was determined eligible for the NRHP under Criterion C for the house's design, the mauka viewshed to the 'Ewa Plain and surrounding mountains is a historically significant and character-defining feature of the property's setting. This portion of the property's setting is intact.

The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent Farrington Highway alignment that would be visible from the makai portion of the property, which is also the property's front yard. Although the guideway's construction would introduce a new component to the property's makai setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. The property's mauka viewshed from the house is a historically significant and character-defining feature of the property, whereas the makai viewshed and setting are neither intact nor evaluated as historically significant. The mauka viewshed from the building would not be altered. Furthermore, no audible or atmospheric affects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century vernacular house. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century vernacular house. Therefore, the Project would have no effect to the property's integrity of association.

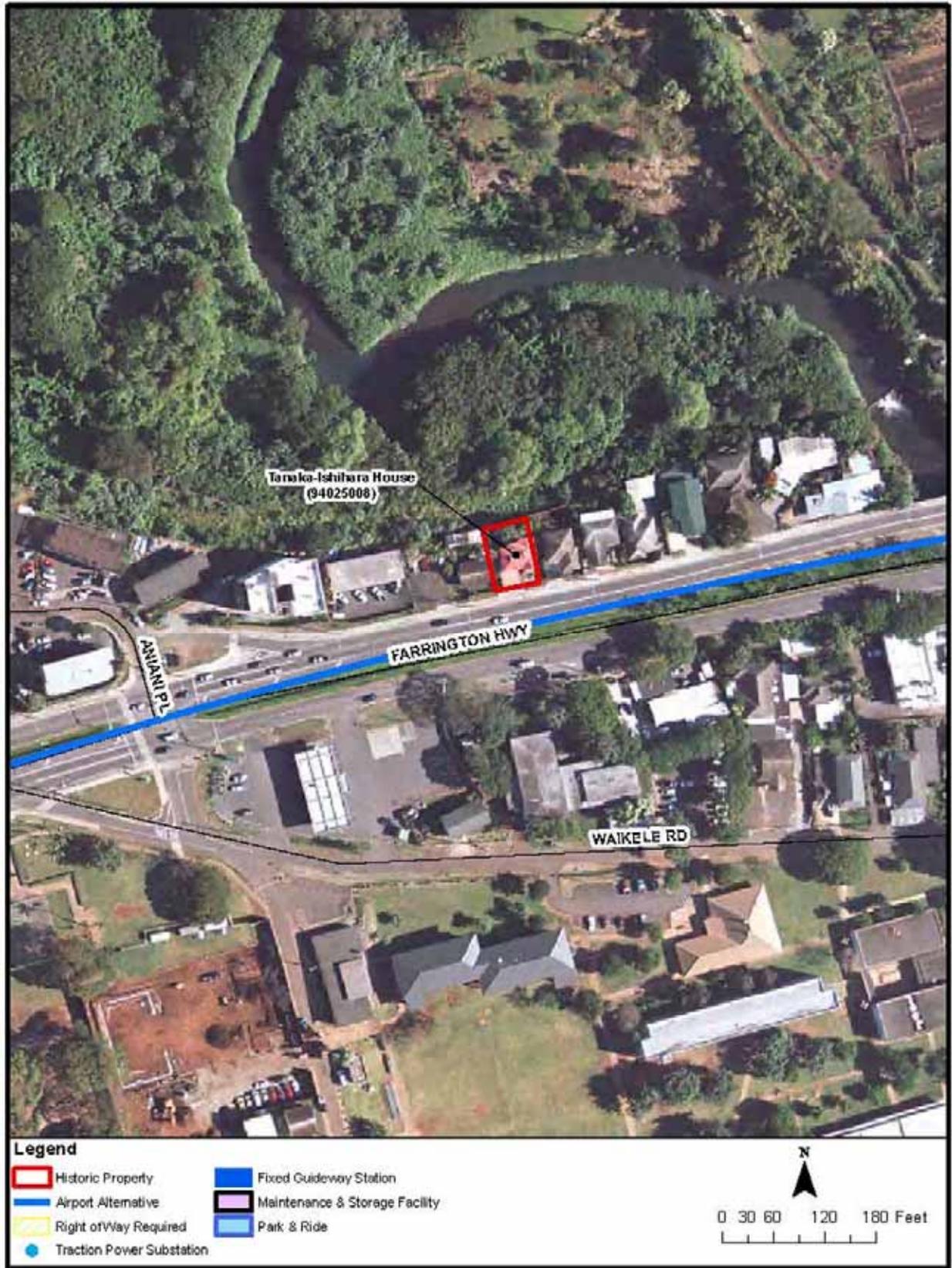


Figure 14. Tanaka-Ishihara House



Figure 15. Tanaka-Ishihara House, facing 'Ewa from property along Farrington Highway



Figure 16. Tanaka-Ishihara House, facing Koko Head from property along Farrington Highway

5.5 Waikele Stream Bridge Eastbound Span and Bridge over OR&L Spur

10 Waipahu Transit Center Station sector—TMK not applicable
Farrington Highway over Waikele Stream

Property description

The Waikele Stream Bridge Eastbound Span and Bridge over OR&L Spur are a pair of concrete deck girder bridges constructed in 1939. The property was determined eligible for the NRHP under Criterion A for its association with the development of the Waipahu community and the transportation history of the area, as well as under Criterion C as good examples of concrete deck girder bridge engineering and design in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure approximately adjacent to and up to 10 feet mauka of the eastbound span of the Waikele Stream Bridge and the bridge over the former OR&L spur line (see Figure 17 and Drawing HP006 in Appendix A). This location would be in the median of Farrington Highway. The guideway's design in this area would correspond to the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 25 to 30 feet above grade and the pair of bridges. In addition, a traction power substation would be constructed approximately 75 feet mauka of both bridges and between Waikele Stream and the former alignment of the defunct OR&L railroad spur.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Waikele Stream Bridge Eastbound Span and Bridge over OR&L Spur.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. Although the Waikele Stream Bridge still spans that waterway, the bridge over the OR&L railroad spur no longer spans an intact rail alignment. The tracks appear to have been removed. Furthermore, although vegetation on both stream banks partially screens the bridges' viewsheds to surrounding development, this development is not historic and comprises a significant portion of the property's viewshed. The Project would introduce an elevated, fixed guideway rail structure

approximately adjacent to and up to 10 feet mauka of the bridges, as well as a power substation approximately 80 feet makai of the bridges. The guideway would be elevated from 25 to 30 feet above grade and the historic bridge pair.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction just mauka of the bridges would significantly alter the property's immediate physical and visual setting. The guideway's proximity to the historic bridges would generally obscure the historic appearance of the bridge pair and their relationship to their surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. Because the property's setting and viewsheds beyond its NRHP boundary are neither historically significant nor intact, construction of the power substation would not alter a historically significant feature of the property. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's construction in close proximity to the historic bridges would obscure the property's historic appearance and immediate environment. Although the bridges would remain physically intact, the proximity of the guideway to the bridges would substantially obscure the visual relationship between the bridges and their immediate surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Feeling and Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's construction in close proximity to the historic bridges would obscure the property's historic appearance and immediate environment. Although the bridges would remain physically intact, the proximity of the guideway to the bridges would substantially obscure the visual relationship between the bridges and their immediate surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

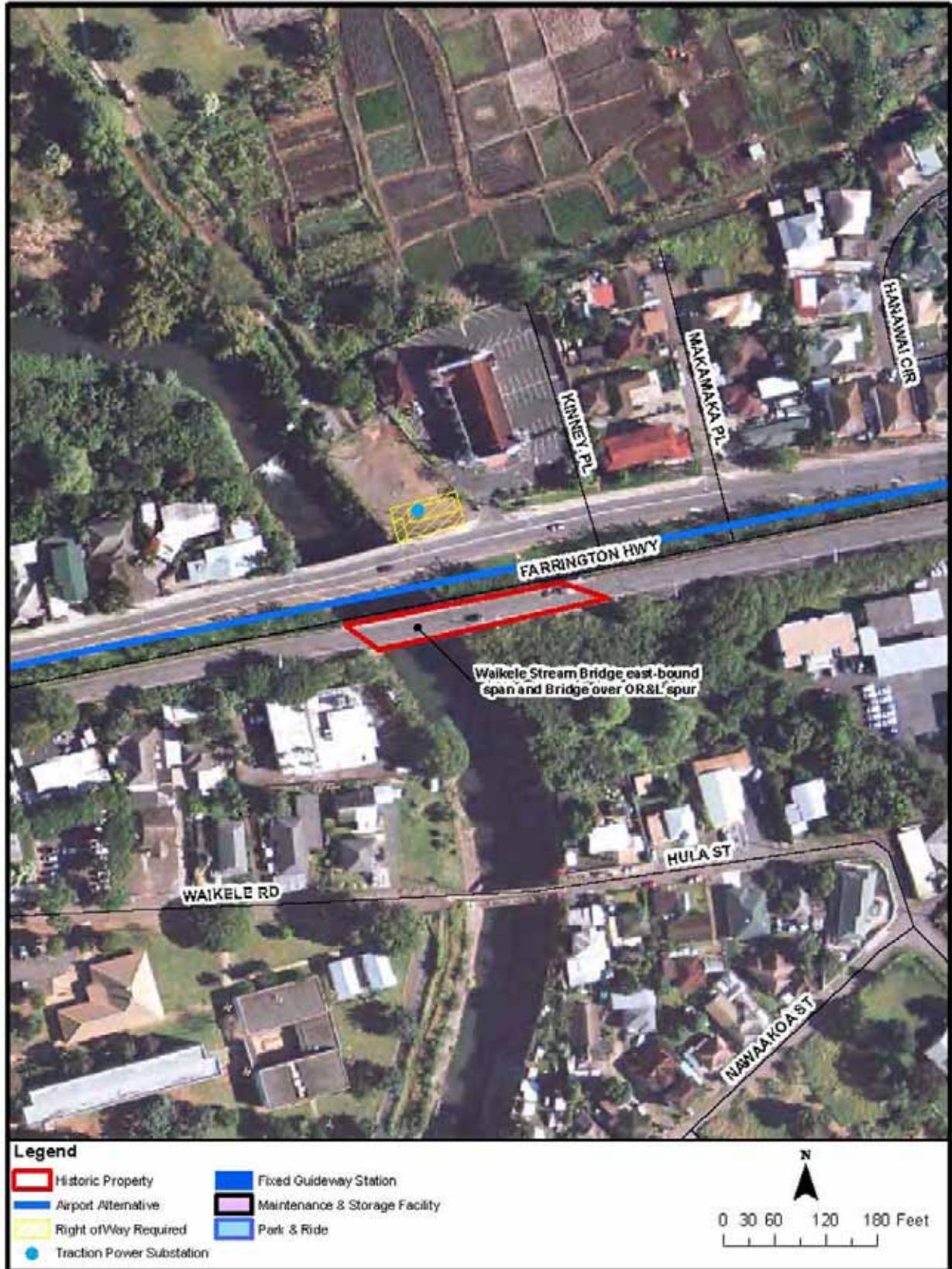


Figure 17. Waikele Stream Bridge Eastbound Span and Bridge over OR&L Spur



Figure 18. Waikele Stream Bridge and Bridge over OR&L spur, facing 'Ewa from Bridge over OR&L spur to Waikele Stream Bridge/Farrington Highway



Figure 19. Waikele Stream Bridge and Bridge over OR&L spur, facing mauka/Koko Head from Bridge over OR&L spur

5.6 Ohara and Okahara Two-Story Apartments

10 Waipahu Transit Center Station sector—TMKs 94019020, 94019021
94-965 and 94-973 Awane Street

Property description

The Ohara and Okahara Two-Story Apartments is a pair of apartment complexes constructed in 1956. The property was determined eligible for the NRHP under Criterion C because the two buildings embody the distinctive characteristics of early post-WWII vernacular apartment buildings.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway approximately 200 feet mauka of the Ohara and Okahara apartment buildings (see Figure 20 and TMKs as shown on Drawing HP007 in Appendix A). The guideway's design in this area would correspond to the typical section illustrated as Figure B-5 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Effect** to the Ohara and Okahara Two-Story Apartments.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. Furthermore, the property would be completely screened from the project area by intervening two-story buildings located between the apartment buildings and the project area. No audible or atmospheric affects to this property were identified. Therefore, the Project would have no effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a pair of mid-20th-century apartment buildings. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a pair of mid-20th-century apartment buildings. Therefore, the Project would have no effect to the property's integrity of association.

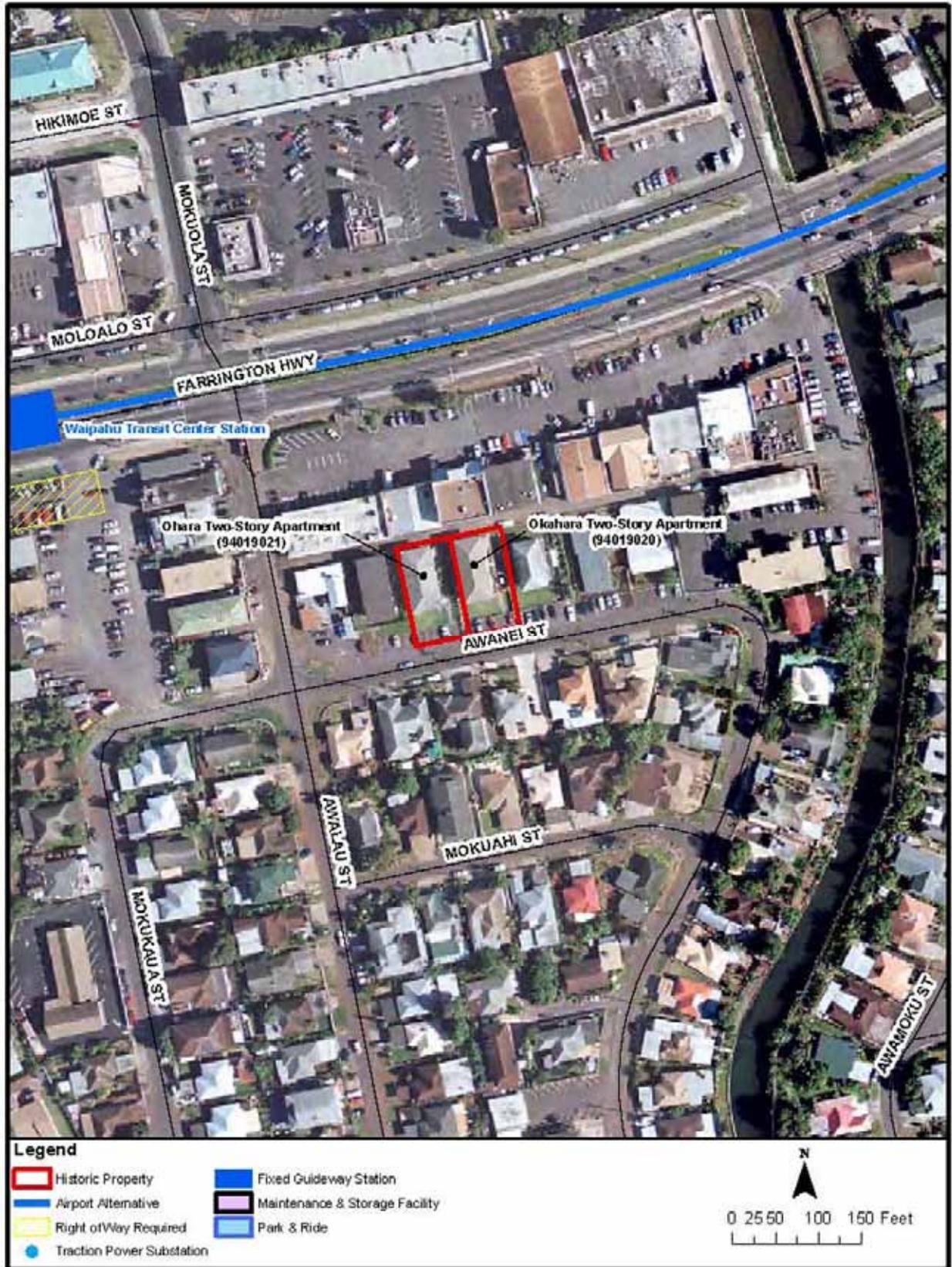


Figure 20. Ohara and Okahara Two-Story Apartments



Figure 21. Ohara and Okahara Two-Story Apartments, facing mauka from Anawei Street to apartment buildings



Figure 22. Ohara and Okahara Two-Story Apartments, facing mauka to Ohara Building and Farrington Highway

5.7 Codera-Carvalho Two-Story Apartments/Waipahu Hale
10 Waipahu Transit Center Station sector—TMK 94017043
94-1031 Kahuamoku Street

Property description

The Codera-Carvalho Two-Story Apartments/Waipahu Hale was constructed in 1965. The property was determined eligible for the NRHP under Criterion C because it embodies the distinctive characteristics of a post-World War II walk-up apartment building. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway makai of the Codera-Carvalho Two-Story Apartments/Waipahu Hale building (see Figure 23 and TMK as shown on Drawing HP007 in Appendix A). The mauka edge of the guideway would be approximately 40 feet from the makai edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-5 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Codera-Carvalho Two-Story Apartments/Waipahu Hale.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature of the property. The building is oriented 'Ewa to Koko Head; entries to apartments and window fenestration are located exclusively on these elevations. No entries or windows are located on the mauka or makai elevations. No windows are oriented to the heavily travelled Farrington Highway and the project area. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent highway alignment that would be partially visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant

views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-to-late-20th-century apartment building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-to-late-20th-century apartment building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 23. Codera-Carvalho Two-Story Apartments/Waipahu Hale



Figure 24. Codera-Carvalho Two-Story Apartments/Waipahu Hale, facing makai from Kahuamoka Street to property and Farrington Highway



Figure 25. Codera-Carvalho Two-Story Apartments/Waipahu Hale, facing Koko Head from property along Farrington Highway

5.8 Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints

10 Waipahu Transit Center Station sector—TMK 94036071
94-210 Kahualii Street

Property description

The Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints is a rectangular-plan, single-story church building constructed in 1963. The property was determined eligible for the NRHP under Criterion A for its association with the establishment and development of the Samoan community on O'ahu and in the Waipahu area. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Farrington Highway just makai of the Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints church building (see Figure 26 and TMK as shown on Drawing HP007 in Appendix A). The mauka edge of the guideway would be approximately 30 feet from the makai edge of the NRHP boundary and approximately 125 feet from the church building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-6 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP-eligible boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature, and no historically significant viewsheds to or from this property were identified. The building is oriented Koko Head to Kahualii Street; the primary entrance is on the Koko Head elevation. The building is surrounded by asphalt-paved parking lots. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent highway

alignment that would be partially visible from the makai portion of the property. The church building is visually and physically separated from heavily travelled Farrington Highway and the project area by a parking lot and both a masonry privacy wall and a line of mature trees along the property's makai parcel boundary (also the NRHP boundary). These features and the distance from the building to the project area would partially screen the property from the guideway. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP-eligible boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-to-late-20th-century church building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-to-late-20th-century church building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 26. Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints



Figure 27. Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints, facing makai/'Ewa from property to Farrington Highway



Figure 28. Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints, facing makai from property to Farrington Highway

5.9 Watercress of Hawai'i

11 Leeward Community College Station sector—TMK 96003026
96-121 Waiawa Road

Property description

Watercress of Hawai'i is an approximately 35-acre agricultural property used to cultivate watercress. The property contains numerous watercress beds that are partitioned by low walls. Waiawa Spring, unnamed springs, and artesian water sources are channeled through the beds. The property was determined eligible for the NRHP under Criterion A for its association with history of wetland agriculture in the Waiawa area.

Project description in vicinity of property

The Project would include construction of an approximately 43-acre transit vehicle maintenance and storage facility mauka of the 'Ewa portion of the historic property. This maintenance and storage facility would include extensive track work, storage of vehicles, and single-story buildings to house equipment and vehicles while under repair (see Figure 29 and TMK as shown in Drawing HP008a in Appendix A). The property that would contain the maintenance and storage facility is across Waiawa Road from the Watercress of Hawai'i and is on considerably higher ground that overlooks the agricultural property. The parcels are approximately 50 feet apart.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Watercress of Hawai'i.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature of the property, and no historically significant viewsheds to or from this property were identified. The Project would include construction of a transit vehicle maintenance and storage facility on higher ground mauka of the smaller 'Ewa portion of the Watercress of Hawai'i property. This portion of the historic property represents only a small portion of the larger agricultural property. Views from the historic property mauka to the facility would be screened by the difference in elevation between the two properties; only vehicles temporarily stored along the makai edge of the maintenance and storage facility would be visible from the 'Ewa portion of the Watercress of Hawai'i property. Although the facility's construction and vehicle storage would introduce a new component to a portion of the property's

setting and viewshed, the presence of the facility and parked vehicles would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to the historic property were identified. Therefore, because no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a watercress farm. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a watercress farm. Therefore, the Project would have no effect to the property's integrity of association.



Figure 29. Watercress of Hawai'i

5.10 Waiawa Stream Bridge 1932 (westbound lanes)

12 Pearl Highlands Station sector—TMK not applicable
Farrington Highway westbound over Waiawa Stream

Property description

The Waiawa Stream Bridge 1932 (westbound lanes) is a six-span, reinforced-concrete continuous deck girder bridge constructed in 1932. The property was determined eligible for the NRHP under Criterion A for its association with the transportation history of the area and under Criterion C as a good example of concrete bridge engineering in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform immediately mauka of the bridge and a park-and-ride facility 40 feet mauka of the bridge (see Figure 30 and Drawing HP009 in Appendix A). A portion of the makai edge of the guideway and station would overhang the Koko Head end of the bridge. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 65 feet above grade and the bridge. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-5 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Waiawa Stream Bridge 1932 (westbound lanes).

Location: No Effect. The Project would not cause the property's removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be removed or physically altered. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. The guideway's construction would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. Within the NRHP boundary, the Waiawa Stream Bridge still spans that waterway. Although vegetation substantially screens the bridge's viewsheds to surrounding development, this development is not historic and comprises a significant portion of the property's viewshed. The Project would include construction of an elevated, fixed-guideway rail structure and passenger station immediately mauka of the bridge and a park-and-ride facility approximately 40 feet mauka of the bridge. Construction of the structure and the park-and-ride facility would require

significant vegetation removal mauka of the bridge and would increase views to surrounding non-historic commercial development and transit-related facilities. The guideway and station would also be elevated approximately 65 feet above grade and the bridge; the structure would cross the Koko Head end of the bridge.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, construction of the guideway and station just mauka and over a portion of the bridge would significantly alter the immediate physical and visual setting of the property. The proximity of the guideway and station to the historic bridge would generally obscure the historic appearance of the bridge and its relationship to its surroundings. In addition, the modern structure and adjacent park-and-ride facility would become prominent elements in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be physically altered. Careful consideration was given to the placement of columns so as to avoid affecting the resource directly. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be physically altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The introduction of the guideway, station, and park-and-ride facility would significantly alter the property's historic setting. Furthermore, construction of the guideway and station immediately adjacent to and partially over the historic bridge would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling and association.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The introduction of the guideway, station, and park-and-ride facility would significantly alter the property's historic setting. Furthermore, construction of the guideway and station immediately adjacent to and partially over the historic bridge would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of feeling and association.

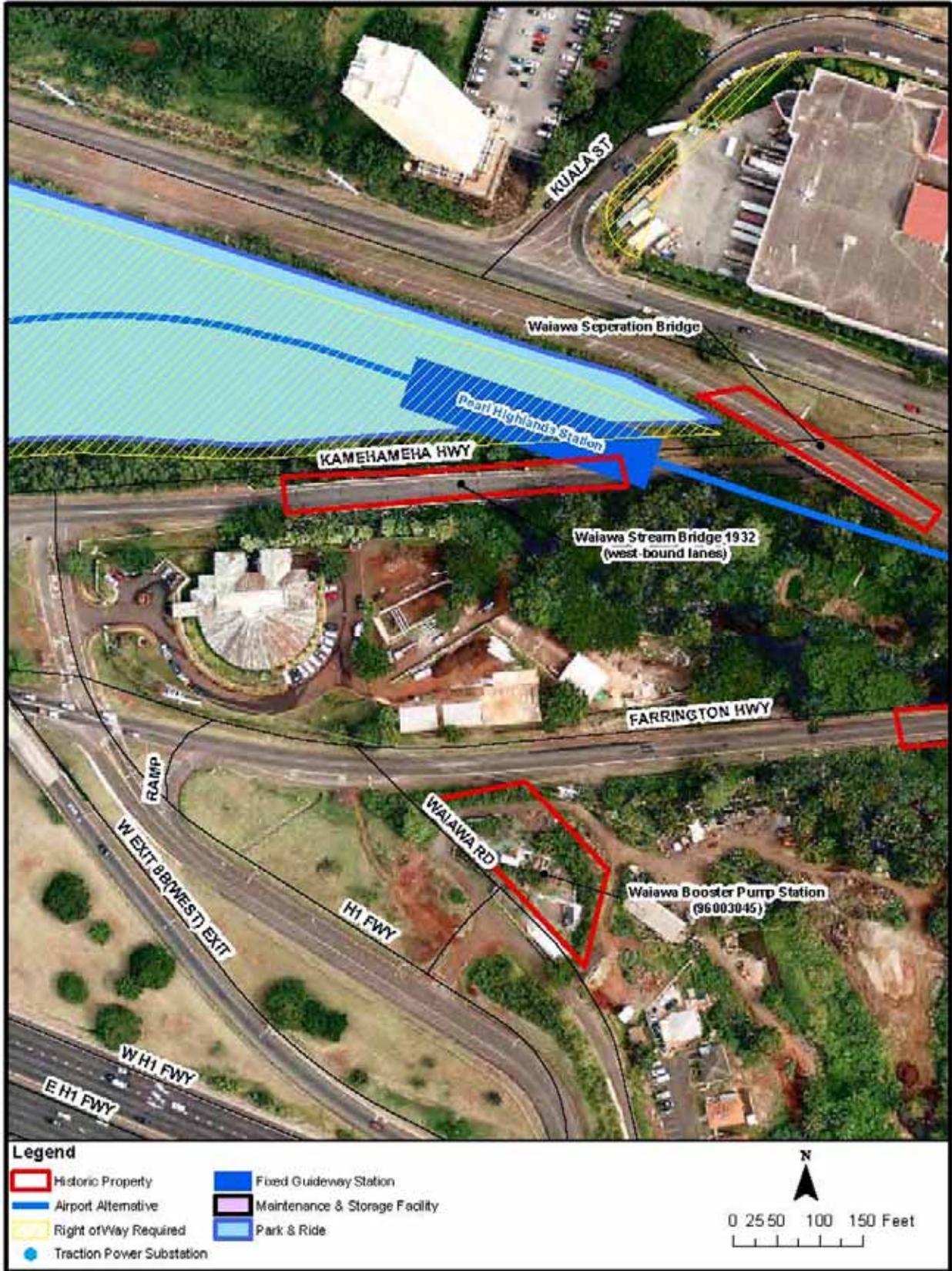


Figure 30. Waiawa Stream Bridge 1932 (westbound lanes)



Figure 31. Waiawa Stream Bridge 1932 (westbound lanes), facing mauka/Koko Head from along Farrington Highway to bridge



Figure 32. Waiawa Stream Bridge 1932 (westbound lanes), facing mauka/Koko Head from bridge along Farrington Highway

5.11 Waiawa Separation Bridge

12 Pearl Highlands Station sector—TMK not applicable
Kamehameha Highway eastbound over Farrington Highway's westbound lanes

Property description

The Waiawa Separation Bridge is a concrete bridge constructed in 1953. The property was determined eligible for the NRHP under Criterion A for its association with the transportation history of the area and under Criterion C as a good example of concrete bridge engineering and design in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure approximately adjacent to and up to 75 feet makai of the bridge and a transit station platform and park-and-ride facility approximately 50 to 150 feet 'Ewa of the bridge (see Figure 33 and Drawing HP009 in Appendix A). The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 65 feet above grade and the bridge. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-5 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Waiawa Separation Bridge.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. The property's setting is diminished by a large, non-historic, multi-story commercial development located mauka of the bridge and by additional non-historic commercial development within the bridge's Koko Head viewshed. This development located within the property's setting and viewshed do not contribute to the historic significance of the bridge. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure approximately adjacent to and up to 75 feet makai of the bridge and a passenger station and park-and-ride facility approximately 50 to 150 feet 'Ewa of the bridge. The guideway would be in close proximity to the

bridge, and the additional structures and facilities would be visible 'Ewa of the bridge.

Although the guideway's introduction and the additional facilities would alter the property's setting and viewshed, the presence of these structures would not alter any historically significant views or visual features. The historic property is an elevated transportation facility designed to cross another transportation facility; additional transportation facilities and bridges are located within the property's setting. Installation of another elevated transportation facility near the property and within its viewshed would not significantly alter the property's setting. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century roadway bridge. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century roadway bridge. Therefore, the Project would have no effect to the property's integrity of association.

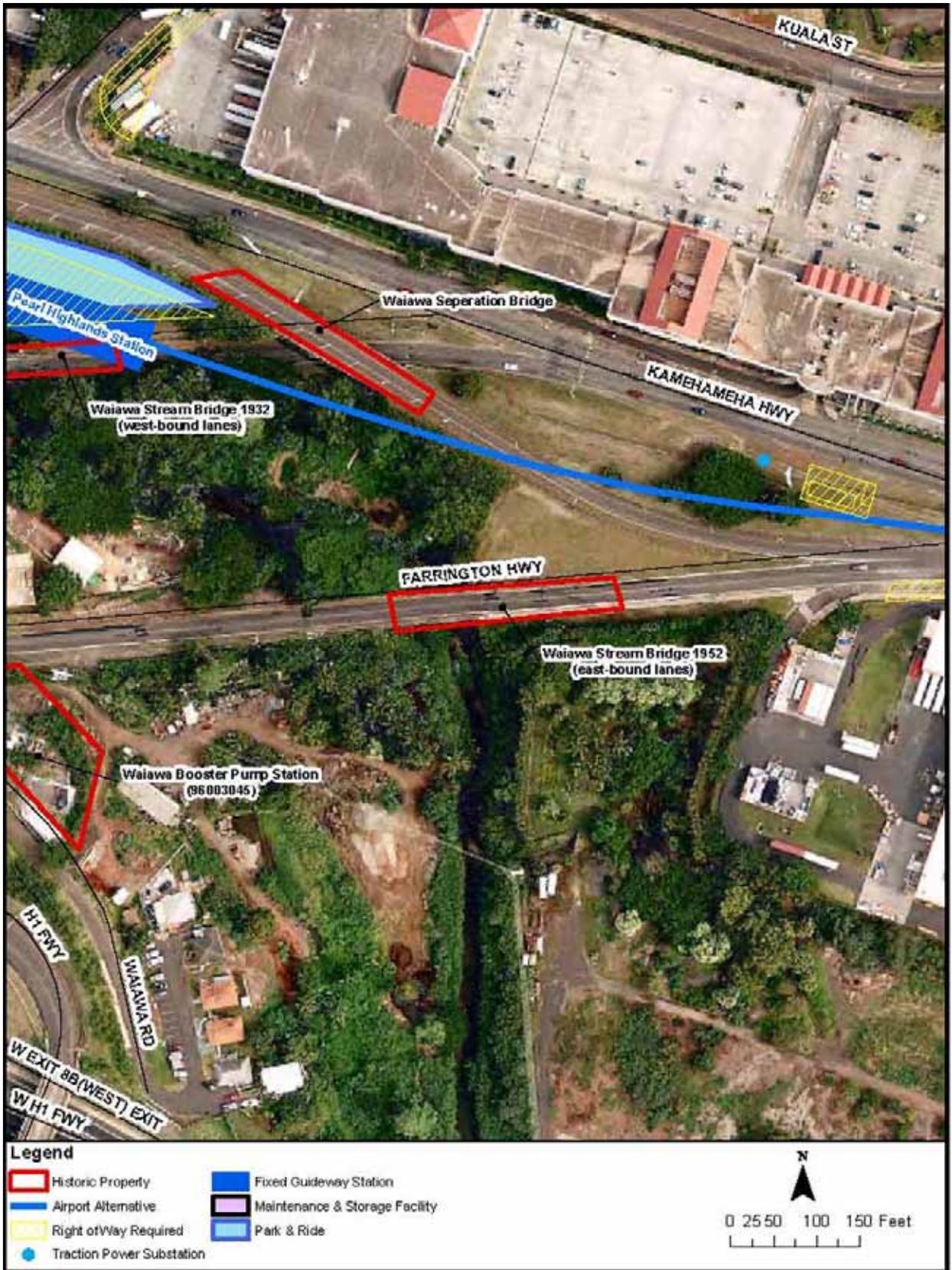


Figure 33. Waiawa Separation Bridge



Figure 34. Waiawa Separation Bridge, facing makai/'Ewa from Kamehameha Highway to bridge



Figure 35. Waiawa Separation Bridge, facing makai/'Ewa from shopping center to bridge

5.12 Waiawa Stream Bridge 1952 (eastbound lanes)

12 Pearl Highlands Station sector—TMK not applicable
Farrington Highway eastbound over Waiawa Stream

Property description

The Waiawa Stream Bridge 1952 (eastbound lanes) is a concrete bridge constructed in 1952. The property was determined eligible for the NRHP under Criterion A for its association with the transportation history of the area and under Criterion C as a good example of concrete bridge engineering and design in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure approximately 100 to 160 feet mauka of the bridge (see Figure 36 and Drawing HP009 in Appendix A). The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 40 to 50 feet above grade and the bridge.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Waiawa Stream Bridge 1952 (eastbound lanes).

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. The property's setting is diminished by large-scale, non-historic commercial development located mauka and Koko Head of the bridge. This development located within the property's setting and viewshed do not contribute to the historic significance of the bridge. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure approximately 100 to 160 feet mauka of the bridge. The guideway would be visible mauka and Koko Head of the bridge. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of these structures and facilities would not alter any historically significant views or visual features. In addition, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century roadway bridge. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century roadway bridge. Therefore, the Project would have no effect to the property's integrity of association.

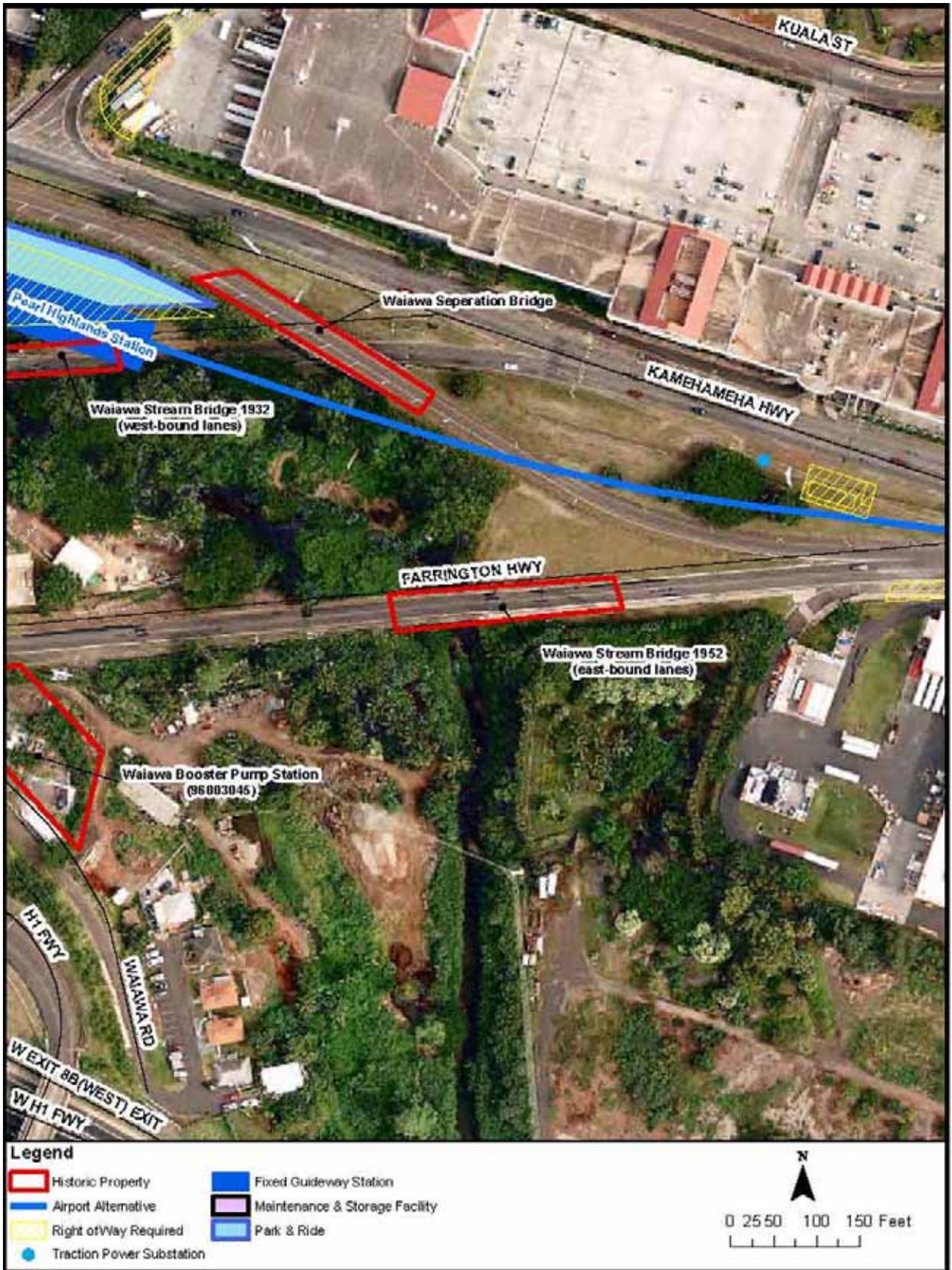


Figure 36. Waiawa Stream Bridge 1952 (eastbound lanes)



Figure 37. Waiawa Stream Bridge 1952 (eastbound lanes), facing mauka/Koko Head from along Farrington Highway to bridge



Figure 38. Waiawa Stream Bridge 1952 (eastbound lanes), facing mauka/Koko Head from bridge along Farrington Highway

5.13 Waiawa Booster Pump Station

12 Pearl Highlands Station sector—TMK 96003045

Waiawa Road at Waiawa Drive (near Farrington Highway)

Property description

The Waiawa Booster Pump Station is a defunct, underground pumping station site once used to move fuel between the military installation at Pearl Harbor and storage facilities at higher elevations at Kīpapa and Waikakalaua; it was constructed in 1943. The property was determined eligible for the NRHP under Criterion A for its association with the built-up of infrastructure at Pearl Harbor and Hickam Field during World War II and under Criterion C as an example of the “military applications of fuel storage and distribution on O‘ahu during World War II and the early Cold War.”

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform approximately 400 feet mauka of the property (see Figure 39 and TMK as shown on Drawing HP009 in Appendix A). The guideway’s design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 65 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Waiawa Booster Pump Station.

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property’s setting was not identified as a historically significant feature of the property. The property is surrounded on all sides by non-historic highway facilities, and views from the property include non-historic commercial and residential development. These features located within the property’s setting and viewshed do not contribute to the historic significance of this property. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure and passenger station approximately 400 feet mauka of the property that would be partially visible from the property. Although the guideway’s construction would introduce a new component to the property’s setting and viewshed, the presence of the guideway would not alter

any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a World War II-era pumping station. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a World War II-era pumping station. Therefore, the Project would have no effect to the property's integrity of association.

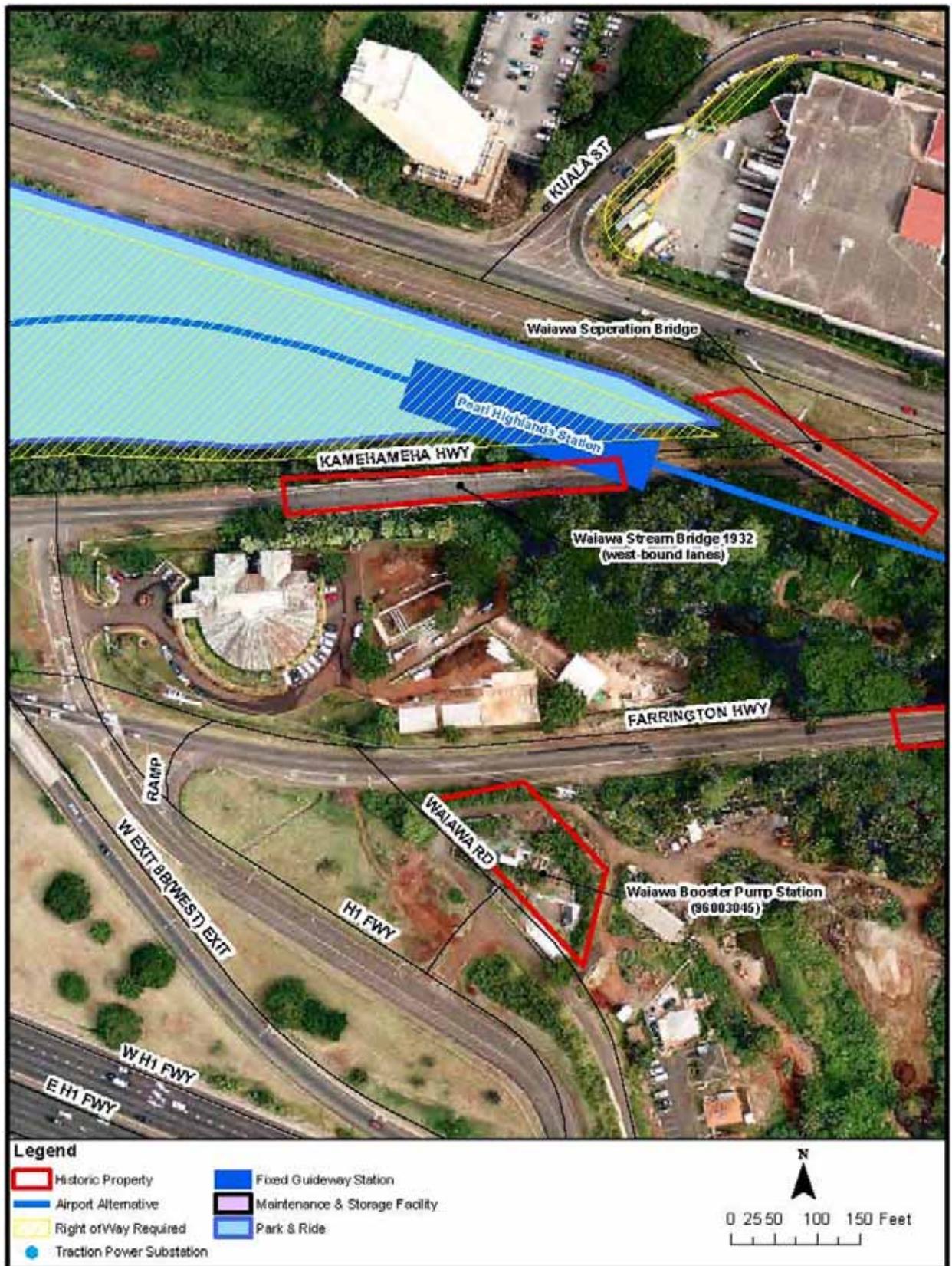


Figure 39. Waiawa Booster Pump Station



Figure 40. Waiawa Booster Pump Station, facing mauka from property to Farrington Highway/Kamehameha Highway



Figure 41. Waiawa Booster Pump Station, facing mauka/Koko Head from property to Farrington Highway/Kamehameha Highway

5.14 HECO Waiau Plant

13 Pearlridge Station sector—TMKs 98003010, 98004003
98-475 Kamehameha Highway

Property description

The HECO Waiau Plant is an electric power generation plant for which construction began in 1938. The property was determined eligible for the NRHP under Criterion A for its association with the history of electric power generation on O‘ahu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway just mauka of the HECO Waiau Plant (see Figure 42 and TMK as shown on Drawing HP010 in Appendix A). The makai edge of the guideway would be approximately 40 feet from the mauka edge of the NRHP boundary. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-7 in Appendix B and would be elevated approximately 40 to 60 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the HECO Waiau Plant.

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property’s setting was not identified as a historically significant feature of the property. The property is adjacent to non-historic commercial, residential, and light industrial development on its mauka and Koko Head boundaries. The H1 Freeway lies immediately ‘Ewa of the property, and Pearl Harbor lies immediately makai of the property. The property’s location adjacent to Pearl Harbor represents its only historically significant feature of its setting or a significant viewshed. Other properties and features within the property’s setting and viewshed do not contribute to its historic significance. The Project would introduce an elevated, fixed guideway rail structure approximately 40 feet mauka of the NRHP boundary.

Although the guideway’s construction would introduce a new component to the property’s setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. The only significant viewshed is from the property makai to Pearl Harbor, and this view would not be affected. The mauka viewshed is not significant and contains non-historic properties and elements,

including tall power transmission poles and lines. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a 20th-century power generation plant. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a 20th-century power generation plant. Therefore, the Project would have no effect to the property's integrity of association.

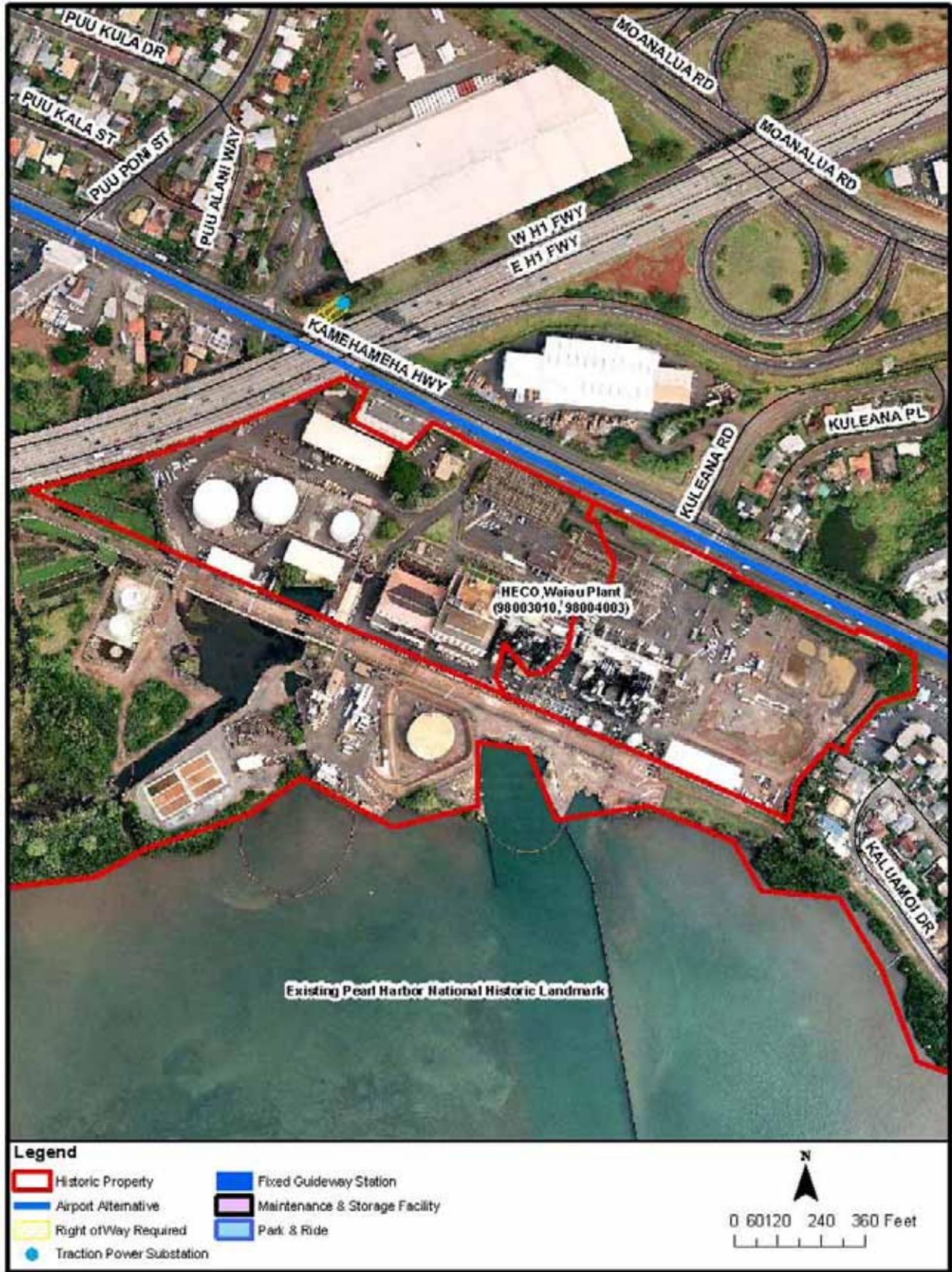


Figure 42. HECO Wai'au Plant



Figure 43. HECO Waiiau Plant, facing Koko Head along Kamehameha Highway to property



Figure 44. HECO Waiiau Plant, facing mauka/‘Ewa from property along Kamehameha Highway

5.15 Nishi Service

13 Pearlridge Station sector—TMK 98006024
98-402 Kamehameha Highway

Property description

The Nishi Service building is a single-story, masonry service station building constructed in 1950. The property was determined eligible for the NRHP under Criterion C as a good example of an automobile service station constructed in Hawai'i during the 1950s.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway makai of the Nishi Service property (see Figure 45 and TMK as shown on Drawing HP010 in Appendix A). The mauka edge of the guideway would be approximately 50 feet from the makai edge of the NRHP boundary and the service canopy attached to the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-7 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Nishi Service.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial properties. Although the highway alignment is present, it has been significantly expanded and altered since construction of the Nishi Service building. Existing elevated structures include tall utility transmission poles that carry multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent highway alignment that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or

atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within its setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century service station. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century service station. Therefore, the Project would have no effect to the property's integrity of association.



Figure 45. Nishi Service



Figure 46. Nishi Service, facing makai/Ewa from property along Kamehameha Highway



Figure 47. Nishi Service, facing makai/Koko Head from property along Kamehameha Highway

5.16 Waimalu Stream Bridge

13 Pearlridge Station sector—TMK not applicable
Kamehameha Highway over Waimalu Stream

Property description

The Waimalu Stream Bridge is a twin-span concrete bridge constructed in 1936 and modified in 1945 and 1966. The property was determined eligible for the NRHP under Criterion A for its association with the roadway infrastructure of Kamehameha Highway in the Pearl City and 'Aiea area and subsequent development.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the median of the Waimalu Stream Bridge (see Figure 48 and Drawing HP011 in Appendix A). The guideway would span the length of the bridge; no columns would be constructed within the median of the bridge spans. However, the guideway would overhang portions of each interior lane within each bridge span. The guideway's design in this area would correspond to the typical section illustrated as Figure B-8 in Appendix B and would be elevated approximately 30 feet above grade and the bridge.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Waimalu Stream Bridge.

Location: No Effect. The Project would not cause the property's removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be removed or physically altered. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. Guideway's construction would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. Within the NRHP boundary, the Waimalu Stream Bridge still spans that waterway. The bridge is surrounded by non-historic commercial, light industrial, and residential development. A park is located across Kamehameha Highway 'Ewa of the bridge. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an

elevated, fixed-guideway rail structure within the median of the two bridge spans and over the interior lanes of each span.

Although setting was not identified as a historically significant feature or characteristic of that contributes to the NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction within the median of the bridge spans and over portions of each span would significantly alter the immediate physical and visual setting. The guideway's proximity to the historic bridge would generally obscure the historic appearance of the bridge and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be physically altered. Careful consideration was given to the placement of columns so as to avoid affecting the resource directly. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of feeling and association.



Figure 48. Waimalu Stream Bridge



Figure 49. Waimalu Stream Bridge, facing Koko Head along Kamehameha Highway to property



Figure 50. Waimalu Stream Bridge, facing Koko Head along Kamehameha Highway to property

5.17 Waimalu Shopping Center

13 Pearlridge Station sector—TMKs 98022074, 98022081
Kamehameha Highway at Hekaha Street

Property description

The Waimalu Shopping Center is a two-story shopping center with roof sections composed of parallel barrel arches; the building was constructed in 1963. The property was determined eligible for the NRHP under Criterion A for its association with the early development of strip malls in Hawai'i and under Criterion C because its distinctive arched roof is characteristic of the intent of Hawaiian architects during the period to produce modern buildings, such as those on the mainland. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway makai of the Waimalu Shopping Center (see Figure 51 and TMKs as shown on Drawing HP011 in Appendix A). The mauka edge of the guideway would be approximately 50 feet from the makai edge of the NRHP boundary and approximately 150 feet from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-8 in Appendix B and would be elevated approximately 30 to 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Waimalu Shopping Center.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. The property is surrounded on all sides by non-historic commercial, light industrial, and residential properties. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant

viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent highway alignment that would be visible from the makai portion of the property, including the shopping center's storefronts and parking lot. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-to-late-20th-century shopping center. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-to-late-20th-century shopping center. Therefore, the Project would have no effect to the property's integrity of association.



Figure 51. Waimalu Shopping Center



Figure 52. Waimalu Shopping Center, facing mauka/Koko Head from Kamehameha Highway to property



Figure 53. Waimalu Shopping Center, facing mauka/Koko Head from Kamehameha Highway to property

5.18 Sumida Watercress Farm

13 Pearlridge Station sector—TMKs 98016047, 98016046
98-160 Kamehameha Highway

Property description

The Sumida Watercress Farm is a 10.7-acre agricultural property used to cultivate watercress. The property contains numerous watercress beds that are partitioned by low walls. Kalauao Spring, a natural artesian spring, is channeled through the beds. The property was determined eligible for the NRHP under Criterion A for its association with the history of diversified agriculture in the Pearl City area, as well as under Criterion C for its distinctive method of construction using artesian spring water and as a rural historic landscape “reflecting the occupational activities of wetland agriculture.”

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway makai of the Sumida Watercress Farm (see Figure 54 and TMK as shown on Drawings HP011 and HP012 in Appendix A). The mauka edge of the guideway would be approximately 50 feet from the makai edge of the NRHP boundary. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-8 in Appendix B and would be elevated approximately 35 to 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Sumida Watercress Farm.

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property’s setting was not identified as a historically significant feature of the property. The property is surrounded on all sides by non-historic commercial and light industrial properties, and an urban, divided highway lies immediately makai of the property. Essentially, the remnant of the original farm is ensconced within a shopping mall and restaurant area and its associated parking. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property’s setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were

identified. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the adjacent highway alignment that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a watercress farm. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a watercress farm. Therefore, the Project would have no effect to the property's integrity of association.

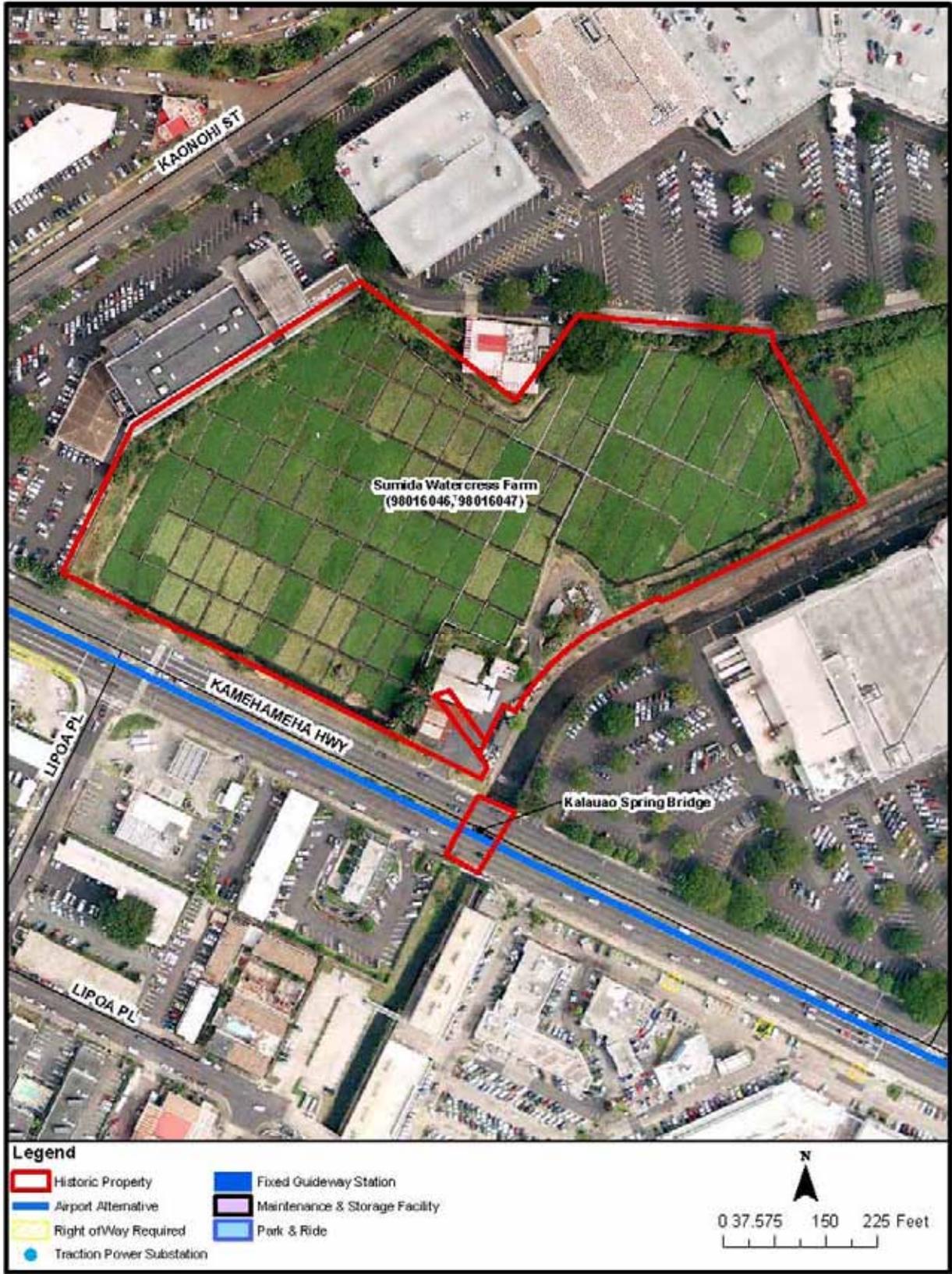


Figure 54. Sumida Watercress Farm



Figure 55. Sumida Watercress Farm, facing mauka from Kamehameha Highway to property



Figure 56. Sumida Watercress Farm, facing Koko Head from just mauka of property

5.19 Kalauao Spring Bridge

13 Pearlridge Station sector—TMK not applicable
Kamehameha Highway over Kalauao Spring

Property description

The Kalauao Spring Bridge is a twin-span concrete bridge constructed in 1936 and modified in 1945 and 1966. The property was determined eligible for the NRHP under Criterion A for its association with the roadway infrastructure of Kamehameha Highway in the Pearl City and 'Aiea area and subsequent development.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the median of the Kalauao Spring Bridge (see Figure 57 and TMK as shown on Drawing HP012 in Appendix A). The guideway would span the length of bridge. No columns would be constructed within the median of the bridge spans. However, the guideway would overhang portions of each interior lane within each bridge span. The guideway's design in this area would correspond to the typical section illustrated as Figure B-8 in Appendix B and would be elevated approximately 30 feet above grade and the bridge.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Kalauao Spring Bridge.

Location: No Effect. The Project would not cause the property's removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be removed or physically altered. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. Guideway's construction would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. Within the NRHP boundary, the Kalauao Spring Bridge still spans that waterway. The bridge is surrounded by non-historic commercial and light industrial development. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an elevated, fixed-guideway rail structure within the median of the two bridge spans and over the interior lanes of each span.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction within the median of the bridge spans and over portions of each span would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the historic bridge would generally obscure the historic appearance of the bridge and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Careful consideration was given to the placement of columns so as to avoid affecting the resource directly. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the bridge. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would obscure the visual relationship between the bridge and its surroundings and thereby obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

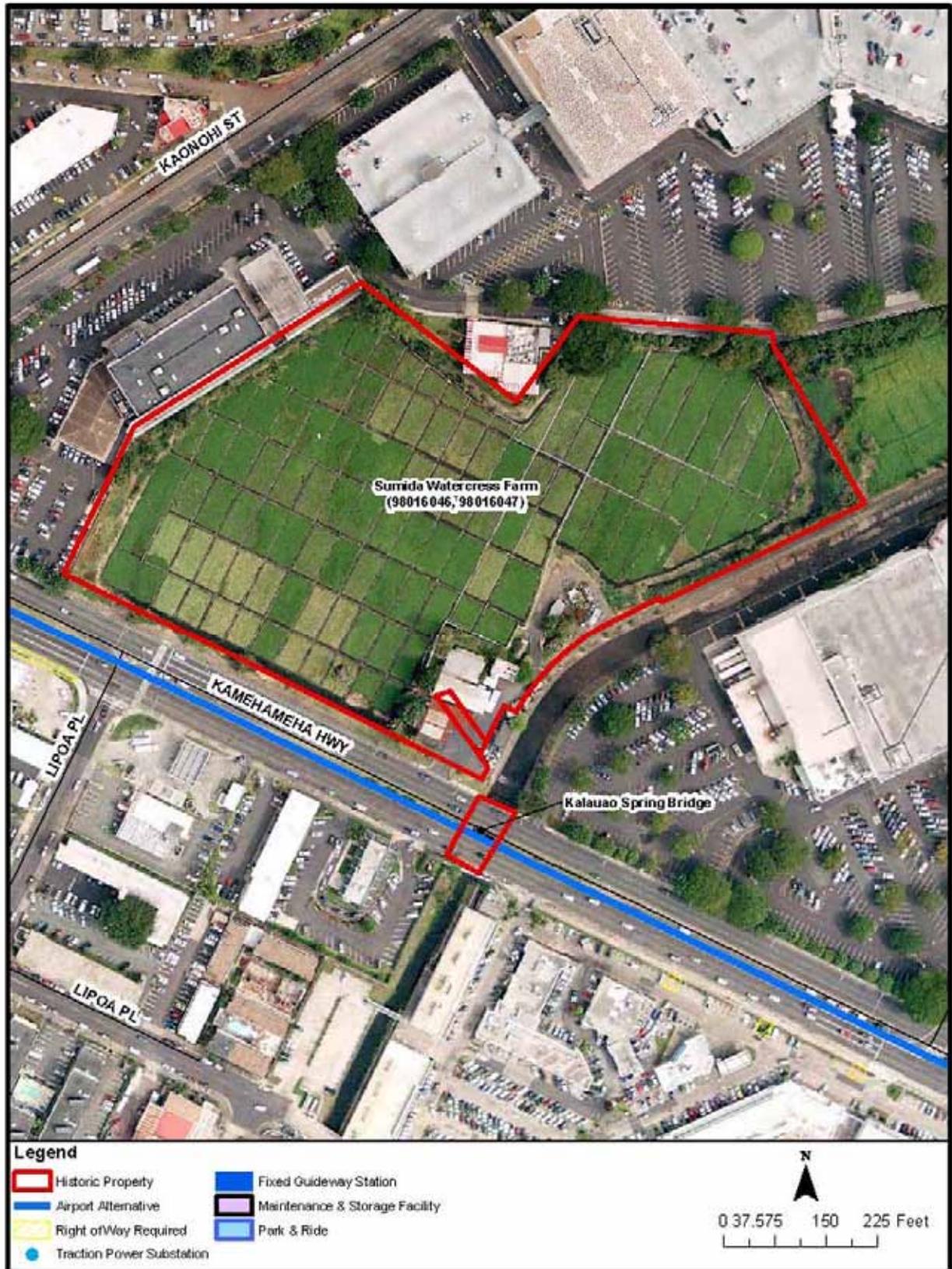


Figure 57. Kalauao Spring Bridge



Figure 58. Kalauao Spring Bridge, facing makai/'Ewa across Kamehameha Highway to property



Figure 59. Kalauao Spring Bridge, facing makai/'Ewa to property from Kamehameha Highway

5.20 Kalauao Stream Bridge

13 Pearlridge Station sector—TMK not applicable
Kamehameha Highway over Kalauao Stream

Property description

The Kalauao Stream Bridge is a twin-span concrete bridge constructed in 1936 and modified in 1945 and 1966. The property was determined eligible for the NRHP under Criterion A for its association with the roadway infrastructure of Kamehameha Highway in the Pearl City and 'Aiea area and subsequent development.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the median of the Kalauao Stream Bridge (see Figure 60 and TMK as shown on Drawing HP012 in Appendix A). The guideway would span the length of bridge. No columns would be constructed within the median of the bridge spans. However, the guideway would overhang portions of each interior lane within each bridge span. The guideway's design in this area would correspond to the typical section illustrated as Figure B-8 in Appendix B and would be elevated approximately 30 feet above grade and the bridge.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Kalauao Stream Bridge.

Location: No Effect. The Project would not cause the property's removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be removed or physically altered. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. Guideway's construction would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. Within the NRHP boundary, the Kalauao Stream Bridge still spans that waterway. The bridge is surrounded by non-historic commercial and residential development. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an elevated, fixed-guideway rail structure within the median of the two bridge spans and over the interior lanes of each span.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction within the median of the bridge spans and over portions of each span would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the historic bridge would generally obscure the historic appearance of the bridge and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the bridge. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of and partially over the bridge spans would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the proximity of the guideway to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

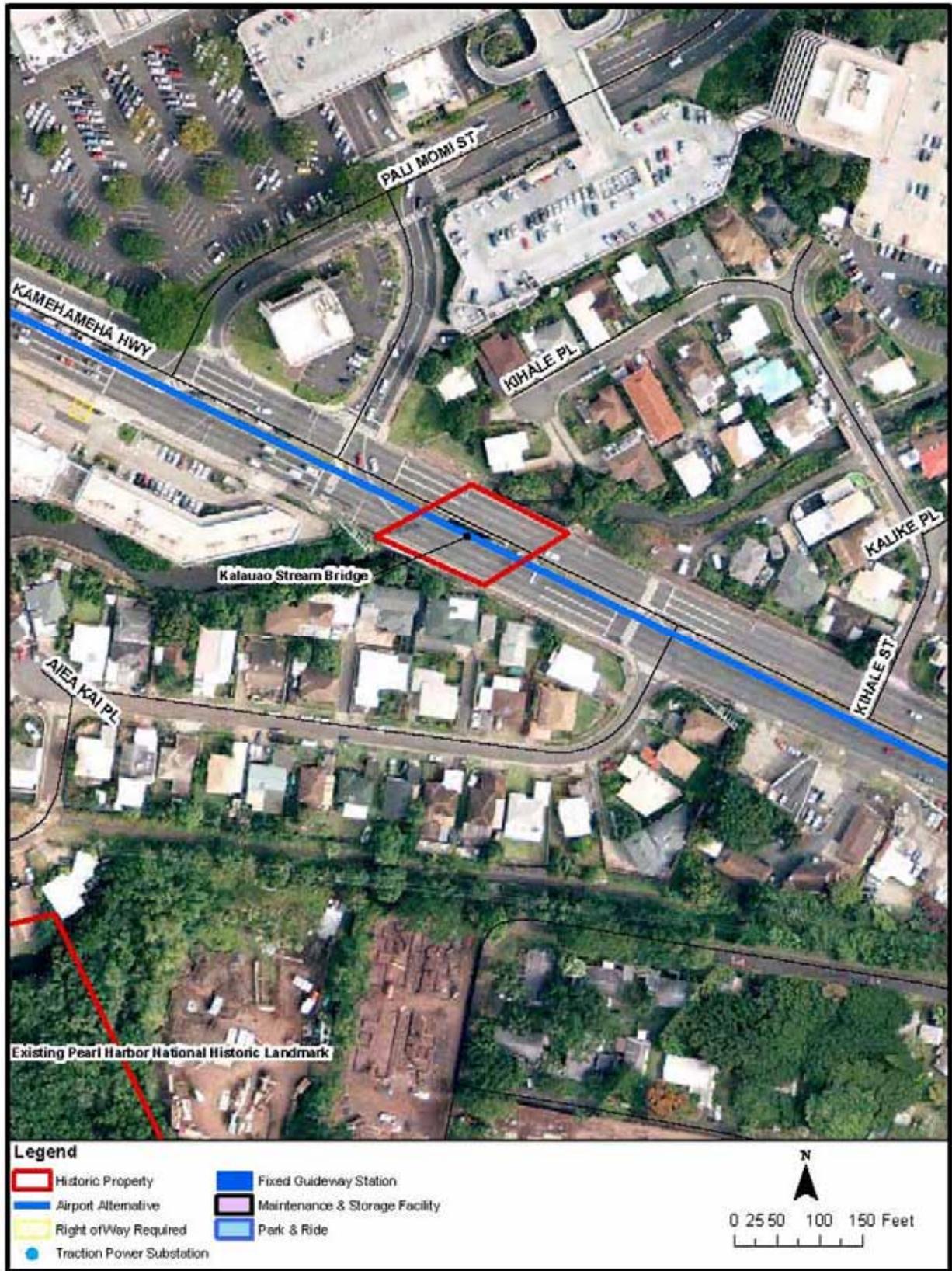


Figure 60. Kalauao Stream Bridge



Figure 61. Kalauao Stream Bridge, facing makai/Koko Head across Kamehameha Highway to property



Figure 62. Kalauao Stream Bridge, facing mauka/'Ewa to property and Kalauao Stream from Kamehameha Highway

5.21 Forty Niner Saimin

13 Pearlridge Station sector—TMK 98018042
98-315 Honomanu Street

Property description

The Forty Niner Saimin building is a flat-roofed, single-story restaurant building constructed in 1947. The property was determined eligible for the NRHP under Criterion A for its association with the early commercialization of saimin, a plantation culinary staple, and under Criterion C as an intact example of an early post-war commercial building 'Ewa of Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway makai of the Forty Niner Saimin building (see Figure 63 and TMK as shown on Drawing HP012 in Appendix A). The makai edge of the guideway would be approximately 100 feet from the mauka edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-7 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Forty Niner Saimin.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial and residential properties. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the highway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the Kamehameha Highway alignment that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or

atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century restaurant building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century restaurant building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 63. Forty Niner Saimin



Figure 64. Forty Niner Saimin, facing makai/'Ewa from property to Kamehameha Highway



Figure 65. Forty Niner Saimin, facing makai from rear of property to Kamehameha Highway

5.22 Akiona House (Quonset)

13 Pearlridge Station sector—TMK 98018041
98-303 Kaamilo Street

Property description

The Akiona House (Quonset) is a corrugated-metal Quonset hut moved to its present location in 1948. The property was determined eligible for the NRHP under Criterion C as a rare surviving example of a World War II Quonset hut reused for residential purposes at the conclusion of the war.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway makai of the Akiona House (see Figure 66 and TMK as shown on Drawing HP012 in Appendix A). The mauka edge of the guideway would be approximately 100 feet from the makai edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-7 in Appendix B and would be elevated approximately 30 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Effect** to the Akiona House (Quonset).

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. (The property has been previously moved, but its significance is directly tied to its relocation and adaptive reuse. The SHPD concurred with the eligibility determination, although the listing of moved properties is relatively rare.) Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Effect. This property's setting was not identified as a historically significant feature of the property. Single-family residential properties are located mauka and 'Ewa of the property, and the historic Forty Nine Saimin restaurant is just Koko Head of the property. A concrete privacy wall screens the property from non-historic commercial properties, and heavily travelled Kamehameha Highway is located makai of the property. The property is purportedly associated with the Forty Niner Saimin restaurant; thus, views from the Akiona House to the Forty Niner Saimin property may be historically significant. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the Kamehameha Highway alignment. The property would be completely visually screened from project construction by the concrete privacy wall immediately makai

of the house; views from the property to the Forty Niner Saimin property would not be altered. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property would be completely visually screened from project construction and thus no historically significant visual features within the property's setting would be altered, the Project would have no effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century Quonset hut. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century Quonset hut. Therefore, the Project would have no effect to the property's integrity of association.



Figure 66. Akiona House (Quonset)



Figure 67. Akiona House (Quonset), facing makai from Kaamilo Street to property



Figure 68. Akiona House (Quonset), facing mauka from adjacent makai commercial property to property

5.23 'Aiea Cemetery/Honolulu Plantation Cemetery

14 Aloha Stadium Station sector—TMKs 99012006, 99012001
Kamehameha Highway at Aiea Access Road

Property description

The 'Aiea Cemetery/Honolulu Plantation Cemetery contains approximately 475 graves of various ethnic groups. Interments date to the late 19th century. The cemetery was decreased in size when Kamehameha Highway was enlarged in the 1940s. The property was determined eligible for the NRHP under Criterion A for its association with the settlement patterns of the 'Aiea area and under Criterion D for the likelihood it may yield information about the lives of Honolulu plantation workers.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway just makai of the 'Aiea Cemetery/Honolulu Plantation Cemetery (see Figure 69 and Drawing HP012 in Appendix A). The mauka edge of the guideway would be approximately 40 to 60 feet from the makai edge of the NRHP boundary. The guideway's design in this area would correspond to the typical section illustrated as Figure B-9 in Appendix B and would be elevated approximately 50 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the 'Aiea Cemetery/Honolulu Plantation Cemetery.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. The property is essentially at the center of the triangular intersection of Kamehameha Highway, Moanalua Freeway (H-201), and Circular Road that surrounds and provides access to the adjacent Aloha Stadium. The property is surrounded on all sides by divided highway alignments and access ramps. Views from the property in all directions are to non-historic properties, including Koko Head to Aloha Stadium and 'Ewa to high-rise buildings and non-historic residential neighborhoods. Views makai to Pearl Harbor are partially obstructed by tall and obtrusive utility transmission poles carrying multiple transmission lines. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or

from this property were identified. The Project would introduce an elevated, fixed guideway rail structure makai of the cemetery and within the center of the adjacent Kamehameha Highway alignment that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a late-19th-century cemetery. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a late-19th-century cemetery. Therefore, the Project would have no effect to the property's integrity of association.

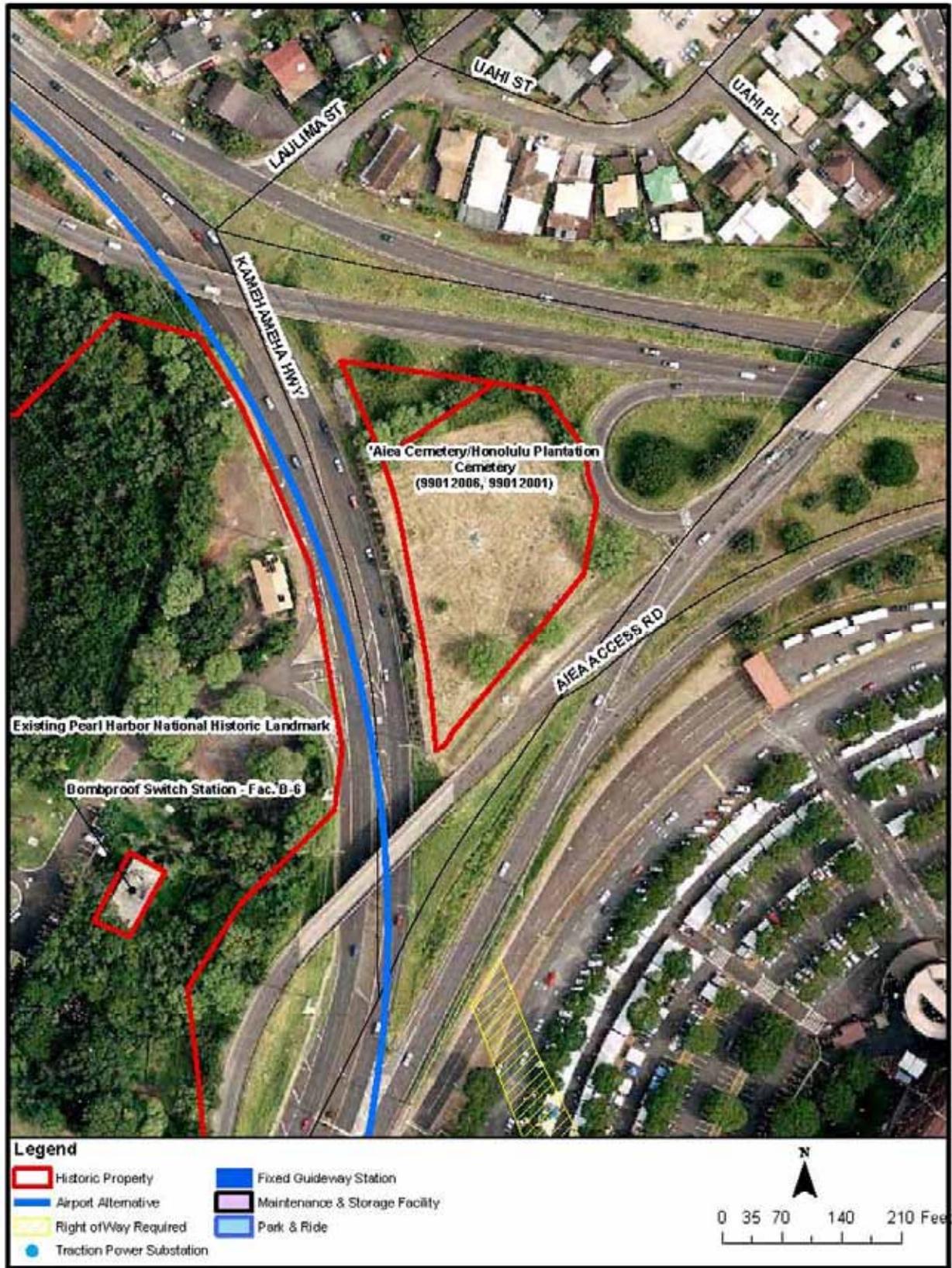


Figure 69. 'Aiea Cemetery/Honolulu Plantation Cemetery



Figure 70. 'Aiea Cemetery/Honolulu Plantation Cemetery, facing makai/'Ewa from property to Kamehameha Highway



Figure 71. 'Aiea Cemetery/Honolulu Plantation Cemetery, facing Koko Head from property to Aloha Stadium



Figure 72. Visual Simulation: View from Aloha Stadium mauka to 'Aiea Cemetery/Honolulu Plantation Cemetery

5.24 Pearl Harbor Introduction

The Pearl Harbor Historic District was listed in the National Register of Historic Places in 1974 (with boundaries accepted in 1978) and designated as a National Historic Landmark (NHL) in 1964. Portions of Pearl Harbor were designated as part of the World War II Valor in the Pacific National Monument in 2008. These designations attest to Pearl Harbor's national significance, which is described in its NHL designation as the site's critical support of the U.S. Navy fleet and establishing the United States as a major power in the Pacific. The NHL nomination specifically states that the national significance of Pearl Harbor stems from its continuing function rather than its physical facilities and those physical changes required to support this mission are "necessary, normal, and expected."

Not all resources within the boundaries of the district are considered contributing resources because they may not retain integrity or they may have been constructed after the district's period of significance. For example, Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society building is within the Pearl Harbor NHL boundaries but is not considered a contributing resource due to its 1975 construction date. Other resources considered part of the Pearl Harbor Naval Base are not within the NHL boundaries (e.g., both the Makalapa and Little Makalapa Housing Districts are not included in the NHL boundaries). The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet National Historic Landmark is individually designated as a NHL. Although the survey form indicates that the property does not retain sufficient integrity to retain NHL status, this determination was made internally. The SHPD did concur with this determination but did not forward the determination to NHL staff. NHL staff has not concurred with this opinion. Because removing NHLs from the national list has not been completed at the state level and SHPD concurrence does not confirm this finding, the CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet National Historic Landmark's status is considered unchanged by the opinions expressed in the recent documentation. In addition, three individually listed NHLs are located within the boundaries of the Pearl Harbor NHL, including the submerged remains of both the *USS Arizona* and *USS Utah* and the decommissioned submarine *USS Bowfin*. However, these historic properties are not located within the APE for the Project. Other contributing resources are located outside of the APE. Conversely, many areas that are within both the APE and NHL boundary are devoid of built resources or significant landscape features.

In addition, although recently evaluated resources could be contributing elements to the Pearl Harbor National Historic Landmark, the SHPD did not forward these findings to NHL staff for evaluation. Section 106 of the National Historic Preservation Act does not recognize potentially contributing NHL status, and contributing status can only be determined by NHL staff and designated by the Secretary of the Interior. Therefore, these resources will be treated as eligible resources only and the existing Pearl Harbor National Historic Landmark will be evaluated for adverse effects. Additionally, National Register of Historic Places criteria were used to evaluate these resources. If they are evaluated for NHL contributing/noncontributing status, NHL

criteria, which vary from NRHP criteria, would need to be used during the evaluations.

As part of the efforts related to the Project, architectural historians identified and evaluated Pearl Harbor resources to determine their eligibility or contributing/noncontributing status using NRHP criteria. This approach was used to fulfill Section 106 obligations and to assist in planning efforts to avoid adverse effects to the Pearl Harbor National Historic Landmark. National Historic Landmarks are afforded specific scrutiny to avoid adverse effects to these nationally significant resources. Although architectural historians could not verify this with absolute certainty, a review of recent Advisory Council on Historic Preservation cases indicates that National Historic Landmarks rarely, if ever, have received adverse effect determinations because project plans are generally altered to avoid such adverse effects. Indeed, when discussing the notion of an adverse effect to a NHL resource with NHL staff, ACHP staff, various SHPOs throughout the country, and multiple preservation professionals during the course of many unrelated projects, no one was able to identify an example of an adverse effect to a NHL.

Architectural historians paid detailed attention to the effects assessments on Pearl Harbor resources that were determined to be eligible, both during the design phase and during the subsequent effects assessments presented in this report. Because of the potential for project impacts and in an effort to identify any potential adverse effects, each resource within the Pearl Harbor NHL that is within the APE was assessed individually for effect determinations. An overall effect determination was also made for the Pearl Harbor National Historic Landmark as a single resource (i.e., a NHL district). Pearl Harbor resources that were either outside of the NHL boundaries, or that were within the boundaries but considered noncontributing, were also evaluated individually.

During the effects assessment phase, an architectural historian visited each eligible and contributing resource within the APE on January 22, 2009. However, at the Navy's request, photography was not permitted for security reasons. Patricia Coleman, Pearl Harbor's National Environmental Policy Act (NEPA) Program Manager, served as escort for this site visit and provided useful information on the historic and current uses of eligible resources, as well as information on changes to buildings and their settings. The Project Team appreciates her assistance.

NHL staff has not reviewed these determinations of eligibility or concurred with the implications of contributing/noncontributing status for resources within the Pearl Harbor National Historic Landmark. Likewise, the Navy has expressed concern that it was not involved in determinations of eligibility on the Navy's resources and that some determinations may possibly conflict with prior consultation and/or programmatic agreements with the SHPD despite SHPD eligibility review. Current and ongoing consultation will resolve any discrepancies and reconcile differences.

5.25 United States Naval Base Pearl Harbor National Historic Landmark

14 Aloha Stadium, 35 Richardson Recreation Center,
and 36 Pearl Harbor Naval Base Station sectors

See National Historic Landmark Documentation for boundary description.

Property description

The United States Naval Base Pearl Harbor (Pearl Harbor) was designated as a National Historic Landmark for its national significance in establishing a strong and strategic military presence in the Pacific in the years leading up to World War II. Since its inception, Pearl Harbor has carried out its mission to support the fleet by sheltering, arming, and repairing naval ships, submarines, and aircraft. On December 7, 1941, the Japanese attacked the base, resulting in the United States entering World War II. The NHL boundaries contain various built resources ranging from buildings directly related to the Navy's wartime work to more modest support buildings.

Project description in vicinity of property

The Project would be located on Kamehameha Highway adjacent to the United States Naval Base Pearl Harbor National Historic Landmark (see Figure 73 and Drawings HP012 through HP015 in Appendix A). The makai edge of the guideway would generally be approximately 25 feet from the mauka edge of the property's NHL boundary. The design of the guideway in this area would generally correspond to the typical sections illustrated as Figures B-9 through B-14 in Appendix B and would be elevated from approximately 30 to 50 feet above grade. The Aloha Stadium Station and the Pearl Harbor Naval Base Station would be adjacent to the NHL. The designs of these stations would correspond to the plans and typical section illustrated as Figures C-1, C-6, and C-7 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Pearl Harbor NHL. Construction of the guideway would not adversely affect the historic landmark's integrity, although the Project would introduce new elements into areas outside of the property's NHL boundary.

Location: No Effect. The Pearl Harbor NHL and its contributing resources retain integrity of location and this would not be altered by the Project. The NHL resources and their related boundaries would remain in their current locations.

Design: No Effect. The Pearl Harbor NHL's contributing resources retain a reasonably high level of integrity of design. As outlined in the NHL documentation, Pearl Harbor is by nature responsive to changes in the base's mission and in military theory and technology. Therefore, some degree of change to historic resources is

essentially inevitable. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NHL boundary.

Setting: No Adverse Effect. The Pearl Harbor NHL is located in a U.S. Navy base that contains built resources from its period of significance during World War II. It also contains numerous resources that were constructed more recently. Changes to the base are inevitable considering the changes in military technology over the past half century. The Project would be visible from select areas of the Pearl Harbor NHL but would not be visible from other portions. Numerous other changes to the setting have not affected the Pearl Harbor NHL's integrity of setting. These changes include the increasingly busy Kamehameha Highway, which has been widened in some areas; the installation of tall power transmission poles and lines that dwarf surrounding structures; and the large, nearby Aloha Stadium, built in 1975. The Project would introduce a new component into the adjacent setting of only a small number of Pearl Harbor's historic resources and would not be visible from the vast majority of the NHL property. Although Kamehameha Highway provides an overland route to the base, Pearl Harbor is ultimately a naval base that is oriented to its harbor. Project construction would occur on its less important land orientation. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and associated stations would introduce new elements into the Pearl Harbor NHL setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Pearl Harbor NHL's contributing resources retain a moderate level of integrity of materials. Changes are due to maintaining the buildings or altering them for new uses. The Project would not alter the current integrity of materials. All Project work would occur outside of the NHL's boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Pearl Harbor NHL's contributing resources retain integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NHL boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Pearl Harbor NHL has a high level of integrity of feeling. It conveys its origins as a significant Pacific naval base, with many World War II-era buildings and structures remaining on the site. The site's military mission remains clear, due in large part to its orientation to the harbor as a naval resource. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would not adversely affect the NHL's integrity of feeling.

Association: No Effect. The Pearl Harbor NHL has a high level of integrity of association. It continues to demonstrate its role in the U.S. Navy's establishment of a strong military presence in the Pacific during World War II. The resources demonstrate the base's self-sufficiency during the war years. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the designated NHL resources and not the surrounding environment.



Figure 73. United States Naval Base Pearl Harbor National Historic Landmark

5.26 Bombproof Switch Station (Facility B-6)

35 Richardson Recreation Center sector—TMK 99003038
Makai of Aloha Stadium

Property description

The Bombproof Switch Station was built in 1942. It is a two-story, reinforced-concrete building with thick walls, square massing, and a flat roof. A bursting chamber on the top of the building was designed to allow bombs to expend their explosive force on the roof or in the chamber so they would not penetrate the shelter below. The structure is eligible under Criterion A for its association with defensive building efforts completed after the Japanese attack on Pearl Harbor.

Project description in vicinity of property

The Project would be located directly mauka of the Bombproof Switch Station on Kamehameha Highway (see Figure 74 and Drawing HP013 in Appendix A). The makai edge of the guideway would be approximately 250 to 300 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-9 in Appendix B and would be elevated approximately 50 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Effect** on the Bombproof Switch Station. Construction of the guideway would not affect the property's integrity, although the Project would introduce new elements into areas outside of the NRHP boundary and out of its viewshed.

Location: No Effect. The Bombproof Switch Station retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Bombproof Switch Station retains a high level of integrity of design. The Project would not alter the integrity of design, and all project-related work would occur outside of the NRHP boundary.

Setting: No Effect. The Bombproof Switch Station is in a recessed area of topography and is surrounded by tall, dense vegetation and substantially set back from Kamehameha Highway. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway would not be visible from the Bombproof Switch Station, there would be no effect to the setting.

Materials: No Effect. The Bombproof Switch Station retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All Project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Bombproof Switch Station retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Bombproof Switch Station has a high level of integrity of feeling. It conveys its origins as a 1940s defensive shelter that was built as a result of the Japanese attack on Pearl Harbor. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character and would have no effect on the station.

Association: No Effect. The Bombproof Switch Station has a high level of integrity of association. It continues to demonstrate its original use on the military installation. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

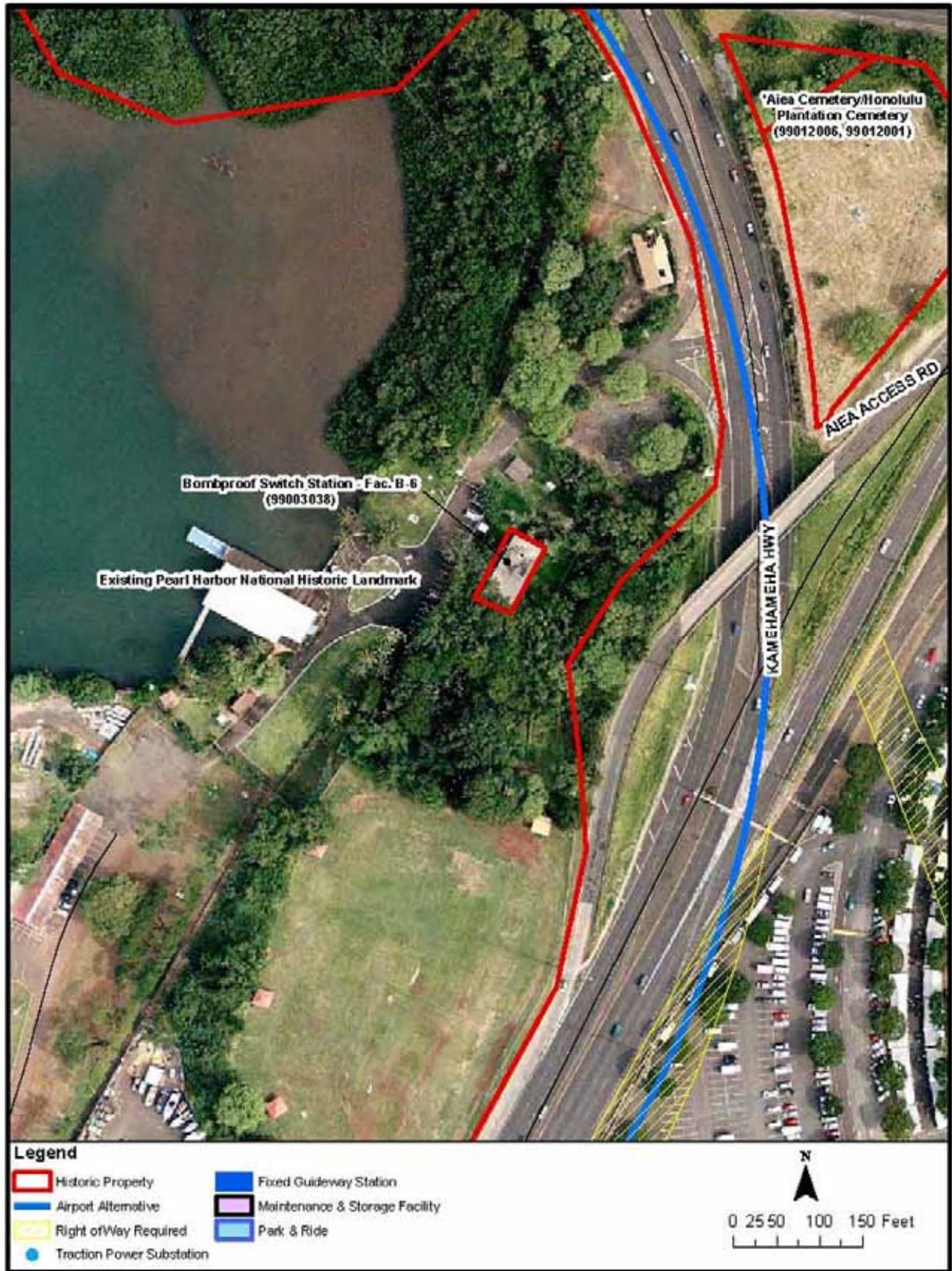


Figure 74. Bombproof Switch Station (Facility B-6)

5.27 Richardson Recreation Center Pool Complex (Swimming Pool—Facility S-21; Recreation Facility—Facility 1; Bath House/Locker Room—Facility 2; Handball Court—Facility S-20)

35 Richardson Recreation Center station sector—TMK 99003029
Kamehameha Highway at Salt Lake Boulevard

Property description

The Richardson Recreation Center Pool Complex, which contains a swimming pool, a two-story recreation facility building, bath house/locker room, and handball court, was built in 1941. The complex offers recreational opportunities for visiting and resident Navy personnel. In recent years, the property has undergone extensive physical changes to its World War II appearance, although it still serves its original recreational function.

Project description in vicinity of property

The Project would be located on Kamehameha Highway, mauka of the Richardson Recreation Center Pool Complex (see Figure 75 and Drawing HP013 in Appendix A). The makai edge of the guideway would be approximately 100 to 150 feet from the mauka edge of the NRHP boundary and the building complex. The Aloha Stadium Station would be approximately 500 feet mauka from the NRHP boundary. The guideway's design in this area would correspond to the typical section illustrated as Figure B-10 in Appendix B and would be elevated approximately 30 to 35 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-6 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Richardson Recreation Center Pool Complex. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Richardson Recreation Center Pool Complex retains integrity of location and this would not be altered by the Project. Select individual components do not retain integrity, but the overall recreational use of the area does retain integrity of location. The current facilities would remain in their current location.

Design: No Effect. The Richardson Recreation Center Pool Complex retains a low level of integrity of design as a result of changes, such as enclosure of the clubhouse's second floor and the removal of tennis courts, baseball and softball diamonds, bleachers, and restrooms, and the addition of the bridge to Ford Island in

1999. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The Richardson Recreation Center Pool Complex is located in an area of low topography that would block some views to the Project. The Project would be visible from select areas of the complex, but would not be visible from other portions because of these changes in topography and vegetation. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility under Criterion A. In fact, it discusses numerous substantial changes to the setting within the proposed NRHP boundary. Furthermore, no audible or atmospheric effects to this property were identified. Although some outdoor resources within the APE may be affected by the Project, those that have already been altered internally and/or are near busy roadways with screening elements would not be adversely affected. Because the guideway and station would introduce a new element into the complex's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Richardson Recreation Center Pool Complex retains a low level of integrity of materials because of the numerous changes discussed above. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Richardson Recreation Center Pool Complex retains a low level of integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Richardson Recreation Center Pool Complex has a moderate level of integrity of feeling. It conveys its origins as a World War II-era recreational facility. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would introduce a new component into the adjacent setting. However, numerous other changes to the building's environment have not adversely affected the complex's overall integrity of feeling, and neither would the Project.

Association: No Effect. Despite numerous changes, the Richardson Recreation Center Pool Complex retains integrity of association. It continues to demonstrate its role in the use of leisure time by Pearl Harbor residents and visitors. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

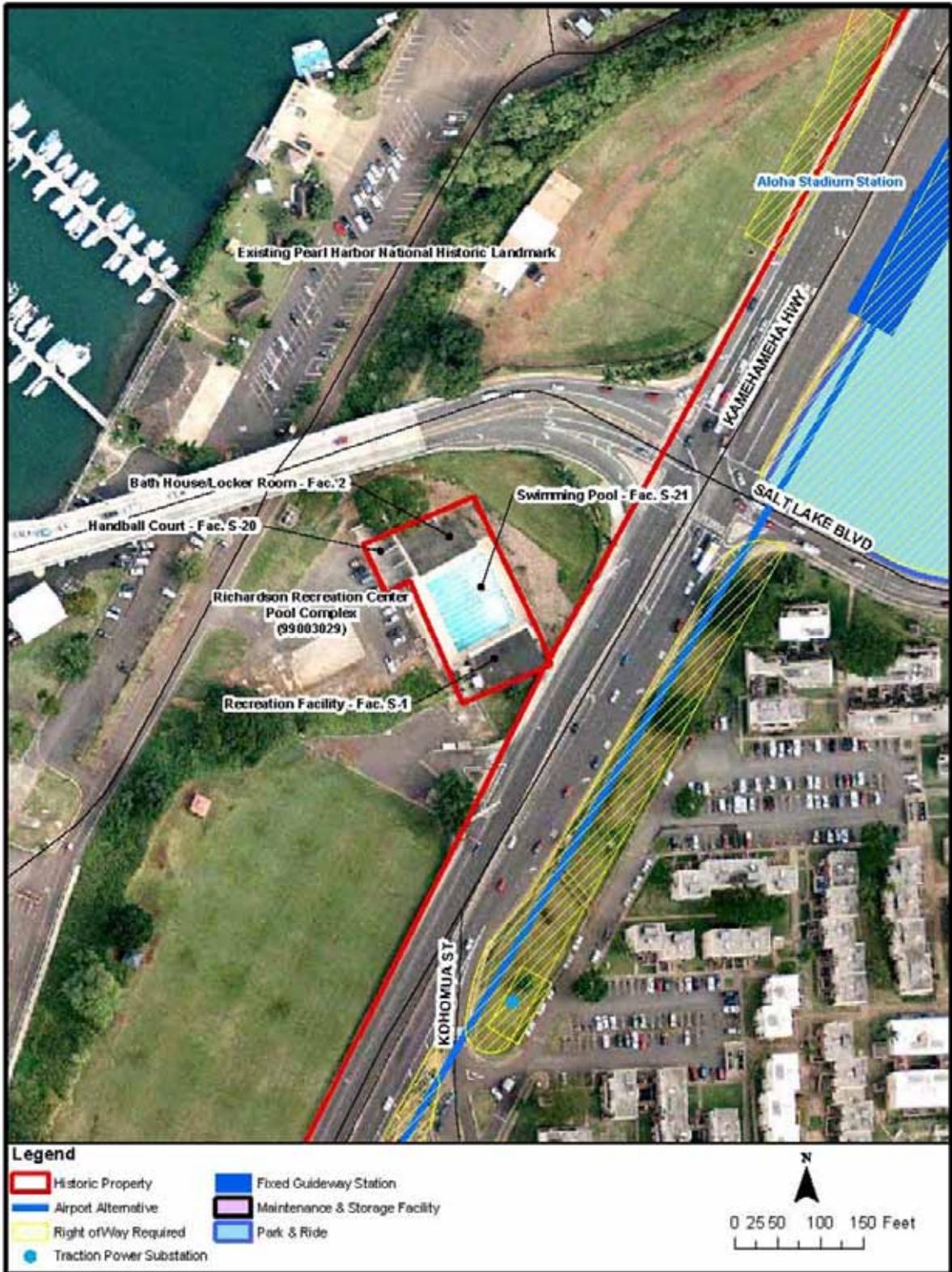


Figure 75. Richardson Recreation Center Pool Complex (Swimming Pool—Facility S-21; Recreation Facility—Facility 1; Bath House/Locker Room—Facility 2; Handball Court—Facility S-20)

5.28 Kamehameha Highway Bridge over Hālawā Stream (mauka span)

35 Richardson Recreation Center Station sector—TMK not applicable
Kamehameha Highway at Hālawā Stream

Property description

The Kamehameha Highway Bridge over Hālawā Stream was built in 1945 and is a concrete deck girder bridge with six spans and a pierced concrete parapet. The end stanchions have a stepped design. The bridge is eligible under Criterion A for its association with World War II-era infrastructure improvements in the vicinity of Pearl Harbor Naval Base. Only the mauka span is being assessed. Although bridges are generally assessed in their entirety, the SHPD has determined that only this span is eligible for the NRHP.

Project description in vicinity of property

The Project would be directly makai of the Kamehameha Highway Bridge over Hālawā Stream (see Figure 76 and Drawing HP013 in Appendix A). The mauka edge of the guideway would be approximately 10 feet from the makai edge of the bridge. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-11 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the mauka span of Kamehameha Highway Bridge over Hālawā Stream. Pre-existing changes to the property have already diminished the bridge's integrity of setting.

Location: No Effect. The mauka span of Kamehameha Highway Bridge over Hālawā Stream retains integrity of location and this would not be altered by the Project. The bridge would remain in its current location.

Design: No Effect. The mauka span of Kamehameha Highway Bridge over Hālawā Stream retains a moderate level of integrity of design on its mauka span. The Project would not alter the remaining integrity of design.

Setting: No Adverse Effect. The mauka span of Kamehameha Highway Bridge over Hālawā Stream is directly adjacent to a dramatically different and incompatible bridge design. Its immediate setting does not retain integrity. Although discussions with the SHPD and consulting parties considered that the bridge could demonstrate the evolution of bridge building on O'ahu, the other portions of the bridge were not evaluated or designated, and the SHPD concurred with a determination that only the mauka span is eligible. Therefore, this is the only portion of the bridge that is considered historic. The recently completed determination of eligibility, with which the SHPD has concurred, states that the mauka span's integrity of setting is "changed by construction of new bridge." Because of these previous changes

directly adjacent to the mauka span, the guideway would have no adverse effect to the already compromised setting. The new bridge is actually closer to the bridge than the new elements would be. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway would introduce a new element into the mauka span's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting because of the previous changes to the bridge's immediate setting by construction of the new bridge.

Materials: No Effect. The mauka span of the Kamehameha Highway Bridge over Hālawā Stream retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The mauka span of the Kamehameha Highway Bridge over Hālawā Stream retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. The mauka span of Kamehameha Highway Bridge over Hālawā Stream does not retain integrity of feeling. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character because the adjacent new bridge has already adversely affected the mauka span. The Project would introduce new components into the setting. However, numerous previous changes to the bridge's setting have not affected the bridge's integrity of feeling, and neither would the Project.

Association: No Adverse Effect. The Kamehameha Highway Bridge over Hālawā Stream does not retain of integrity of association. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is a directly related to the bridge and not the surrounding environment.

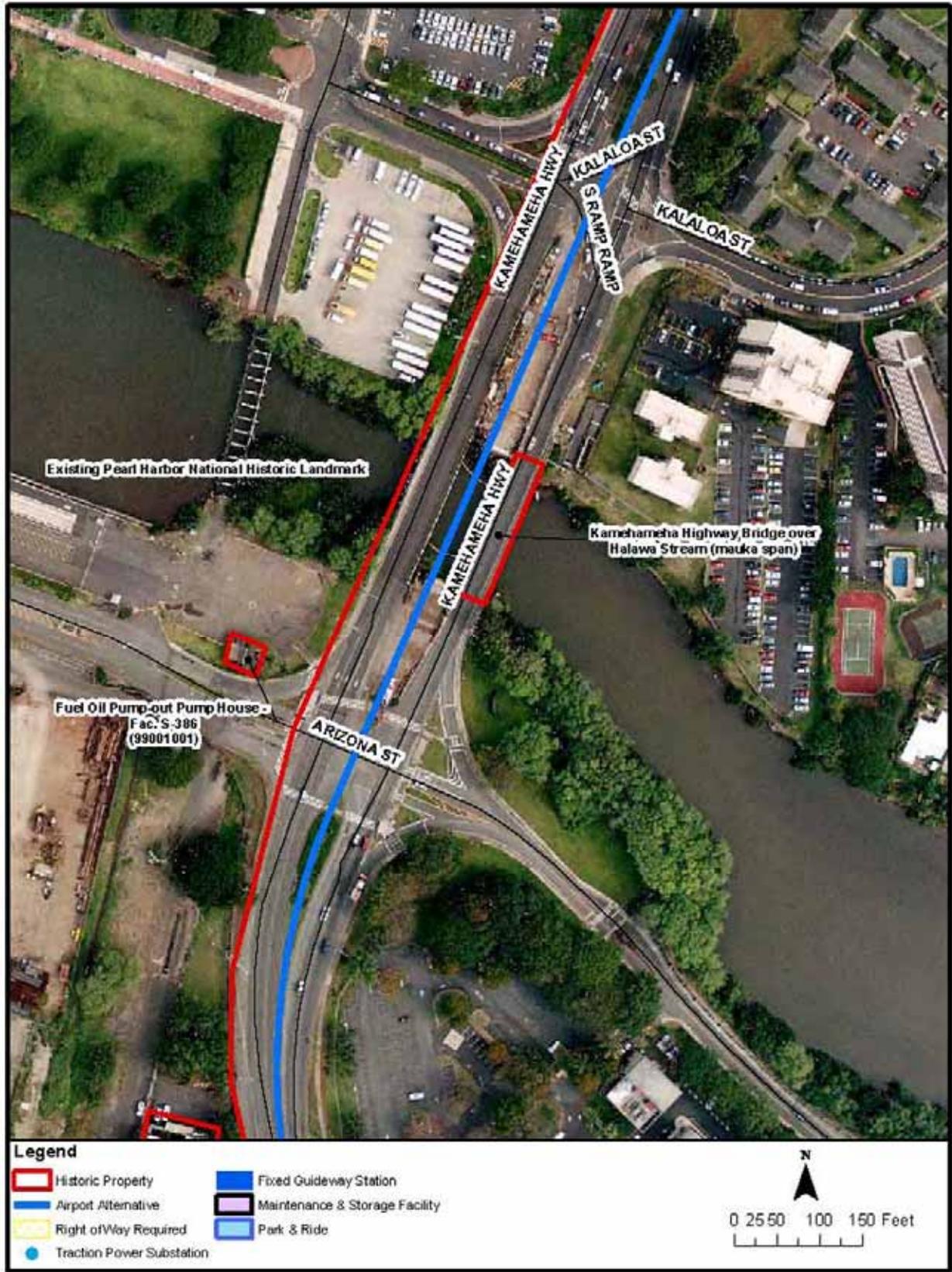


Figure 76. Kamehameha Highway Bridge over Hālawā Stream (mauka span)

5.29 CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet National Historic Landmark; referred to as Commander-in-Chief Pacific Fleet (CINCPACFLT) Headquarters—Facility 250 in technical report

35 Richardson Recreation Center Station sector—TMK 99002004
Hālawa Drive overlooking Kamehameha Highway

Property description

The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet was built in 1942 on Makalapa Hill. Originally constructed of reinforced concrete, a third story was added in 1945. The building is individually listed in the NRHP, although neither the original 1986 NRHP documentation nor the recent determination of eligibility addresses NRHP criteria. The NRHP nomination form states that the building is 300 feet outside of the NHL boundary. However, the building was individually designated as a NHL in 1987. Recent documentation states that the building would no longer be eligible as a NHL because of a lack of integrity. This opinion has not been reviewed by NHL staff and SHPD concurrence with this opinion is not appropriate for NHL status changes, so the building is being reviewed as a NHL for this effects determination.

Project description in vicinity of property

The Project would be makai of the CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet on Kamehameha Highway (see Figure 77 and Drawing HP013 and HP014 in Appendix A). The mauka edge of the guideway would be approximately 900 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-12 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet retains minimal integrity of design as a result of alterations to the building. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet building is located a substantial distance from the Project. A long, winding road flanked by tall and dense vegetation leads from Kamehameha Highway to the building. The Project would be visible from a small portion of the property, but would not be visible from other portions. Tall power transmission poles (which are considerably higher than the guideway would be) and power lines are located along the roadway and are a substantial intrusion that is already present in the building's setting, as is the busy Kamehameha Highway. The recently completed NRHP determination of eligibility update does not discuss the building's setting as an important component relating to its eligibility. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the building's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet retains a moderate level of integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet has a high level of integrity of feeling. It conveys its origins as a 1940s military headquarters building, although it has been altered. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would introduce a new component into the adjacent setting. However, other changes to the building's setting have not affected the building's integrity of feeling and neither would the Project.

Association: No Effect. The CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet also has a high level of integrity of association. It continues to demonstrate its role in Pearl Harbor's management during World War II. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

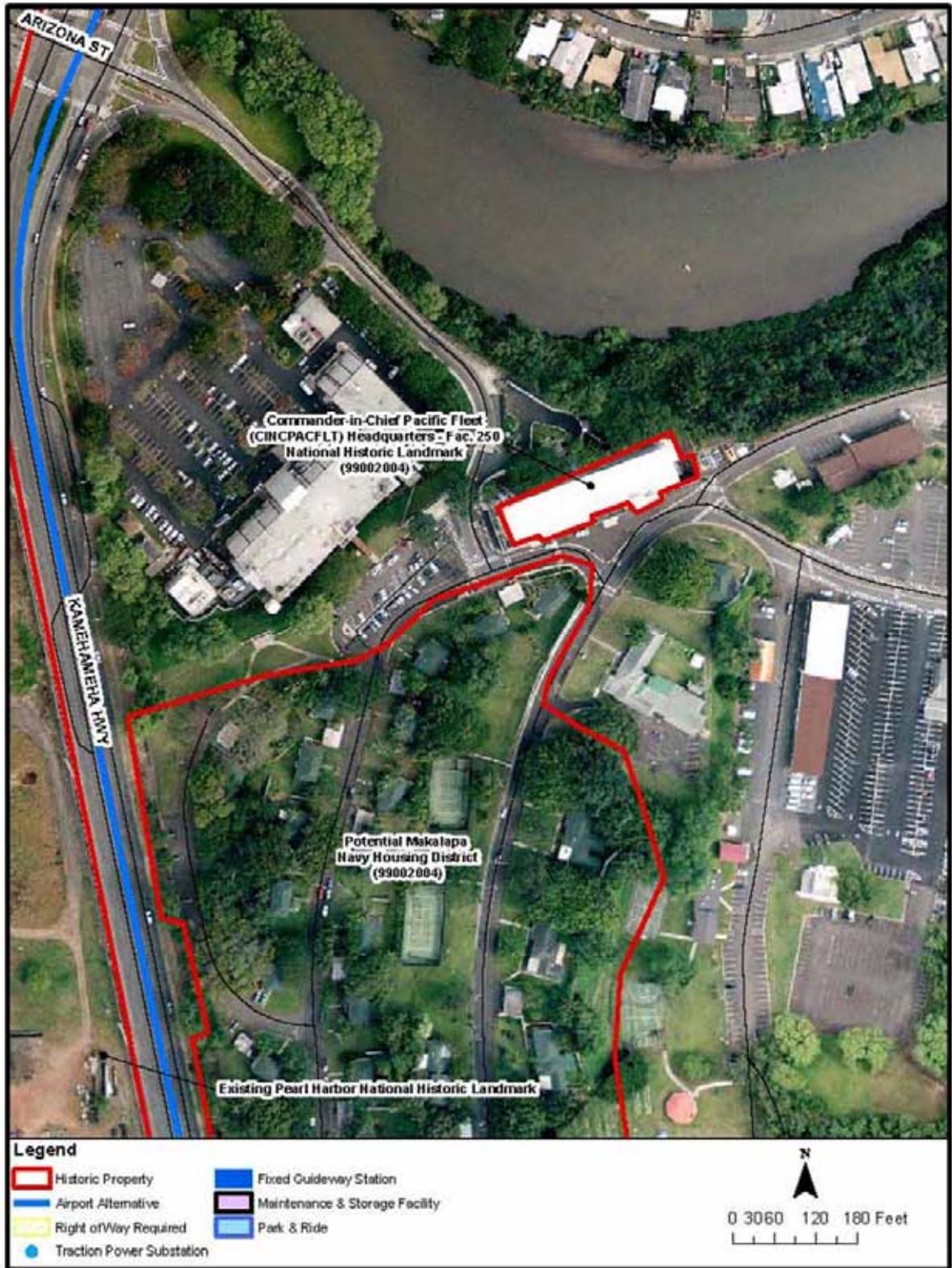


Figure 77. CINCPAC Headquarters/Headquarters, Commander in Chief, Pacific Fleet National Historic Landmark; referred to as Commander-in-Chief Pacific Fleet (CINCPACFLT) Headquarters—Facility 250 in technical report

5.30 Fuel Oil Pump-out Pump House—Facility S-386

35 Richardson Recreation Center Station sector—TMK 99001001
Hālawa Gate at the intersection of Arizona Street and Kamehameha Highway

Property description

The Fuel Oil Pump-out Pump House was built in 1942. It is a small, modest, utilitarian concrete building with a box-like form and flat roof. A tall fence surrounds the building. The recently completed determination of eligibility states that it is eligible under Criterion C for its association with the Red Hill fuel system developed during World War II. This pump house drained pipelines between the fuel pier and the underground receiving pump house. However, although the SHPD concurred on this finding, it appears as if perhaps Criterion A and not C should have been addressed and the original form contains a typographical error. Thus, the effects evaluation below considers impacts to the property's significance under both Criteria A and C. At this time, this will allow for Section 106 adherence without reCOORDINATING with the SHPO for what appears to be a minor typographical error. In either case, the property is eligible and the effects assessment for Criteria A and C results in a No Adverse Effect determination.

Project description in vicinity of property

The Project would be directly mauka of the Fuel Oil Pump-out Pump House (see Figure 78 and Drawing HP013 in Appendix A). The makai edge of the guideway would be approximately 125 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-12 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Fuel Oil Pump-out Pump House. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Fuel Oil Pump-out Pump House retains integrity of location, and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Fuel Oil Pump-out Pump House retains a high level of integrity of design, although this design is modest. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The Fuel Oil Pump-out Pump House's integrity of setting has been compromised according to the recent determination of eligibility form,

which states that it is altered “by highway widening and fencing changes.” The guideway would be visible from select areas of the pump house but would not be visible from other portions because of the surrounding fence. Finally, the setting of this building is not critical to its eligibility. The building is eligible because of its function within the Red Hill fuel system. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway would introduce a new element into the building’s setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Fuel Oil Pump-out Pump House retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All Project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Fuel Oil Pump-out Pump House retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property’s historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of workmanship.

Feeling: No Effect. The Fuel Oil Pump-out Pump House has a high level of integrity of feeling. It conveys its origins as a modest fuel pump house. The Project would not affect any of the property’s physical features or diminish the property’s expression of its historic character. The Project would introduce a new component into the adjacent setting, but would not affect the building’s integrity of feeling.

Association: No Effect. The Fuel Oil Pump-out Pump House has a high level of integrity of association. It continues to demonstrate its role in Pearl Harbor’s fuel pump system. The Project would not affect any of the property’s historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

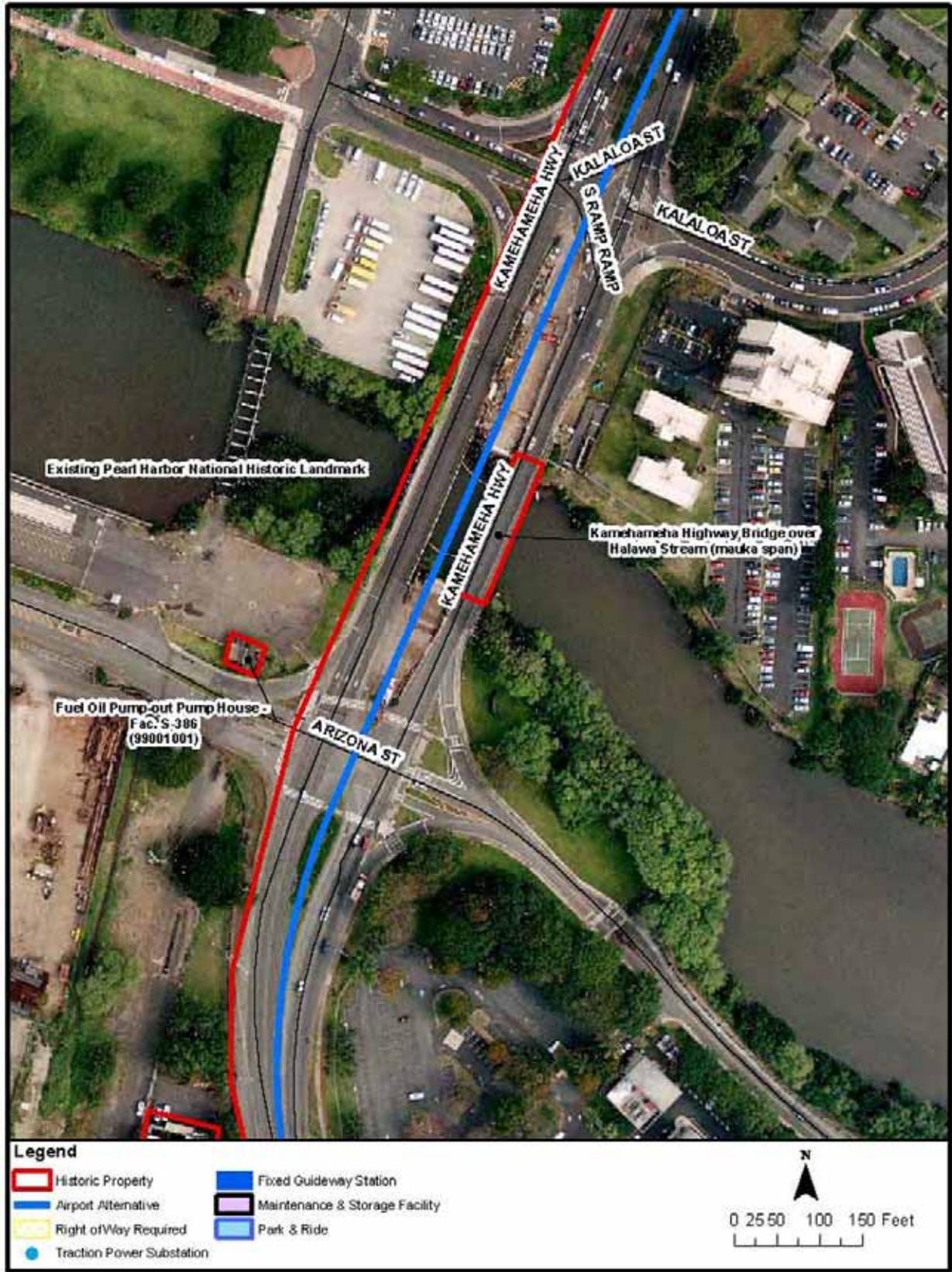


Figure 78. Fuel Oil Pump-out Pump House—Facility S-386

5.31 Publications Printing Office and Plant—Facility 550/District Printing Plant

36 Pearl Harbor Naval Base Station sector—TMK 99001008
Near Hālawā Gate

Property description

The Publications Printing Office and Plant/District Printing Plant was built in 1946 after the end of World War II, although it was designed before that time to accommodate the massive increase in printed registered publications. The building exhibits elements of the International Style of architecture and features cast-in-place concrete walls. A 1949 addition is located on the Koko Head side. The building was determined to be eligible under Criterion A because it “indirectly represents an important aspect of wartime military logistics.”

Project description in vicinity of property

The Project would be located on Kamehameha Highway directly mauka of the Publications Printing Office and Plant/District Printing Plant (see Figure 79 and Drawing HP014 in Appendix A). The makai edge of the guideway would be approximately 75 to 100 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-12 in Appendix B and would be elevated approximately 30 to 35 feet above grade. The building is located in a low topographical area, with Kamehameha Highway and the proposed guideway located substantially above the building’s ridgeline. Vegetation also screens the building from Kamehameha Highway.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Publications Printing Office and Plant/District Printing Plant. Construction of the guideway would not adversely affect the property’s integrity, although the Project would introduce new elements into areas outside the NRHP boundary. The guideway would not be visible from the building because of vegetation cover and large variations in topography between the building and the guideway. Views from the building would be beneath the guideway. Supporting columns would be visible from select areas.

Location: No Effect. The Publications Printing Office and Plant/District Printing Plant retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Publications Printing Office and Plant/District Printing Plant retains a high level of integrity of design. The 1949 addition does not detract from this integrity of design, primarily because it was added soon after initial construction. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

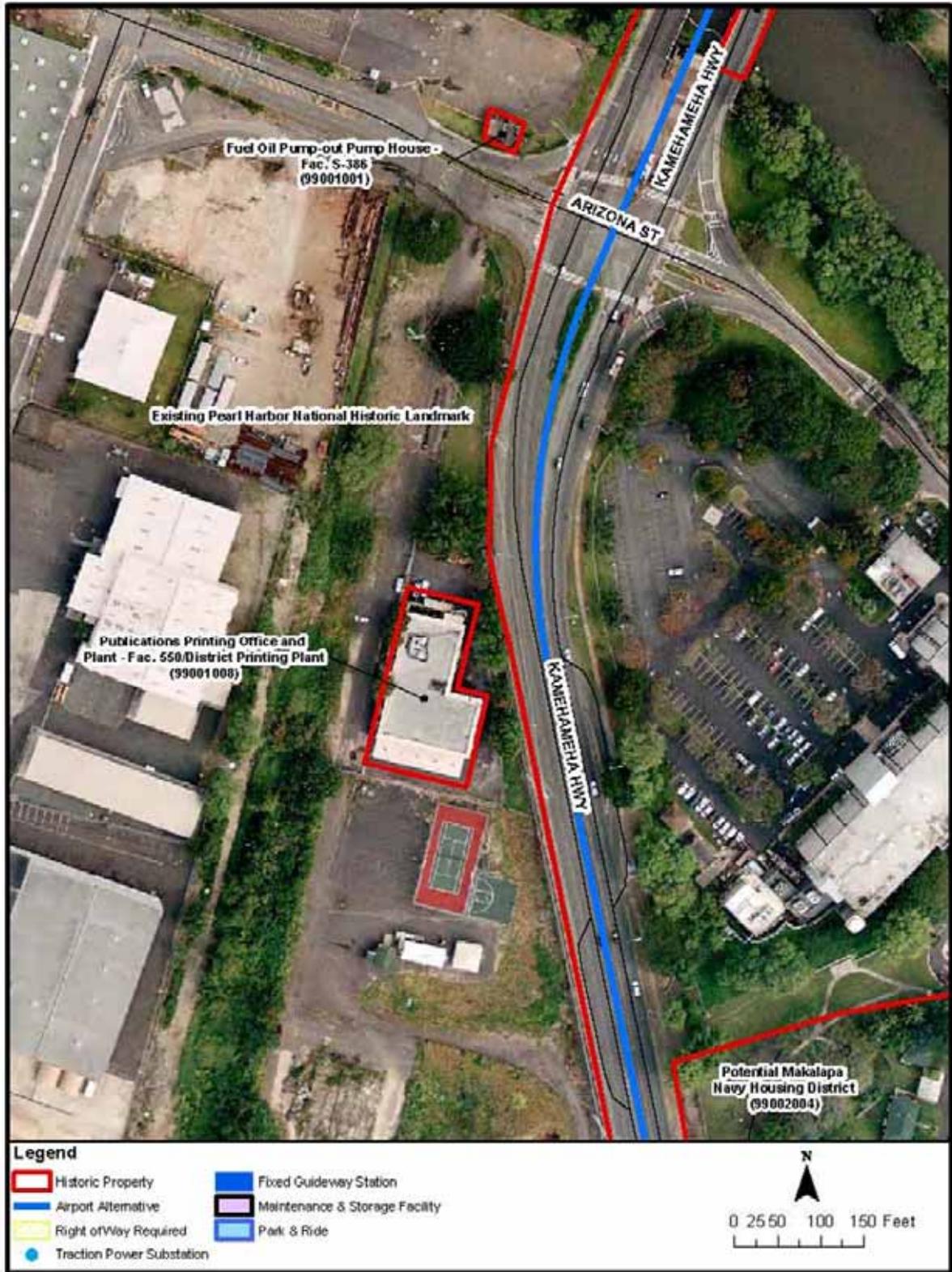
Setting: No Adverse Effect. The Publications Printing Office and Plant/District Printing Plant is in an area of low topography. Views from the building to the project area would be beneath the guideway, although supporting columns may be visible. The setting is not a primary feature of the building, and it does not contribute to its NRHP eligibility. The building's significance is derived solely from its function as a printing plant that was built in the years immediately following World War II. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the building's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Publications Printing Office and Plant/District Printing Plant retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Publications Printing Office and Plant/District Printing Plant retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. The Publications Printing Office and Plant/District Printing Plant has a high level of integrity of feeling. It conveys its origins as a 1940s printing and publication facility. The project would introduce a new component into the adjacent setting. However, this would not adversely affect the building's integrity of feeling.

Association: No Effect. The Publications Printing Office and Plant/District Printing Plant has a high level of integrity of association. It continues to demonstrate its supporting role in Pearl Harbor's post-war mission and is a particularly interesting reminder of the need for massive amounts of printed directives during this time. The Project would not affect any of the property's historically significant physical features. The Project would not alter the building's integrity of association, which is directly related to the building and not the surrounding environment.



**Figure 79. Publications Printing Office and Plant—
Facility 550/District Printing Plant**

5.32 Makalapa Navy Housing

36 Pearl Harbor Naval Base Station sector—TMK 99002004
Kamehameha Highway between Radford and Hālawā Streets

Property description

The Makalapa Navy Housing was built circa 1941 and consists of 14 types of single-family and duplex houses. It is eligible under NRHP Criterion A for its association with the effort to build officers' housing prior to the onset of World War II; under Criterion B for its association with Admiral Chester Nimitz, who lived in the housing for much of World War II; and Criterion C for its association with master architect C.W. Dickey's firm and as an example of military residential housing.

Project description in vicinity of property

The Project would be directly makai of the Makalapa Navy Housing (see Figure 80 and Drawing HP014 in Appendix A). The mauka edge of the guideway would be approximately 10 to 25 feet from the makai edge of the NRHP boundary. The Pearl Harbor Naval Base Station would be located on Kamehameha Highway at the intersection with Radford Drive. The guideway's design in this area would correspond to the typical sections illustrated as Figures B-12 and B-13 in Appendix B and would be elevated approximately 30 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-7 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have an **Adverse Effect** on the Makalapa Navy Housing. Construction of the guideway would adversely affect the property's integrity of setting, although the Project would be built outside the NRHP boundary.

Location: No Effect. The Makalapa Navy Housing retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Makalapa Navy Housing retains a moderate level of integrity of design. The recent determination of eligibility form describes remodeling that has altered the original 14 houses. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: Adverse Effect. The Makalapa Navy Housing's setting would be altered by the guideway. Views from the backyards of residences with views to Kamehameha Highway would be adversely affected by the elevated guideway. No audible or atmospheric effects to this property were identified. Because the guideway would introduce a substantial new element into the Makalapa Navy Housing's setting that is not in keeping with the area's residential appeal, there would be an adverse effect. The station would be located on a tangential point of the NRHP boundary and would not adversely affect the historic property.

Materials: No Effect. The Makalapa Navy Housing retains a moderate level of integrity of materials. Numerous alterations to the houses have occurred. The Project would not alter the current integrity of materials. All project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Makalapa Navy Housing retains moderate integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Makalapa Navy Housing has a moderate level of integrity of feeling. It conveys its origins as a 1940s military housing complex despite changes to the houses. The Project would not affect any of the property's physical features but it would diminish the property's expression of its historic residential character. The Project would introduce a new and incompatible component into the adjacent setting, resulting in an adverse effect.

Association: No Adverse Effect. The Makalapa Navy Housing has a moderate level of integrity of association. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment. Despite the presence of the guideway, the district would still be easily understood as a military housing neighborhood.

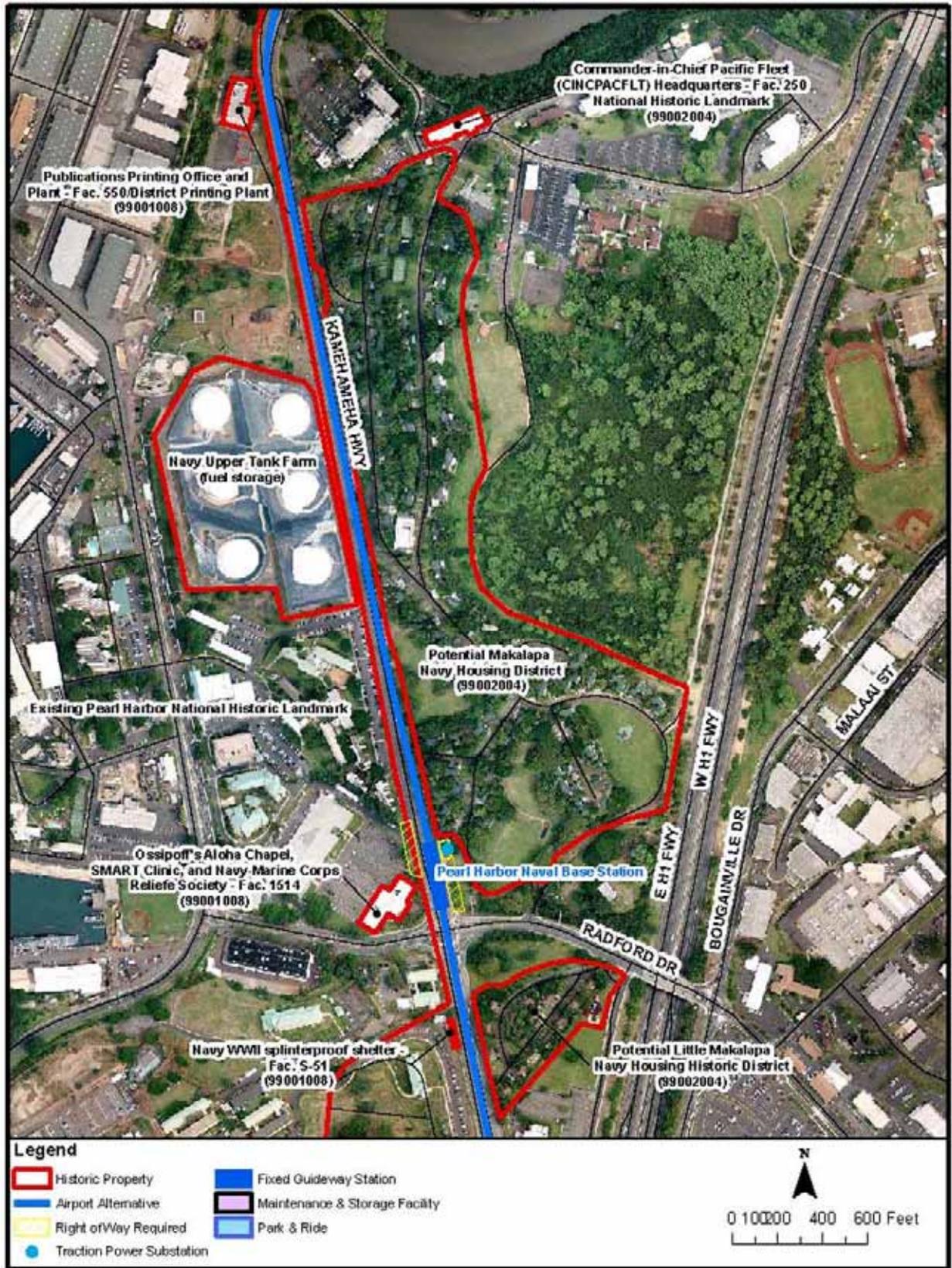


Figure 80. Makalapa Navy Housing



Figure 81. Visual Simulation: Kamehameha Highway near Radford Road and the Pearl Harbor Naval Base Station area, looking 'Ewa, Makalapa Navy Housing on right

5.33 Navy Upper Tank Farm (fuel storage)

36 Pearl Harbor Naval Base Station sector—TMK 99001008
Kamehameha Highway, between Radford Drive and Arizona Street

Property description

The Navy Upper Tank Farm consists of six fuel storage tanks (five of which are original) that were constructed in 1924 to provide fuel for Navy ships. These tanks are 40 feet in height, with diameters of 164 feet and are constructed of riveted steel sheets atop concrete foundations topped by replacement metal roofing. The tanks were determined to be eligible for listing in the NRHP for their association with naval efforts and achieving Pearl Harbor's mission.

Project description in vicinity of property

The Project would be located on Kamehameha Highway, mauka of the Navy Upper Tank Farm (see Figure 82 and Drawing HP014 in Appendix A). The makai edge of the guideway would be approximately 75 to 125 feet from the mauka edge of the NRHP boundary and the tanks. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-12 in Appendix B and would be elevated approximately 30 to 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Navy Upper Tank Farm. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Navy Upper Tank Farm retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. Although the tanks are utilitarian in nature, the Navy Upper Tank Farm retains a high level of integrity of design, which ultimately serves their function. The Project would not alter this integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The Navy Upper Tank Farm's setting is not a critical component of its NRHP eligibility. The resources are utilitarian and their setting does not contribute to their significance. Furthermore, no audible or atmospheric effects to this property were identified. The Project would introduce a new component to the tank farm's setting, but it would not adversely affect the integrity of setting.

Materials: No Effect. The Navy Upper Tank Farm retains a moderate level of integrity of materials. One original tank has been removed and replaced, and the roofs of the others have been replaced. The Project would not alter the current integrity of materials. All Project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Navy Upper Tank Farm displays minimal levels of workmanship because of the functional nature of the resources, but does retain moderate integrity. The Project would not affect any characteristics or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. The Navy Upper Tank Farm has a high level of integrity of feeling. The tank farm conveys its origins as a functional resource that allowed Pearl Harbor to fulfill its mission. The Project would introduce a new component into the adjacent setting but would not alter the industrial and military feelings that the tank farm evokes. Other changes to the surrounding environment have occurred, but the tanks have not lost their integrity of feeling. The Project also would not affect the tank farm's integrity of feeling.

Association: No Effect. The Navy Upper Tank Farm has a high level of integrity of association. It continues to demonstrate its role in the support of naval vessels and in establishing a U.S. military presence in the Pacific. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the structures and not their surrounding environment.

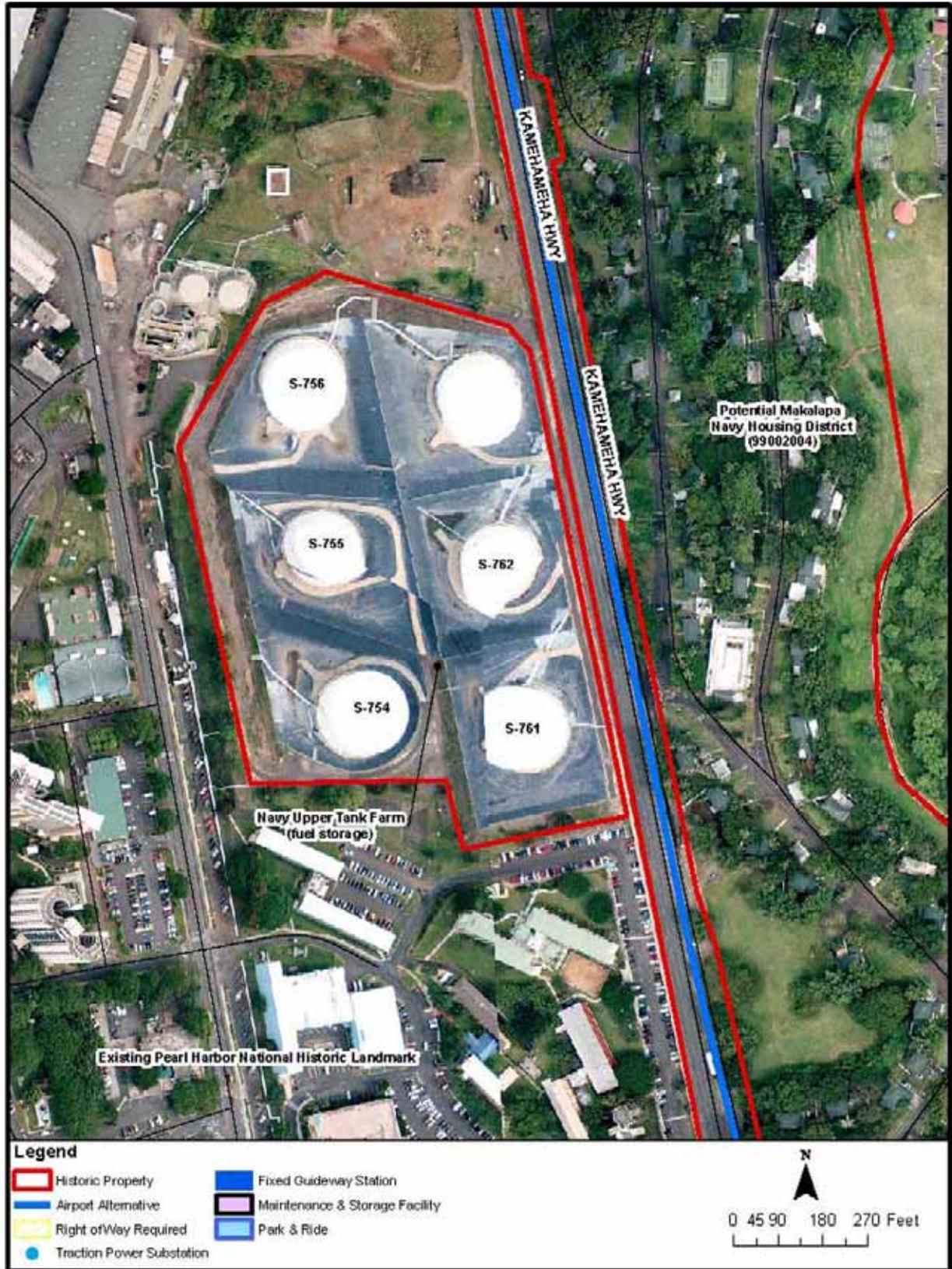


Figure 82. Navy Upper Tank Farm (fuel storage)

5.34 Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society—Facility 1514

36 Pearl Harbor Naval Base Station sector—TMK 99001008
Kamehameha Highway and Radford Drive at Makalapa Gate

Property description

The building that contains the Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society consists of distinct uses, each occurring in discrete areas of the building. Two of the three rectangular, single-story sections of the building are surmounted by barrel vaults, and one has a flat roof. The building is constructed of split-concrete brick and concrete. It was built in 1975 and is an excellent example of architect Vladimir's Ossipoff's Modern architecture. The eligibility assessment only stipulates that the building is eligible under Criteria G as a resource that is less than 50 years of age, but it does not address the four standard NRHP criteria, as is required for an NRHP eligibility determination. Most likely, it would be eligible under Criterion A for its significance as reportedly being the first specifically Jewish house of worship on a military base, and also under Criterion C as a Modern masterpiece by Vladimir Ossipoff. The building is not considered a contributing element to the Pearl Harbor National Historic Landmark because of its recent construction date.

Project description in vicinity of property

The Project would be directly mauka of Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society on Kamehameha Highway (see Figure 83 and Drawing HP014 in Appendix A). The makai edge of the guideway would be approximately 60 feet from the mauka edge of the NRHP boundary and the building. The Pearl Harbor Naval Base Station would be located on Kamehameha Highway at the intersection with Radford Drive. The guideway's design in this area would correspond to the typical section illustrated as Figure B-13 in Appendix B and would be elevated approximately 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-7 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have an **Adverse Effect** on Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society. Construction of the guideway would adversely affect the property's integrity of setting because it substantially alters critical views to the exterior from a significant worship space. The Project would not affect the building or parcel.

Location: No Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society retains a high level of integrity of design, most notably on the exterior.

Interior spaces have been altered to accommodate new uses, but these changes do not detract from the exterior integrity. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: Adverse Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society is adjacent to Kamehameha Highway, where the guideway would be constructed. The building is nestled in an area of low topography, which affords views up to the guideway. The Project would be visible from select areas of the building, but would not be visible from other portions. Overall, the building's setting would not be substantially altered by the Project. The building's location within the topography, coupled with its architectural design, protect much of it from the Project. However, most critically, the Project would be visible from the interior of the chapel. Although the orientation of the chapel is organized so that worshippers' backs would be toward the Project, the fact that the Project would be visible through the semi-circular clerestory windows to those leading the services substantially depletes the integrity of setting and constitutes an adverse effect.

Materials: No Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society has a high level of integrity of feeling. It conveys its origins as a Modern building and exhibits a higher quality of design than many other buildings of its era. The Project would introduce a new component into the adjacent setting but would not affect any of the property's physical features or diminish the property's expression of its historic character.

Association: No Effect. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society has a high level of integrity of association. It continues to demonstrate its role in the construction of Modern architecture on O'ahu. It is a skillful design that attests to Ossipoff's expertise. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

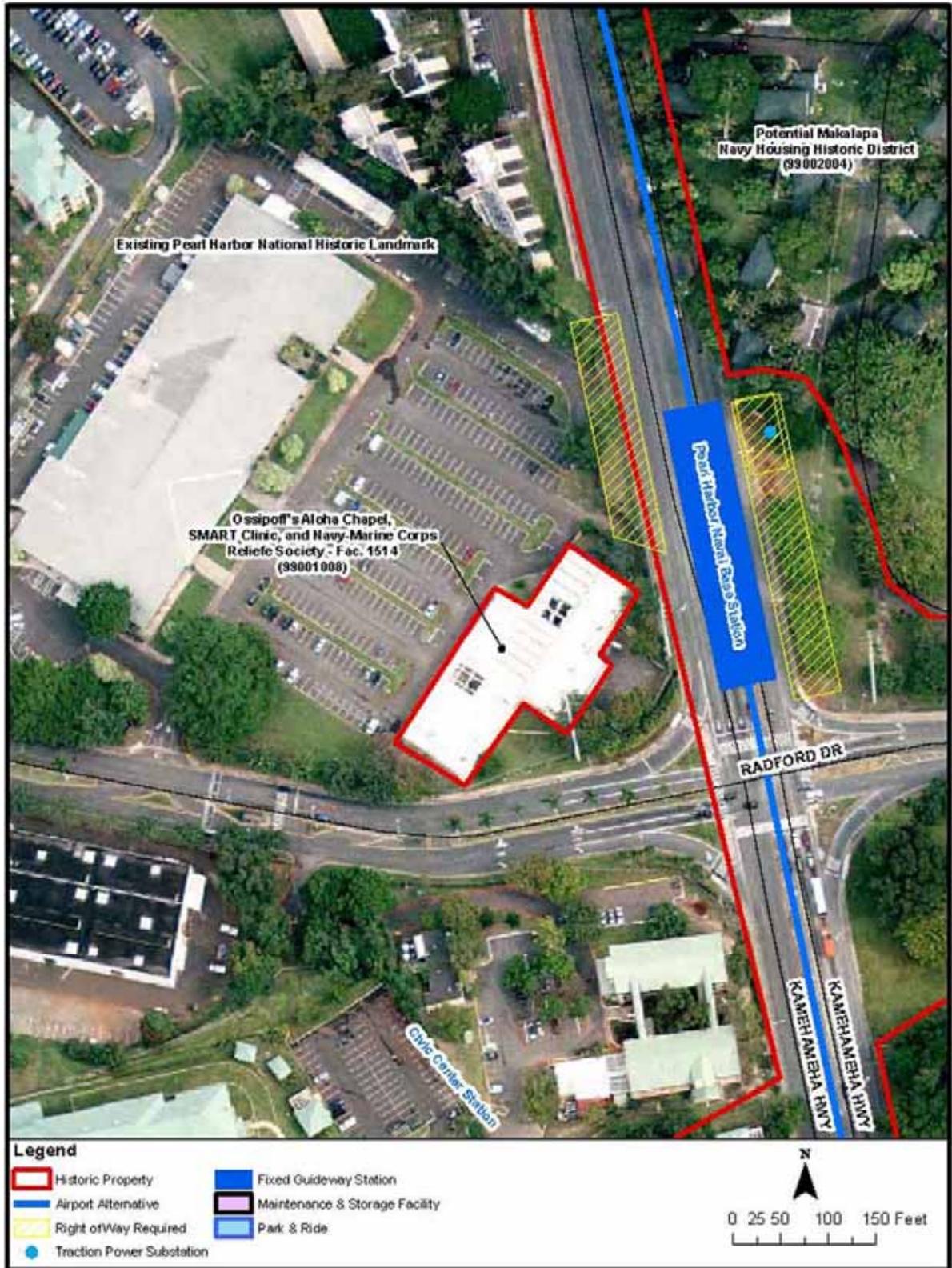


Figure 83. Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society—Facility 1514



Figure 84. Visual Simulation: Kamehameha Highway near Radford Road and the Pearl Harbor Naval Base Station area, looking 'Ewa, Ossipoff's Aloha Chapel visible on left

5.35 Little Makalapa Navy Housing

36 Pearl Harbor Naval Base Station sector—TMK 99002004
Kamehameha Highway/Tarawa Drive/Palmyra Drive

Property description

The buildings that comprise Little Makalapa Navy Housing were built in 1941 by master architect C.W. Dickey. The 15 duplexes, which have prefabricated sections, were determined to be eligible under Criterion A for their association with increased Pearl Harbor civilian housing in the years prior to World War II.

Project description in vicinity of property

The Project would be located on Kamehameha Highway directly 'Ewa of the Little Makalapa Navy Housing (see Figure 85 and Drawing HP014 in Appendix A). The mauka edge of the guideway would be approximately 50 feet from the makai edge of the NRHP boundary. The Pearl Harbor Naval Base Station would be located mauka of Kamehameha Highway's intersection with Radford Drive and would be approximately 450 feet from the NRHP boundary. The guideway's design in this area would correspond to the typical section illustrated as Figure B-14 in Appendix B and would be elevated approximately 30 to 35 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-7 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Little Makalapa Navy Housing. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Little Makalapa Navy Housing retains integrity of location and this would not be altered by the Project. The housing would remain in its current location.

Design: No Effect. The Little Makalapa Navy Housing retains a high level of integrity of design, although some interior changes have occurred. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. Little Makalapa Navy Housing is adjacent to Kamehameha Highway. Within the NRHP boundary, the Project would not be visible from select areas because of considerable distance to the guideway. Houses that are closer to the project alignment would be shielded from the guideway because of an existing tall sound wall that screens residences from the roadway and would also block views to the guideway. The station would be located a considerable distance from the NRHP boundary and would be screened by substantial vegetation from the rear of the closest houses. Furthermore, no audible or atmospheric effects to this

property were identified. Because the guideway and station would introduce a new element into the Little Makalapa Navy Housing's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. Little Makalapa Navy Housing retains a high level of integrity of materials on the exterior. The Project would not alter the current integrity of materials. All project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. Little Makalapa Navy Housing retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. Little Makalapa Navy Housing has a high level of integrity of feeling. It conveys its origins as a 1940s military housing complex. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would introduce a new component into the adjacent setting but it would be screened by an existing wall. Numerous other changes to the building's environment have not affected the Little Makalapa Navy Housing's integrity of feeling, and neither would the Project.

Association: No Effect. Little Makalapa Navy Housing has a high level of integrity of association. It continues to demonstrate the Navy's efforts to increase housing availability, and its link to the greater Pearl Harbor establishment would remain clear. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

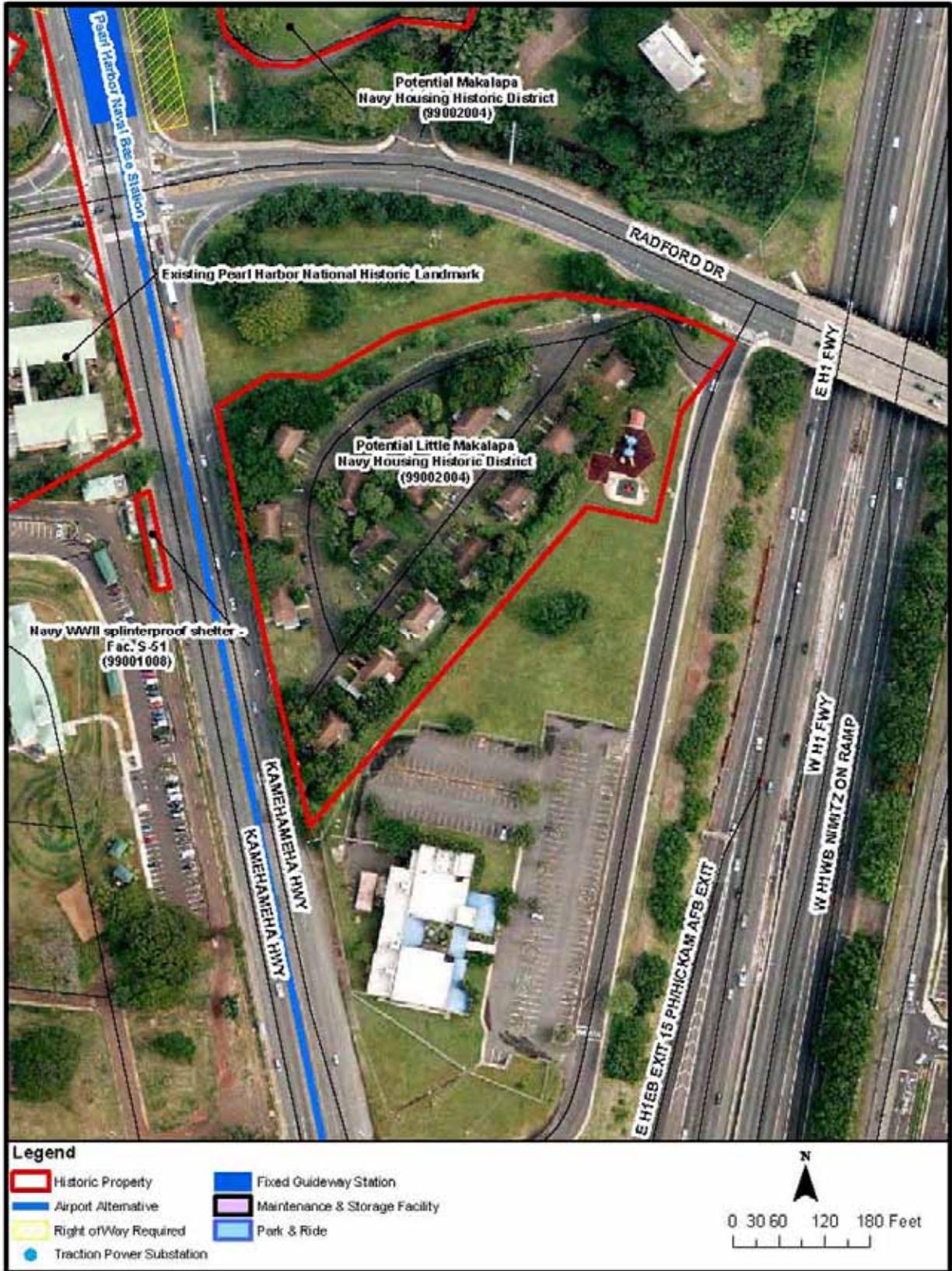


Figure 85. Little Makalapa Navy Housing

5.36 Navy WWII Splinterproof Shelter—Facility S-51

36 Pearl Harbor Naval Base Station sector—TMK 99001008
Kamehameha Highway between Radford and Center Drives

Property description

The Navy WWII Splinterproof Shelter is constructed of arched segments of pre-cast reinforced concrete built on a thick concrete slab. The structure is 113 feet long with thick concrete walls. It was designed to protect against shrapnel and bullets and could hold approximately 225 people. The shelter was determined to be eligible under Criterion A for its association with Pearl Harbor's defenses after the Japanese attack, and under Criterion C as an example of Pearl Harbor's splinterproof construction. The structure is directly next to Kamehameha Highway.

Project description in vicinity of property

The Project would be located adjacent to the Navy WWII Splinterproof Shelter along Kamehameha Highway (see Figure 86 and Drawing HP014 in Appendix A). The makai edge of the guideway would be approximately 25 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-14 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Navy WWII Splinterproof Shelter. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Navy WWII Splinterproof Shelter retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Navy WWII Splinterproof Shelter retains a moderate level of integrity of design. Character-defining 16-foot-tall vent stacks were removed from the structure. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The Navy WWII Splinterproof Shelter is located along busy Kamehameha Highway. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility. After field views during the effects assessment phase, architectural historians determined that the property has a very low level of integrity of setting. The shelter is on a ridge above Kamehameha Highway and is surrounded by power transmission lines. It does not retain its historic setting. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and

station would be new elements in the building's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Navy WWII Splinterproof Shelter retains a moderate level of integrity of materials because of the removal of wood vent stacks. The Project would not alter the current integrity of materials. All project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Navy WWII Splinterproof Shelter retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Navy WWII Splinterproof Shelter has a high level of integrity of feeling. It conveys its origins as a WWII-era defensive military building. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would introduce new components into the adjacent setting. However, numerous other changes to the building's environment have not affected the building's integrity of feeling, and neither would the Project.

Association: No Effect. The Navy WWII Splinterproof Shelter has a high level of integrity of association. It continues to demonstrate its role in Pearl Harbor's post-attack defensive construction efforts. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

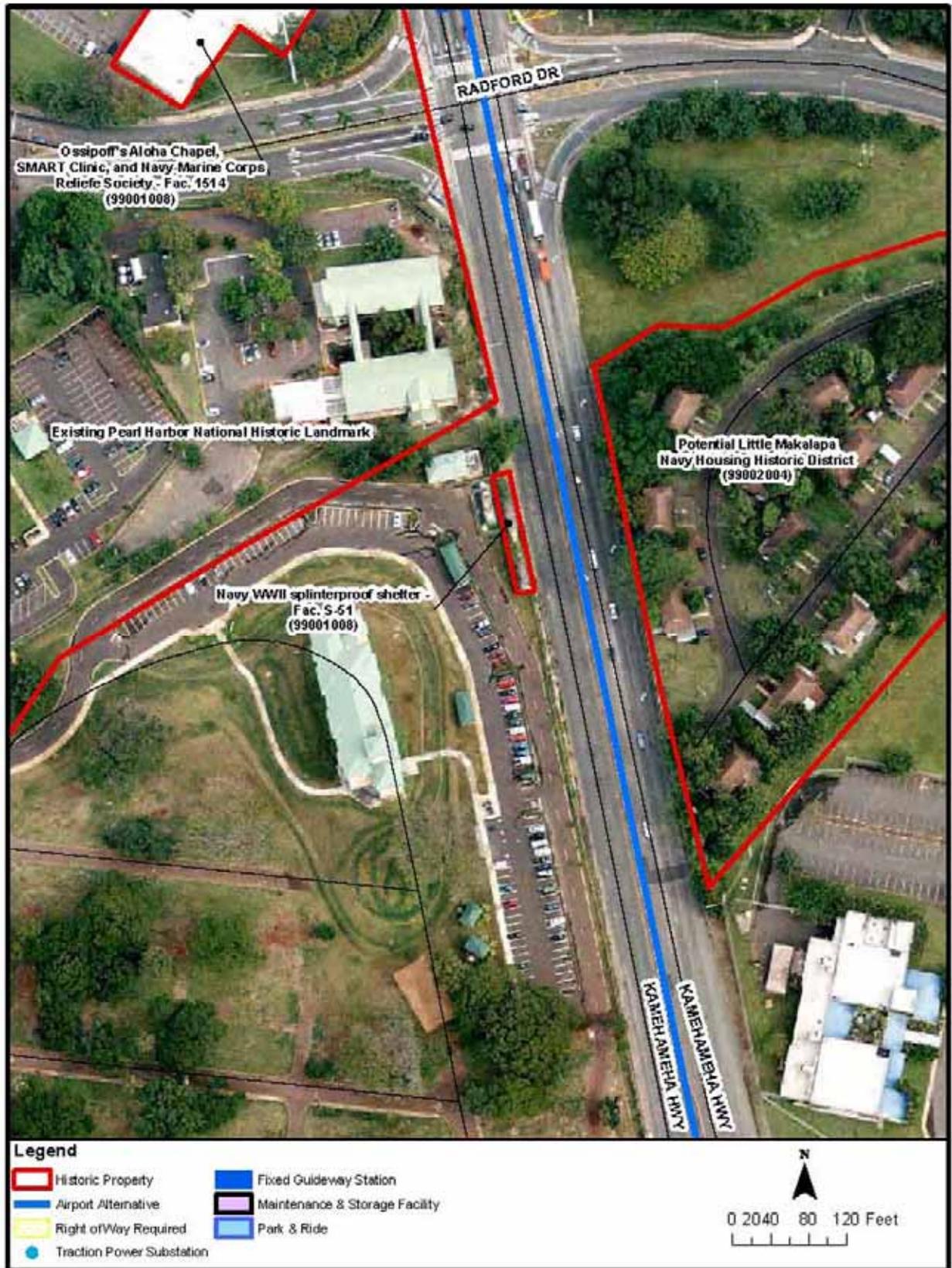


Figure 86. Navy WWII Splinterproof Shelter—Facility S-51

5.37 Rehab Center/Former Navy Fire Station—Facility 199

36 Pearl Harbor Naval Base Station sector—TMK 99001008
Kamehameha Highway at Center Drive

Property description

The Rehab Center/Former Navy Fire Station was built in 1941 and has an irregular L-shaped plan. It is a single-story building with a tall central tower. It is constructed of concrete masonry units. The property is eligible for the NRHP under Criterion A for its association with World War II facilities expansion, and under Criterion C as an example of a distinctive type of construction during a distinct time period. Changes to the building include replacement of the original wood folding doors with concrete masonry units and new windows, and installation of replacement windows.

Project description in vicinity of property

The Project would be located mauka of the Rehab Center/Former Navy Fire Station (see Figure 85 and Drawing HP015 in Appendix A). The makai edge of the guideway would be approximately 75 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-14 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Rehab Center/Former Navy Fire Station. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Rehab Center/Former Navy Fire Station retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Rehab Center/Former Navy Fire Station retains a moderate level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: No Adverse Effect. The Rehab Center/Former Navy Fire Station's setting is not a character-defining feature and does not contribute to its NRHP eligibility. The guideway would be visible from select areas of the parcel, but it would not adversely affect the setting, which is already compromised by surrounding new construction and busy Kamehameha Highway. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway would introduce a new element into the building's setting, there would be an effect, but these changes are minimal when evaluated in relationship to this particular building and would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Rehab Center/Former Navy Fire Station retains a low level of integrity of materials because the changes described above. The Project would not alter the current integrity of materials. All project work would occur outside the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Rehab Center/Former Navy Fire Station retains a low level of integrity of workmanship because of the changes described above. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Rehab Center/Former Navy Fire Station has a moderate level of integrity of feeling. It conveys its origins as a 1940s fire station, although significant alterations have diminished its integrity. The Project would introduce a new component into the adjacent setting, but this alteration would not diminish the integrity of feeling.

Association: No Effect. The Rehab Center/Former Navy Fire Station has a moderate level of integrity of association. It continues to demonstrate its role as a support building for the naval base. The Project would not affect any of the property's historically significant physical features, many of which have been altered substantially. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

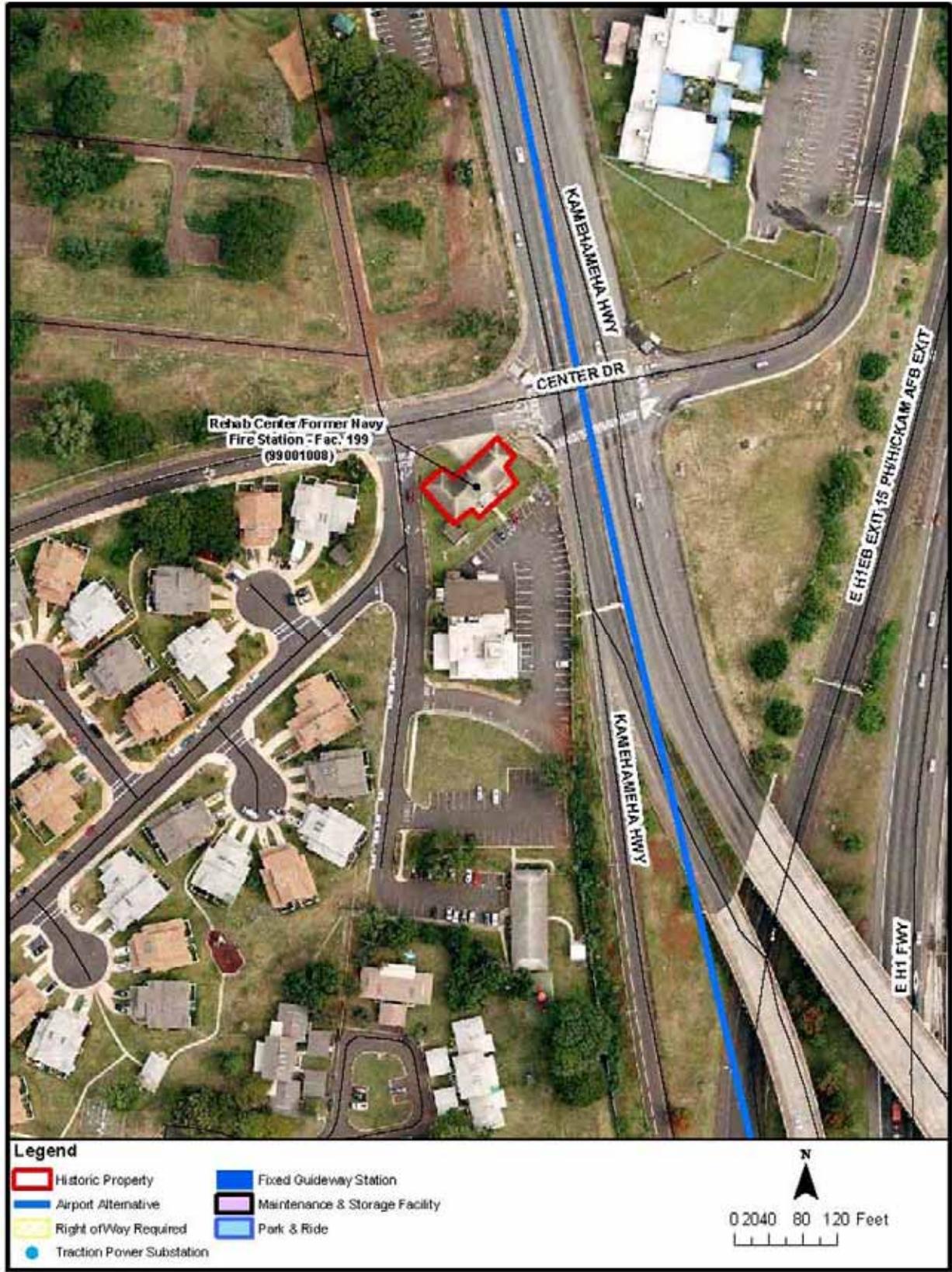


Figure 87. Rehab Center/Former Navy Fire Station—Facility 199

5.38 Hawai'i Employers Council

38 Lagoon Drive Station sector—TMK 11016004
2682 Waiwai Loop

Property description

The Hawai'i Employers Council building is a two-story, flat-roofed building constructed in 1961 of reinforced concrete and concrete block. Its irregular plan conforms to the parcel abutting Ke'ehi Lagoon Park. The property was determined eligible for the NRHP under Criterion A for its association with the history of labor relations in Hawai'i and under Criterion C for its association with the architectural firm Wimberly and Cook and its successor firm Wimberly, Allison, Tong & Goo, which had a major influence on Hawaiian architecture in this period. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure Koko Head of the Hawai'i Employers Council and within Ke'ehi Lagoon Park (see Figure 88 and TMK as shown on Drawing HP018 in Appendix A). The 'Ewa edge of the guideway would be approximately 100 feet from the Koko Head edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Hawai'i Employers Council.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property's setting was not identified as a historically significant feature. The building is oriented 'Ewa to Koko Head. Industrial and light industrial properties are 'Ewa of the property, and Ke'ehi Lagoon Park is Koko Head and adjacent to the property. Although the setting was not initially identified as a historically significant feature, architectural historians have determined that the property's Koko Head viewshed to Ke'ehi Lagoon Park, the Downtown Honolulu

skyline, and mauka mountain ranges is an integral component of the property and building's design. The Koko Head elevation of the building is fenestrated with multiple, large windows, presumably to accommodate the aforementioned views. In contrast, the 'Ewa elevation is fenestrated with few windows, possibly to reduce or eliminate visibility of the surrounding industrial properties and Honolulu International Airport. Because the property was determined eligible for the NRHP under Criterion C for its design, the Koko Head viewshed to the park, Downtown Honolulu, and surrounding mountains is a historically significant and character-defining feature of its setting. This portion of the property's setting is intact.

The Project would introduce an elevated, fixed guideway rail structure Koko Head of the property and within the adjacent Ke'ehi Lagoon Park that would be visible from the Koko Head portion of the property. The guideway's introduction would significantly alter and obstruct the property's Koko Head views to the park, to Downtown Honolulu, and to the mountains. Because this historically significant viewshed would be obstructed, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects were identified.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The building is oriented 'Ewa to Koko Head to take advantage of views to the adjacent park, to Downtown Honolulu, and to the mountains. Although the property's physical features would not be altered and the building would remain intact, the guideway would obstruct the property's historically significant views and negate the significance of the building's orientation. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The building is oriented 'Ewa to Koko Head to take advantage of views to the adjacent park, to Downtown Honolulu, and to the mountains. Although the property's physical features would not be altered and the building would remain intact, the guideway would obstruct the property's historically significant views and obscure the significance of the building's orientation. Therefore, the Project would have an adverse effect to the property's integrity of association.



Figure 88. Hawai'i Employers Council



Figure 89. Hawai'i Employers Council, facing mauka from Kalihi Lagoon Park parking lot, property on left



Figure 90. Hawai'i Employers Council, facing Koko Head from property to Kalihi Lagoon Park and downtown Honolulu



Figure 91. Visual Simulation: Ke‘ehi Lagoon Park Representative View, looking Koko Head, building location to left

5.39 Gaspro Store

19 Middle Street Station sector—TMK 12013007
2323 Kamehameha Highway

Property description

The Gaspro Store is a two-story office and retail building constructed in 1958. The property was determined eligible for the NRHP under Criterion C as an example of a distinctive creative application of the International style and for its use of prestressed concrete floor planking.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform in the median of Kamehameha Highway mauka of the Gaspro Store building (see Figure 92 and TMK as shown on Drawing HP018 in Appendix A). The makai edge of the guideway would be approximately 50 feet from the mauka edge of the NRHP boundary and approximately 75 to 100 feet from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-15 in Appendix B and would be elevated approximately 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-8 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Gaspro Store.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and its setting was not identified as a historically significant feature. Non-historic industrial and commercial properties are located mauka, 'Ewa, and makai. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure and transit station platform mauka of the property and within the center of the adjacent heavily travelled Kamehameha Highway alignment. These structures would be visible from the mauka portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the

presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of association.

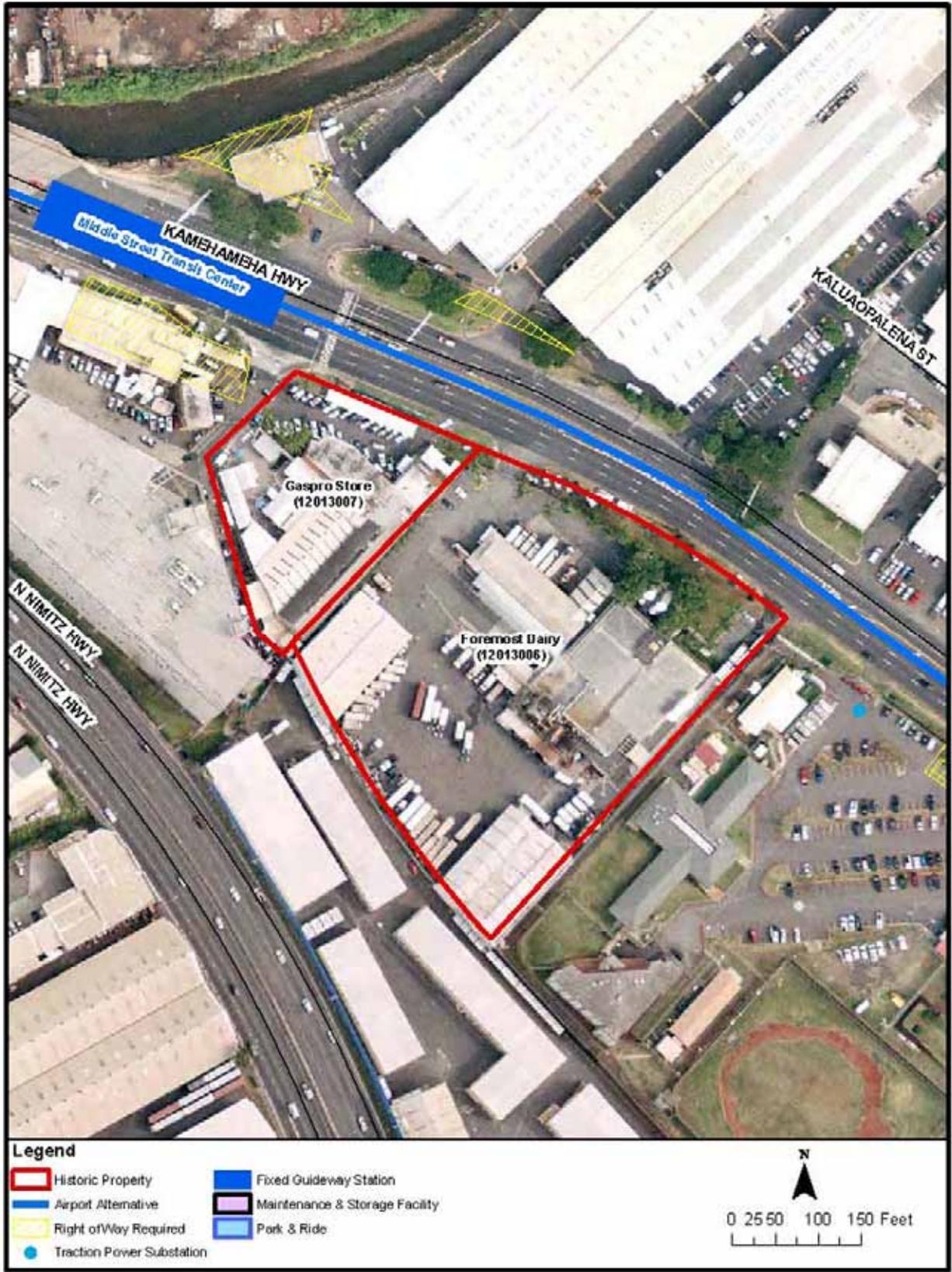


Figure 92. Gaspro Store



Figure 93. Gaspro Store, facing mauka/‘Ewa from property to Kamehameha Highway



Figure 94. Gaspro Store, facing mauka/Koko Head from property to Kamehameha Highway

5.40 Foremost Dairy

19 Middle Street Station sector—TMK 12013006
2277 Kamehameha Highway

Property description

The Foremost Dairy is a two-story, International-style building and dairy complex constructed in 1957. The property was determined eligible for the NRHP under Criterion A for its association with the development of the dairy industry on O‘ahu and under Criterion C as an example of an International-style building with distinctive Hawaiian detailing.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Kamehameha Highway mauka of the Foremost Dairy (see Figure 95 and TMK as shown on Drawing HP0018 in Appendix A). The makai edge of the guideway would be approximately 40 to 50 feet from the mauka edge of the NRHP boundary and approximately 125 feet from the building. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-15 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Foremost Dairy.

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property’s setting was not identified as a historically significant feature. Non-historic industrial, commercial, and institutional properties are located mauka, Koko Head, and makai. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway. These properties and features within the property’s setting and viewshed do not contribute to the historic significance of this property. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure mauka of the property and within the center of the adjacent Kamehameha Highway alignment that would be visible from the mauka portion of the property. Although the guideway’s construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or

atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a late mid-20th-century dairy complex. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century dairy complex. Therefore, the Project would have no effect to the property's integrity of association.

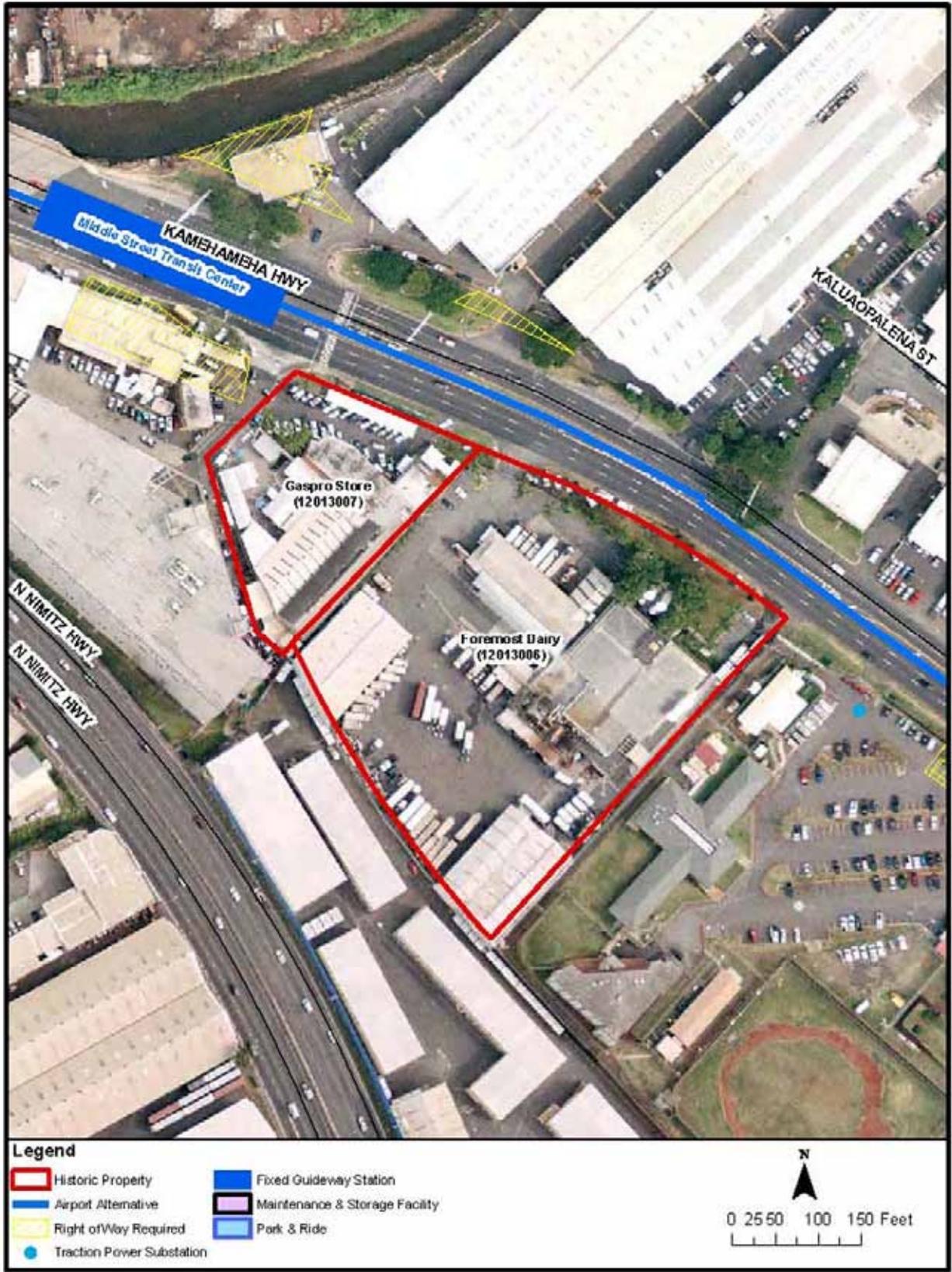


Figure 95. Foremost Dairy



Figure 96. Foremost Dairy, facing mauka/'Ewa from property to Kamehameha Highway



Figure 97. Foremost Dairy, facing mauka from property to Kamehameha Highway

5.41 Lava Rock Curbs

20 Kalihi through 25 Civic Center Station sectors—TMK not applicable
Along proposed alignment from approximately Laumaka Street to South Street (excluding Nimitz Highway)

Property description

The Lava Rock Curbs are dense sections of basalt lava rock laid along the edges of roadways between the late 19th and mid-20th centuries. These objects were determined eligible for the NRHP under Criterion A for their association with the roadway infrastructure development of Honolulu and under Criterion C as examples of the distinctive method of street construction in Honolulu during the late 1800s and early 1900s.

Project description in vicinity of objects

The Project would include construction of an elevated, fixed-guideway rail structure within and along sections of roadway in Honolulu that contain lava rock curbs. Generally, project construction would not affect roadway curbs. However, along Dillingham Boulevard between approximately Laumaka Street and Ka'aahi Street, the roadway does not contain a median and would require widening to accommodate the guideway. In this location, lava rock curbstones along the makai edge of Dillingham Boulevard would be removed (see Figures 98a through 98d and Drawings HP019 and HP020 in Appendix A). In addition, to accommodate a short offset along Halekauwila Street in Downtown Honolulu, a small portion of curbing along the makai side of this street would also be removed (see Figure 98e and Drawing HP022 in Appendix A). The design of the guideway along Dillingham Boulevard from Laumaka Street to Ka'aahi Street would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 30 to 45 feet above grade. The design of the guideway along Halekauwila Street from Richards Street to Punchbowl Street would correspond to the typical section illustrated as Figure B-24 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation and conditions below, the Project would have an **Adverse Effect** to the Lava Rock Curbs.

Location: Adverse Effect. The Project would require widening Dillingham Boulevard along its makai edge between Laumaka Street and Ka'aahi Street and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. As a result, the Project would have an adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would require widening Dillingham Boulevard along its makai edge between Laumaka Street and Ka'aahi Street and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed

along the makai edge of Halekauwila Street. As a result, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. These objects generally do not retain integrity of setting outside of their NRHP boundary, and setting was not identified as a historically significant feature of the lava rock curbs. Generally, the roadway alignments containing the curbstones have been altered by previous widening and are characterized by non-historic commercial, light industrial, office/institutional, and residential properties. These properties within the property's setting and viewshed do not contribute to the historic significance of the curbstones. Furthermore, no historically significant viewsheds to or from the curbs were identified. The Project would introduce an elevated, fixed guideway rail structure within the roadway alignments containing lava rock curbs. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the Project would significantly alter the immediate physical and visual setting of the property. The guideway's construction within the median of Dillingham Boulevard and the widening of that roadway along its makai edge between Laumaka and Ka'aahi Streets would require the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. This would represent a significant removal of a portion of the property (approximately 1.4 miles) within its NRHP boundary and would physically alter and visually obscure the property's immediate setting. Essentially, the removal or demolition of any eligible resource obviously affects its setting. Although the setting may not be a critical component contributing to why the curbs were determined eligible, it is nonetheless adversely affected. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the curbstones are neither noise receptors nor known to be sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: Adverse Effect. The Project would require widening Dillingham Boulevard along its makai edge between Laumaka and Ka'aahi Streets and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. As a result, the Project would have an adverse effect to the property's integrity of materials.

Workmanship: Adverse Effect. The Project would require widening Dillingham Boulevard along its makai edge between Laumaka and Ka'aahi Streets and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. As a result, the Project would have an adverse effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The Project would require widening Dillingham Boulevard along

its makai edge between Laumaka and Ka'aahi Streets and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. The removal of a significant number of curbstones, and on one side of a roadway, would significantly obscure the property's historic physical features, appearance, and immediate setting. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The Project would require widening Dillingham Boulevard along its makai edge between Laumaka and Ka'aahi Streets and the removal of lava rock curbstones in this area. Additional lava rock curbs would be removed along the makai edge of Halekauwila Street. The removal of a significant number of curbstones, and on one side of a roadway, would significantly obscure the property's historic physical features, appearance, and immediate setting. Therefore, the Project would have an adverse effect to the property's integrity of association.

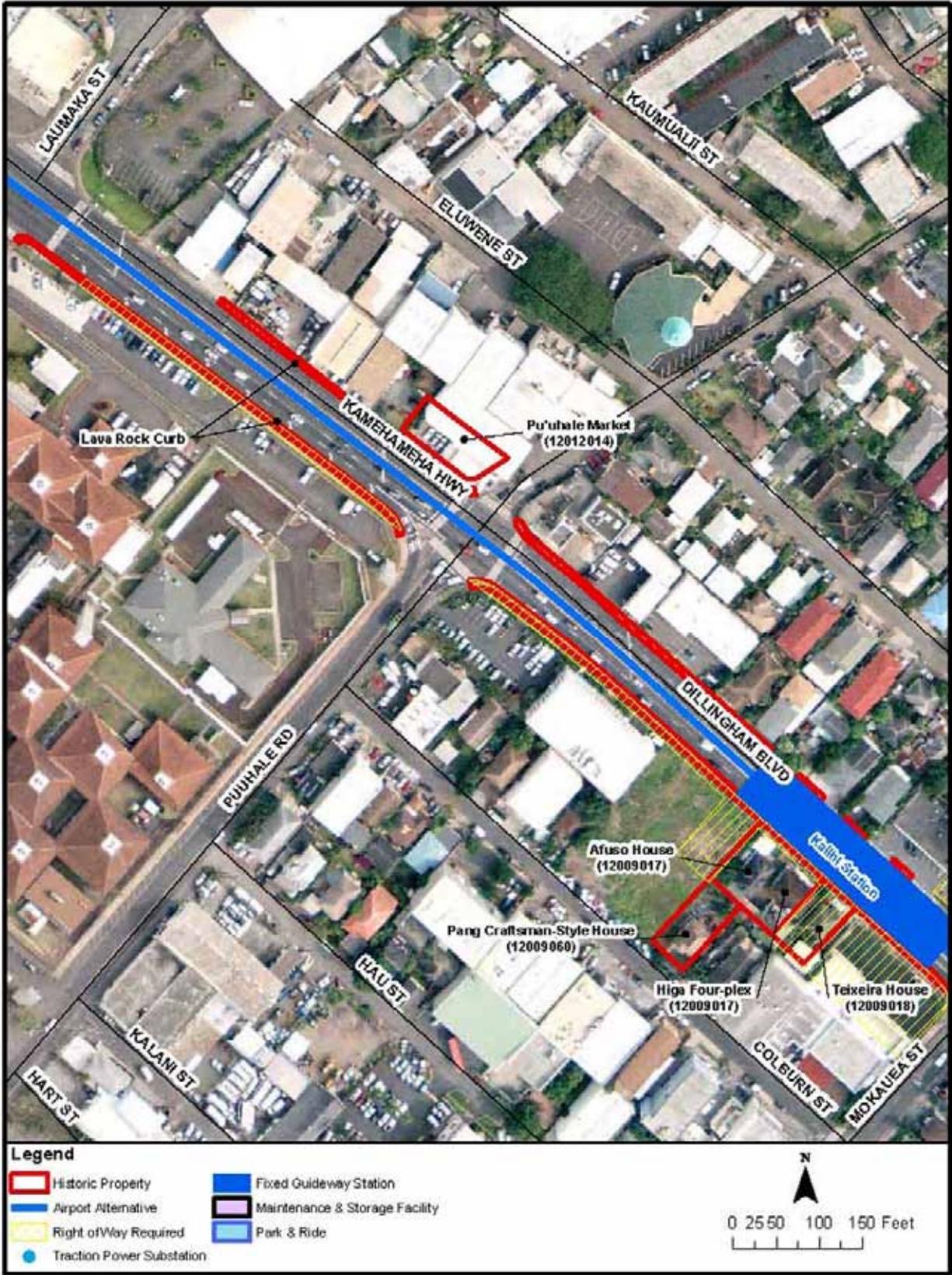


Figure 98a. Lava Rock Curbs

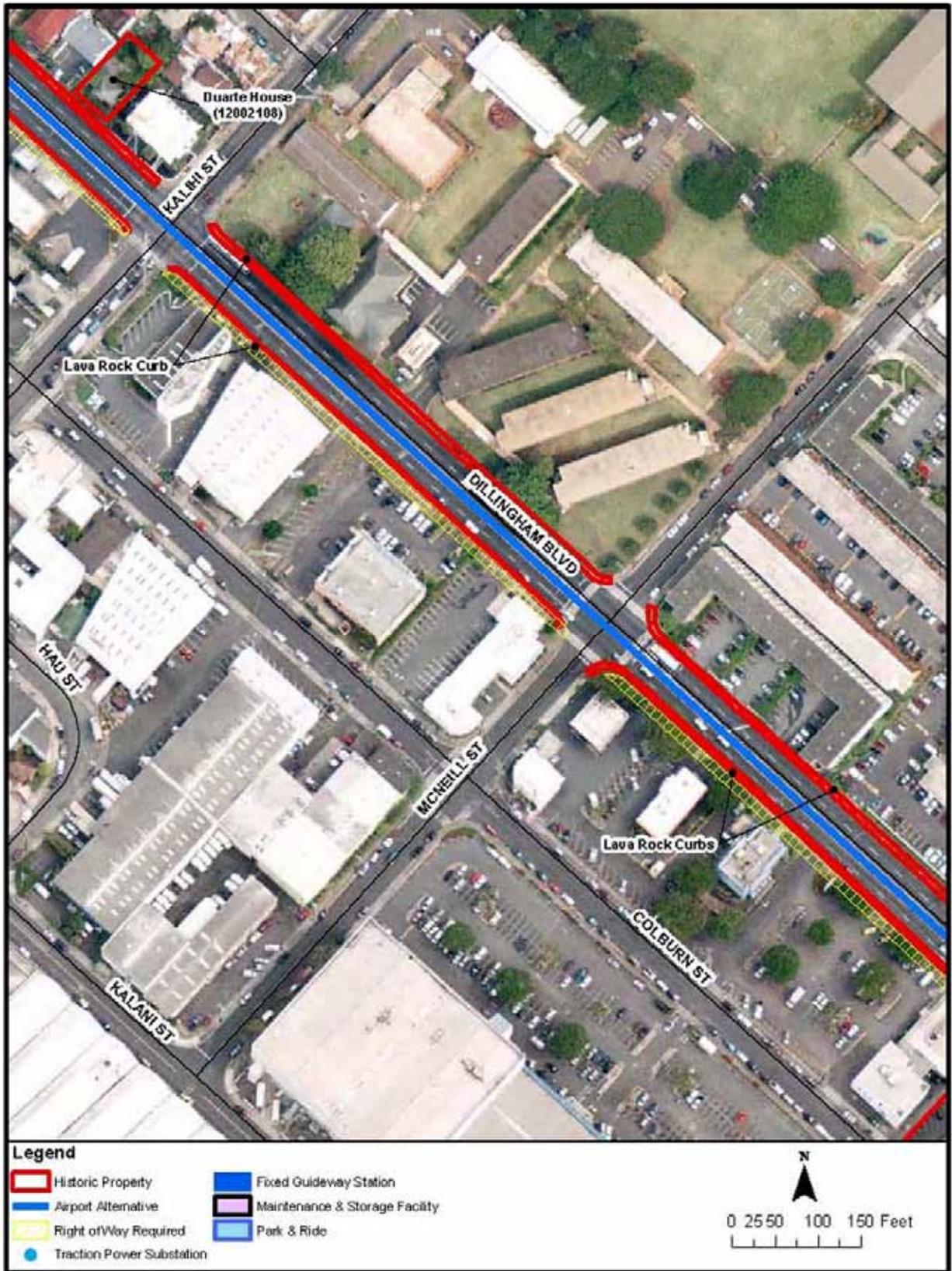


Figure 98b. Lava Rock Curbs

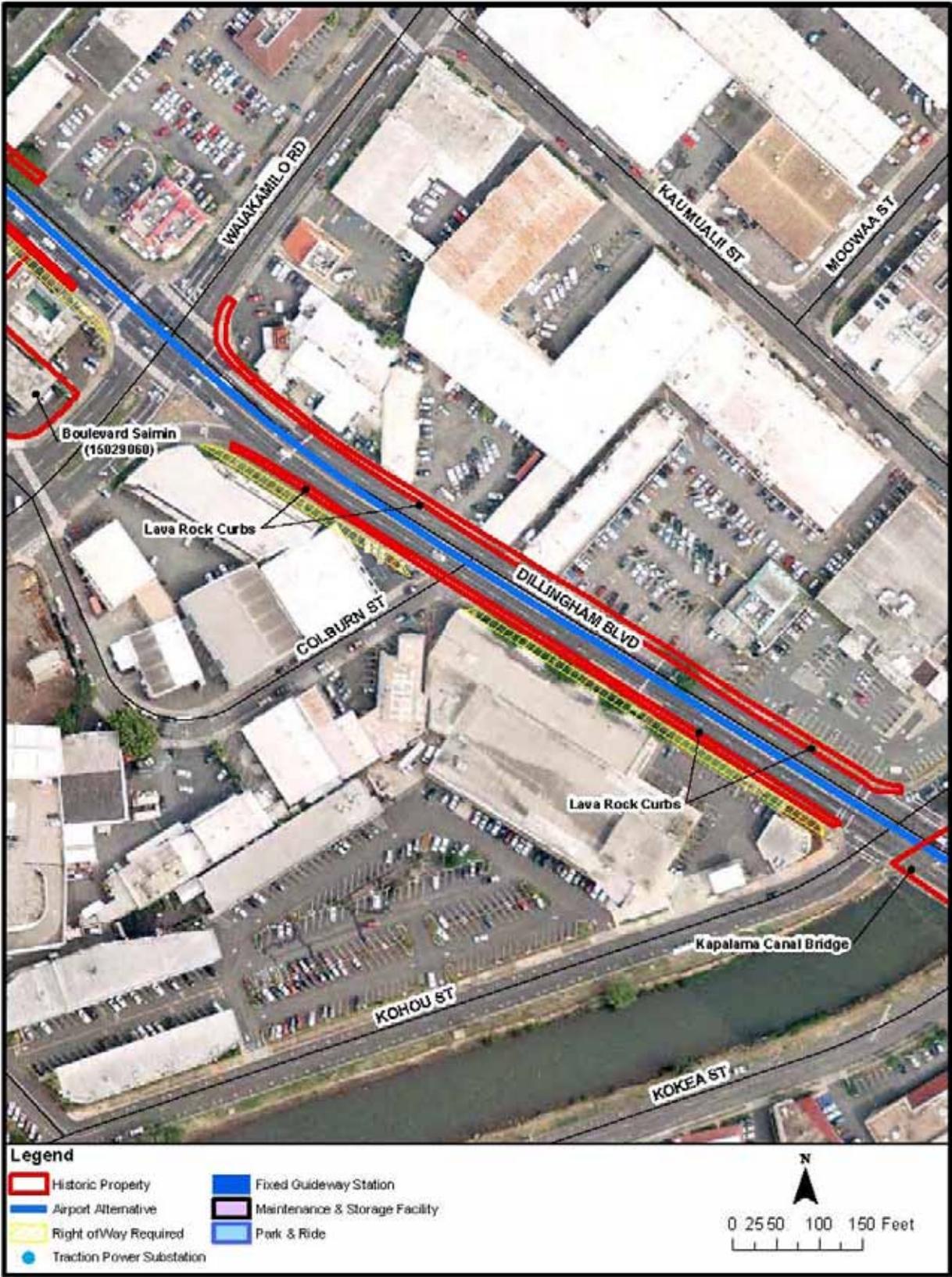


Figure 98c. Lava Rock Curbs

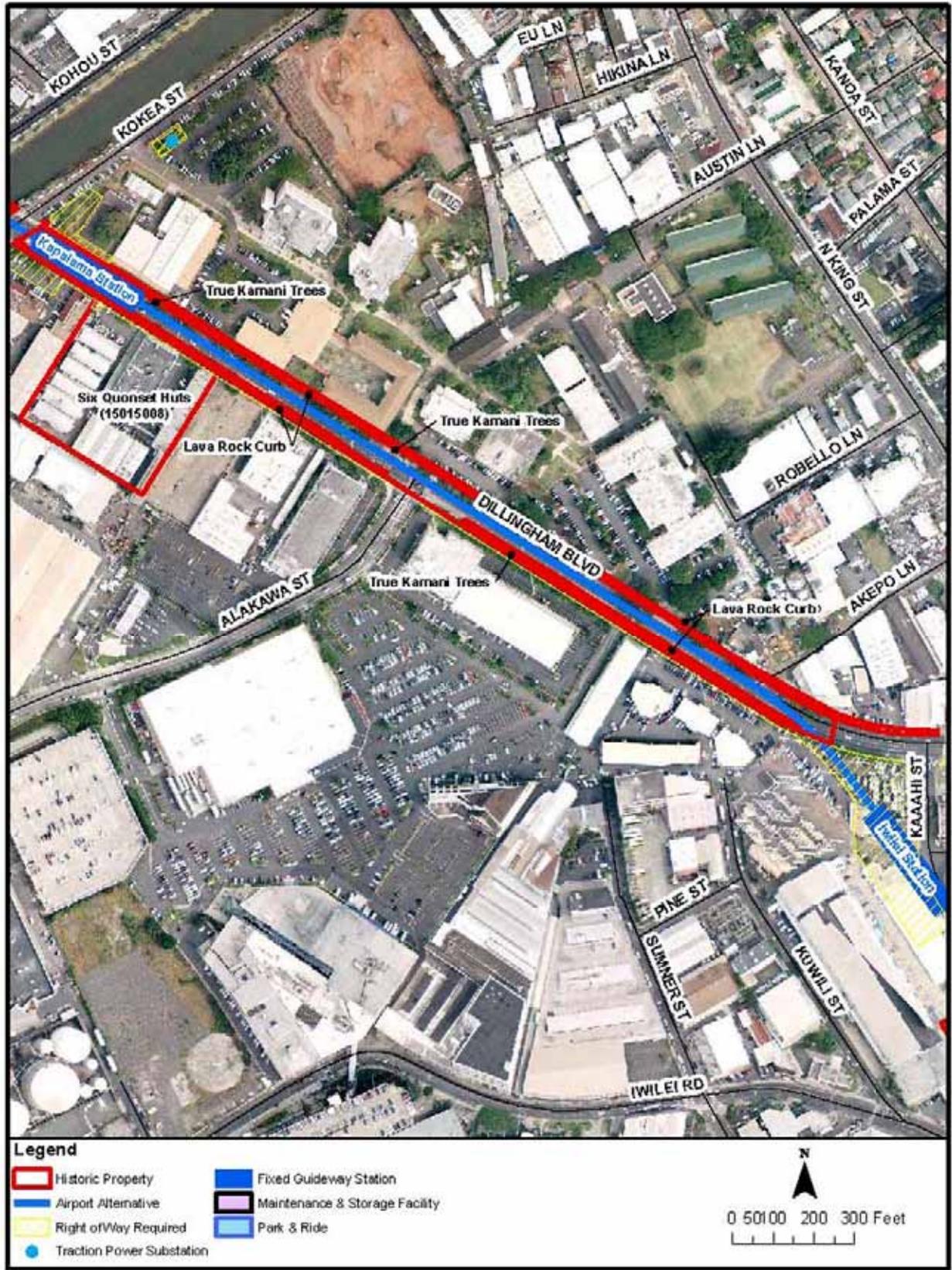


Figure 98d. Lava Rock Curbs

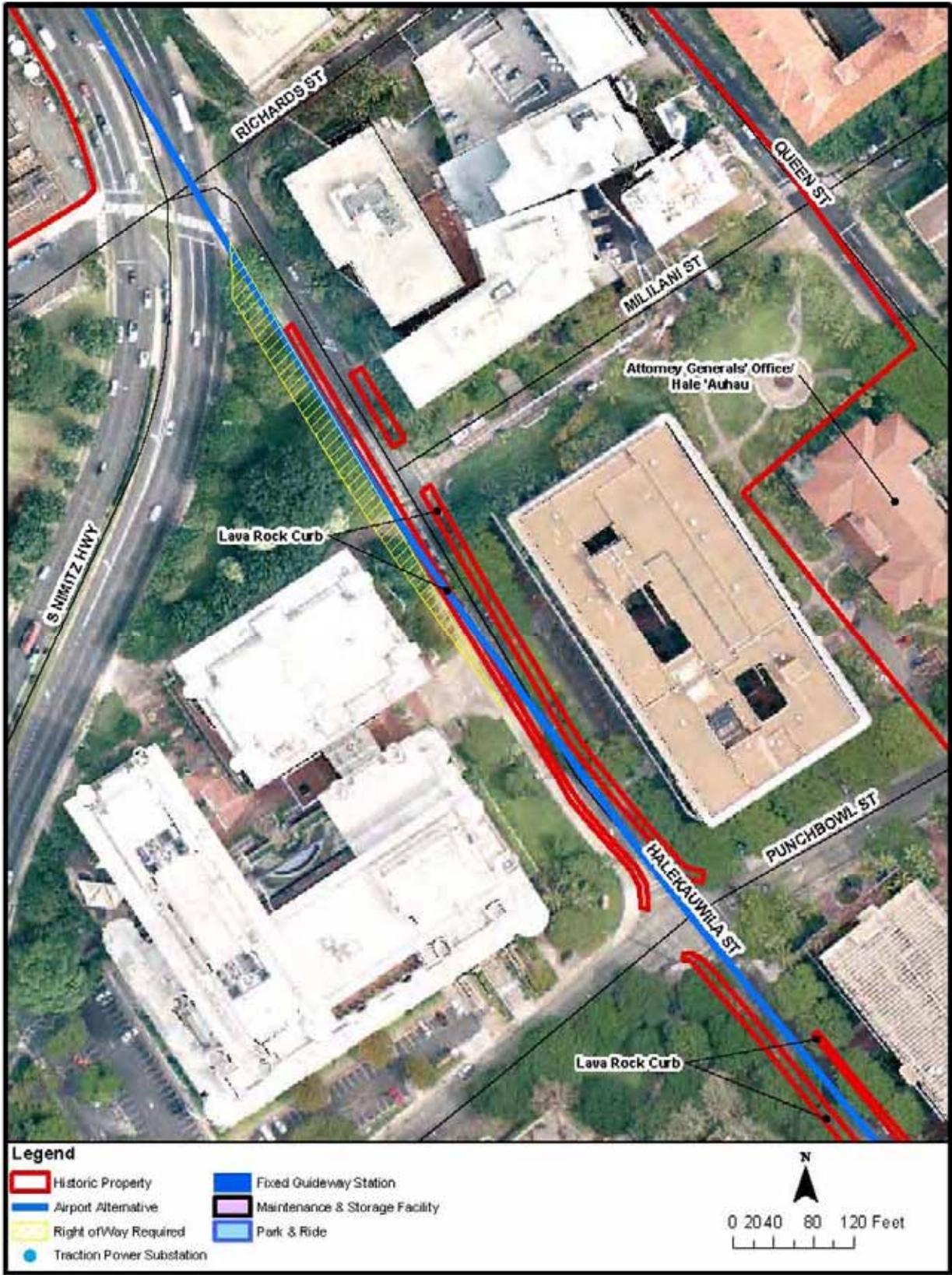


Figure 98e. Lava Rock Curbs

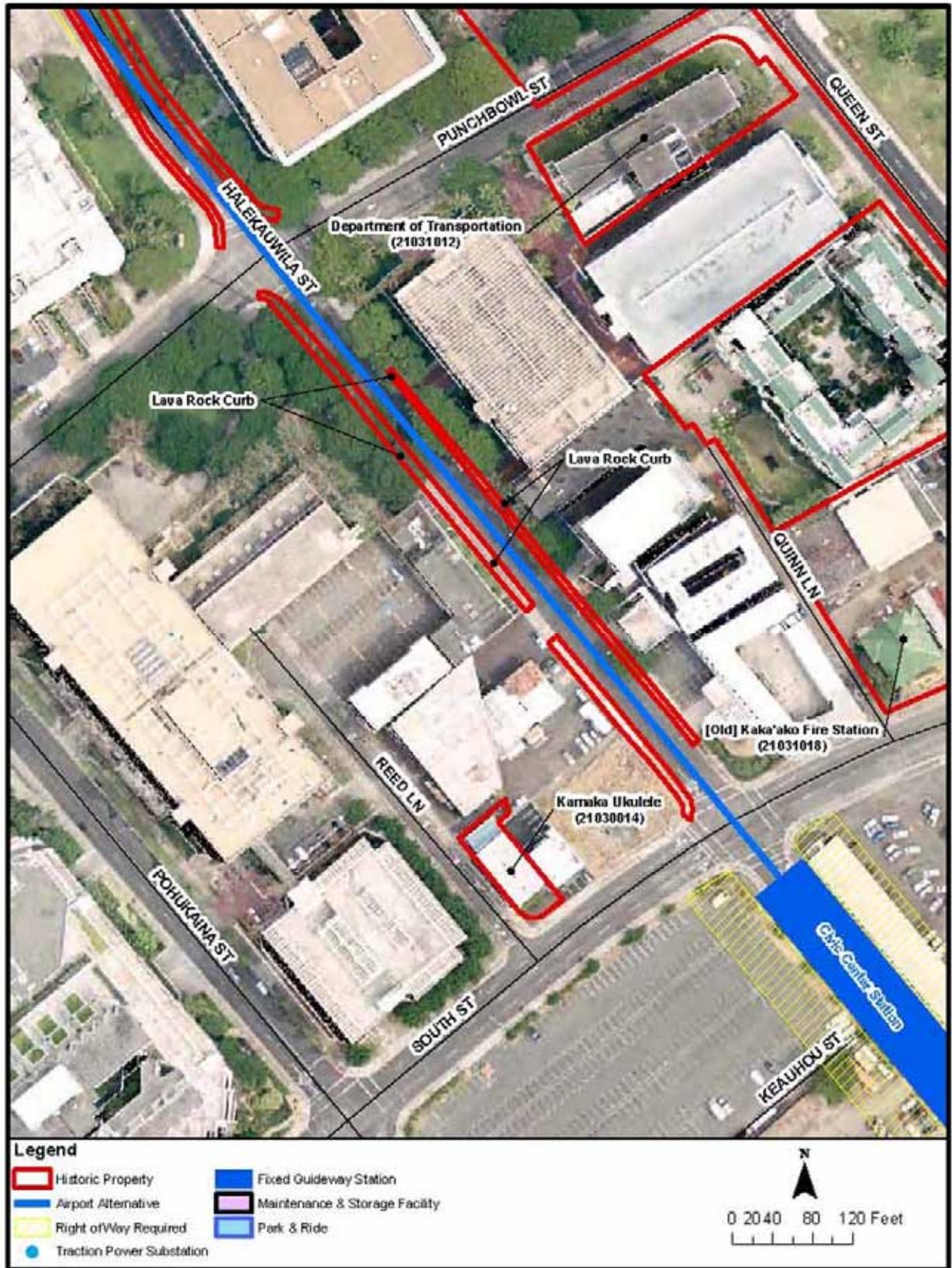


Figure 98f. Lava Rock Curbs



Figure 99. Lava Rock Curbs, representative view along Kamehameha Highway from just Koko Head of Kōkea Street



Figure 100. Lava Rock Curbs, detail of lava rock roadway curbing

5.42 Pu‘uhale Market

20 Kalihi Station sector—TMK 12012014
608 Pu‘uhale Road

Property description

The Pu‘uhale Market is a single-story commercial building originally constructed in 1918; the building has been altered by successive renovations. The property was determined eligible for the NRHP under Criterion A for its association with the “development of the Kalihi Kai neighborhood from the early 1900s through the period of transition to a mixed commercial-residential area.”

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Dillingham Boulevard makai of the Pu‘uhale Market (see Figure 101 and TMK as shown on Drawing HP019 in Appendix A). The mauka edge of the guideway would be approximately 25 feet from the makai edge of the NRHP boundary and from the building. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Pu‘uhale Market.

Location: No Effect. The Project would not cause the property’s removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property’s setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial, residential, and institutional properties; the non-historic O‘ahu Community Correctional Center is located makai and across Dillingham Boulevard, a heavily traveled roadway, from the property. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway and just above the historic property. These properties and features within the property’s setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure just makai of the property and within the center of the adjacent Dillingham Boulevard alignment that would be visible from the property. Although the guideway’s construction would introduce a new component to

the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of association.

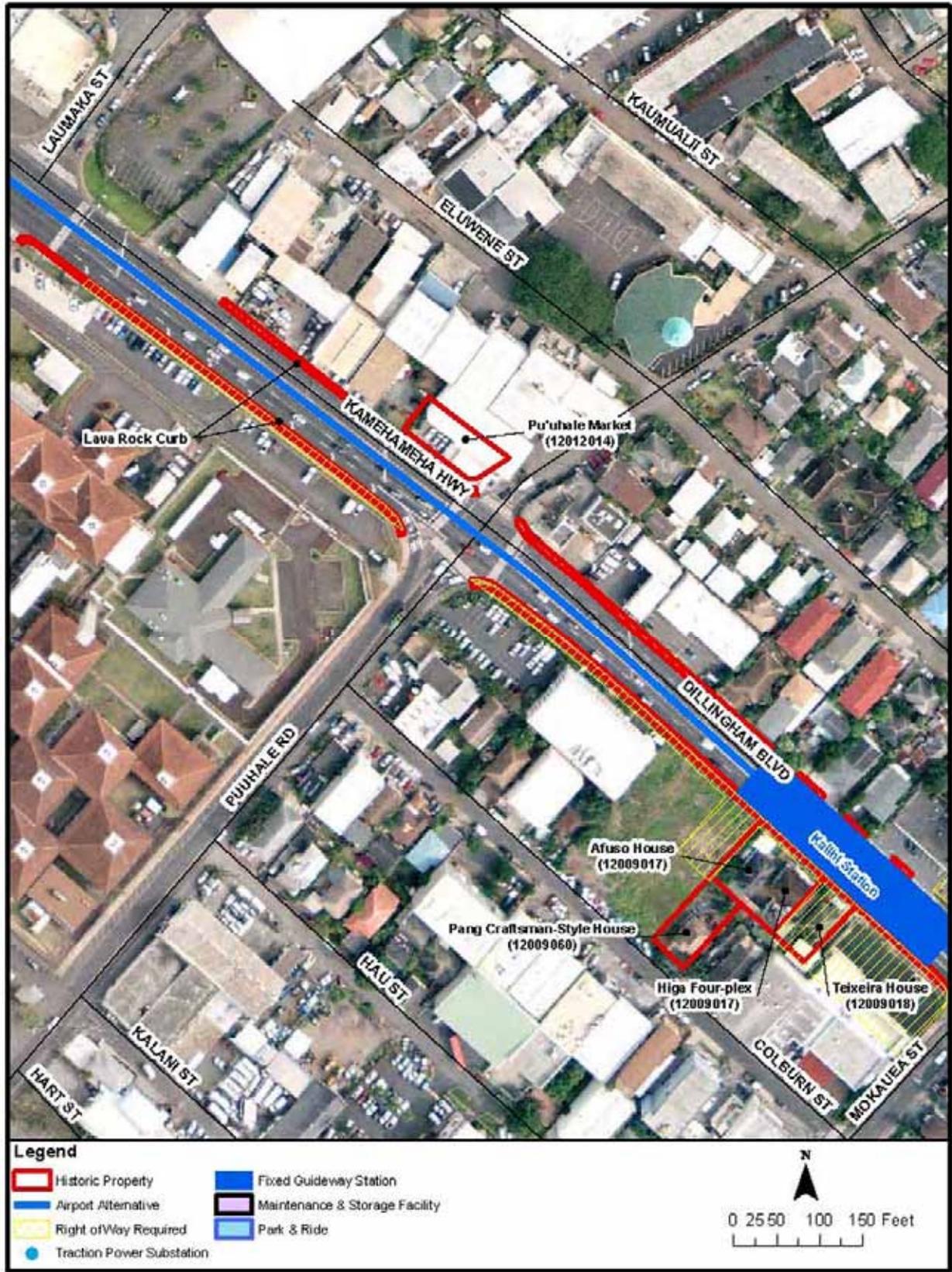


Figure 101. Pu'uhale Market



Figure 102. Pu'uhale Market, facing makai/'Ewa from Pu'uhale Road to Kamehameha Highway, property on right



Figure 103. Pu'uhale Market, facing Koko Head from property to Kamehameha Highway

5.43 Afuso House

20 Kalihi Station sector—TMK 12009017
1933 Dillingham Boulevard

Property description

The Afuso House is a single-story, plantation-style residence constructed in 1914 and expanded in 1939. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Kalihi Kai neighborhood in the early 1900s and Dillingham Boulevard's transition to a commercial-residential area through the 1930s and under Criterion C because it embodies the distinctive characteristics of an early urban house in the plantation style with some unusual features.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform within Dillingham Boulevard and immediately mauka of the Afuso House (see Figure 104 and TMK as shown on Drawing HP019 in Appendix A). The makai edge of the station structure would be approximately 10 feet from the mauka edge of the NRHP boundary and approximately 25 feet from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-9 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Afuso House.

Location: Adverse Effect. The Project would not remove the property from its historic location but would cause physical damage or destruction to a portion of the property. Construction would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard. This right-of-way would be acquired from the mauka portion of the historic property, also its front yard. Thus, the residential property's relatively small front yard would be significantly reduced. Furthermore, the guideway's and passenger platform's proximity to this property may reduce its desirability for future residential use, which is the property's historic use. Therefore, because project construction would significantly reduce the property's front yard, a feature within the NRHP boundary, and because the guideway's proximity may reduce the future viability of the property for residential use, the Project would have an adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would significantly reduce the property's front yard, a historic feature integral to the property's overall design and located within the NRHP boundary. Therefore, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary; it is surrounded by non-historic commercial and light industrial properties. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an elevated, fixed-guideway rail structure and passenger platform within Dillingham Boulevard just mauka of the property.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction would reduce the size of the property's front yard and would significantly alter the property's immediate setting. An approximately 10-foot strip of right-of-way would be acquired within the property's front yard and along Dillingham Boulevard, and the guideway's and station platform's placement immediately mauka of the property would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the historic house would generally obscure the historic appearance of the property and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, including reducing the property's front yard, which is a historic feature of the property's setting within its NRHP boundary, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Adverse Effect. The Project would significantly reduce the property's front yard. However, no historically significant materials related to the construction of the house or historically significant landscape features within the front yard would be altered. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would significantly reduce the property's front yard. However, no historically significant characteristics or features related to the workmanship used to create the property would be altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter portions of the property's setting within the NRHP boundary and also obscure the property's historic appearance and immediate environment. Although the house would remain physically intact, the proximity of the guideway to the house would substantially obscure the visual relationship between the house and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter portions of the property's setting within the NRHP boundary and also obscure the property's historic appearance and immediate environment. Although the house would remain physically intact, the proximity of the guideway to the house would substantially obscure the visual relationship between the house and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

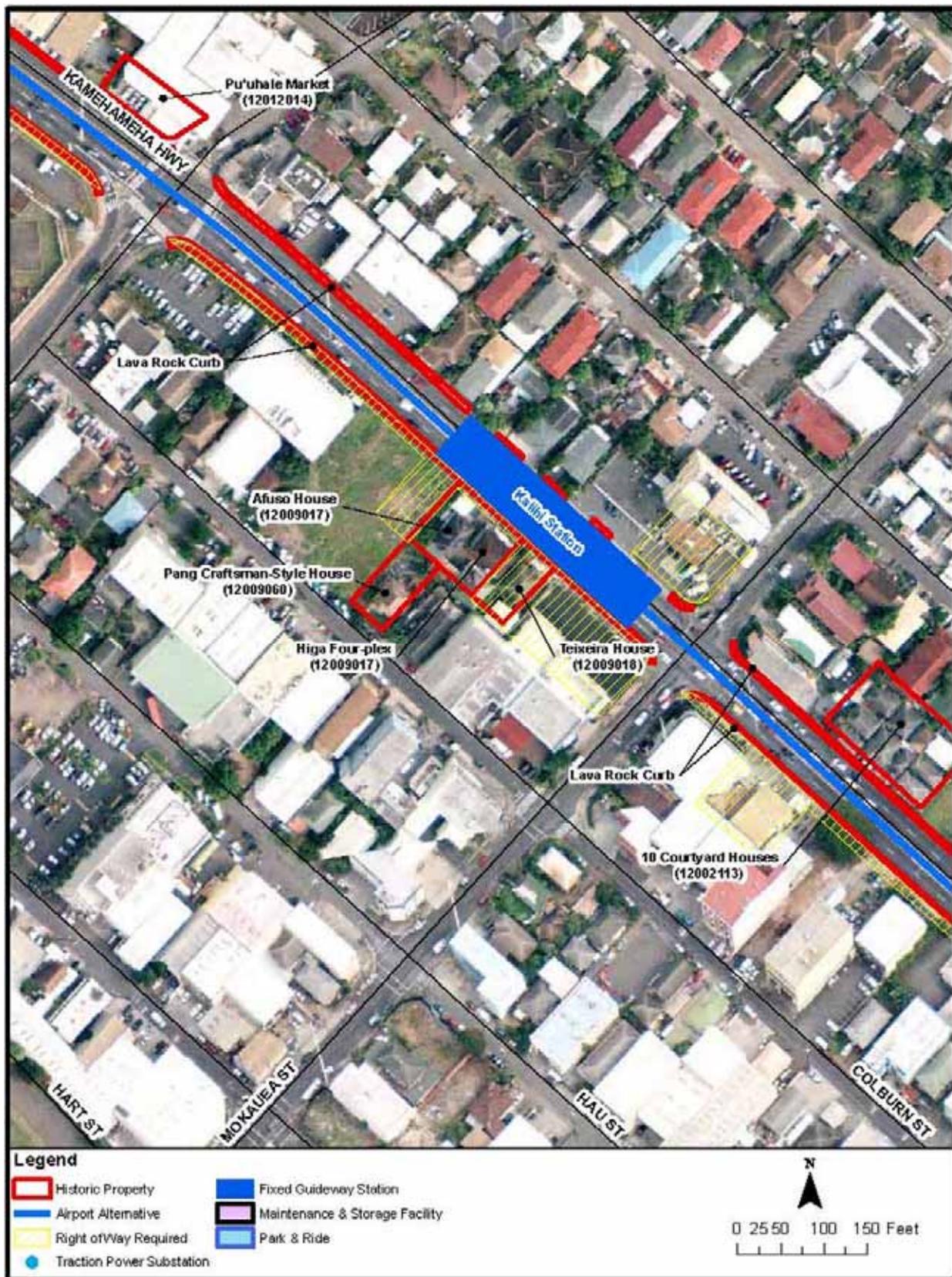


Figure 104. Afuso House



Figure 105. Afuso House, facing mauka/Ewa from property to Dillingham Boulevard



Figure 106. Afuso House, facing mauka/Koko Head from property to Dillingham Boulevard

5.44 Higa Four-plex

20 Kalihi Station sector—TMK 12009017
1945 Dillingham Boulevard

Property description

The Higa Four-plex is a two-story, plantation-style four-plex residence constructed in 1941 and expanded in 1944. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Dillingham Boulevard area in the 1940s when there was increased demand for housing in the build-up prior to World War II and under Criterion C as a distinctive example of a “plantation-style duplex design (the top story) transmuted into a four-plex in an urban neighborhood.”

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform within Dillingham Boulevard and immediately mauka of the Higa Four-plex (see Figure 107 and TMK as shown on Drawing HP019 in Appendix A). The makai edge of the station structure would be approximately 10 feet from the mauka edge of the NRHP boundary and from the building. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-9 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Higa Four-plex.

Location: Adverse Effect. The Project would not remove the property from its historic location but would cause physical damage or destruction to a portion of the property. Construction would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard at this property. This right-of-way would be acquired from the mauka portion of the historic property, which is also its front yard. Thus, the residential property’s small front yard would be eliminated. Furthermore, the guideway and passenger platform’s proximity to this property may reduce its desirability for future residential use, which is the property’s historic use. Therefore, because project construction would eliminate the property’s front yard, a feature located within the NRHP boundary, and because the guideway’s proximity may reduce the future viability of the property for residential use, the Project would have an adverse effect to the property’s integrity of location.

Design: Adverse Effect. The Project would eliminate the property’s front yard, a historic feature integral to the property’s overall design and located within the NRHP boundary. Therefore, the Project would have an adverse effect to the property’s integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary; it is surrounded by non-historic commercial and light industrial properties. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an elevated, fixed-guideway rail structure and passenger platform within Dillingham Boulevard just mauka of the property.

Although setting was not identified as a historically significant feature or characteristic of the property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction would require the acquisition and use of the property's front yard and would significantly alter the property's immediate setting. An approximately 10-foot strip of right-of-way would be acquired along Dillingham Boulevard and eliminate the property's front yard, and the guideway's and station platform's placement immediately mauka of the property would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the historic residence would generally obscure the historic appearance of the property and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, including elimination of the property's front yard, a historic feature of the property's setting within its NRHP boundary, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Adverse Effect. The Project would eliminate the property's front yard. However, no historically significant materials related to the construction of the residence or historically significant landscape features within the front yard would be altered. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would eliminate the property's front yard. However, no historically significant characteristics or features related to the workmanship used to create the property would be altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter portions of the property's setting within the NRHP boundary and also obscure the property's historic appearance and immediate environment. Although the house would remain physically intact, the proximity of the guideway to the house would substantially obscure the visual relationship between the house and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter portions of the property's setting within the NRHP boundary and also obscure the property's historic appearance and immediate environment. Although the house would remain physically intact, the proximity of the guideway to the house would substantially obscure the visual relationship between the house and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

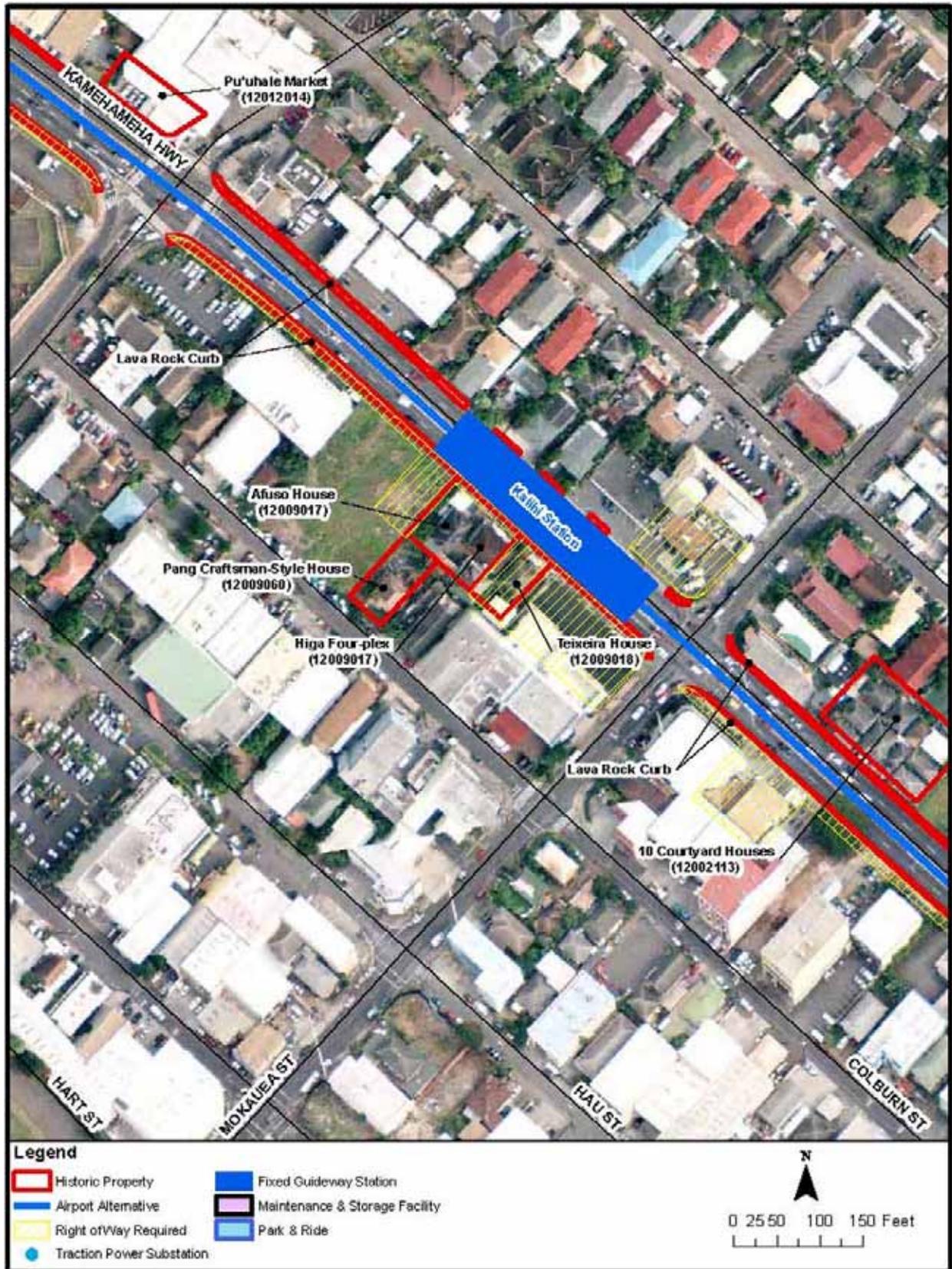


Figure 107. Higa Four-plex



Figure 108. Higa Four-plex, facing mauka/Ewa from property to Dillingham Boulevard



Figure 109. Higa Four-plex, facing mauka/Koko Head from property to Dillingham Boulevard

5.45 Teixeira House

20 Kalihi Station sector—TMK 12009018
1927 Dillingham Boulevard

Property description

The Teixeira House is a single-story, plantation-style house constructed in 1945. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Kalihi Kai neighborhood in the first half of the 20th century and with Dillingham Boulevard's transition to a commercial-residential area through the 1930s, as well as under Criterion C because it embodies the distinctive characteristics and is a good example of a 1940s single-wall, plantation-style dwelling.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform within Dillingham Boulevard and substantially within portions of the parcel and NRHP boundary containing the Teixeira House (see Figure 110 and TMK as shown on Drawing HP019 in Appendix A). The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-9 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Teixeira House.

Location: Adverse Effect. The Project would require acquisition of the property parcel containing the Teixeira House and removal of the building. Therefore, because project construction would require the removal of the Teixeira House, the Project would have an adverse effect to the property's integrity of location.

Because the Project would require the removal of the historic property, the removal or demolition of the property would inherently result in an adverse effect to all other aspects of integrity.

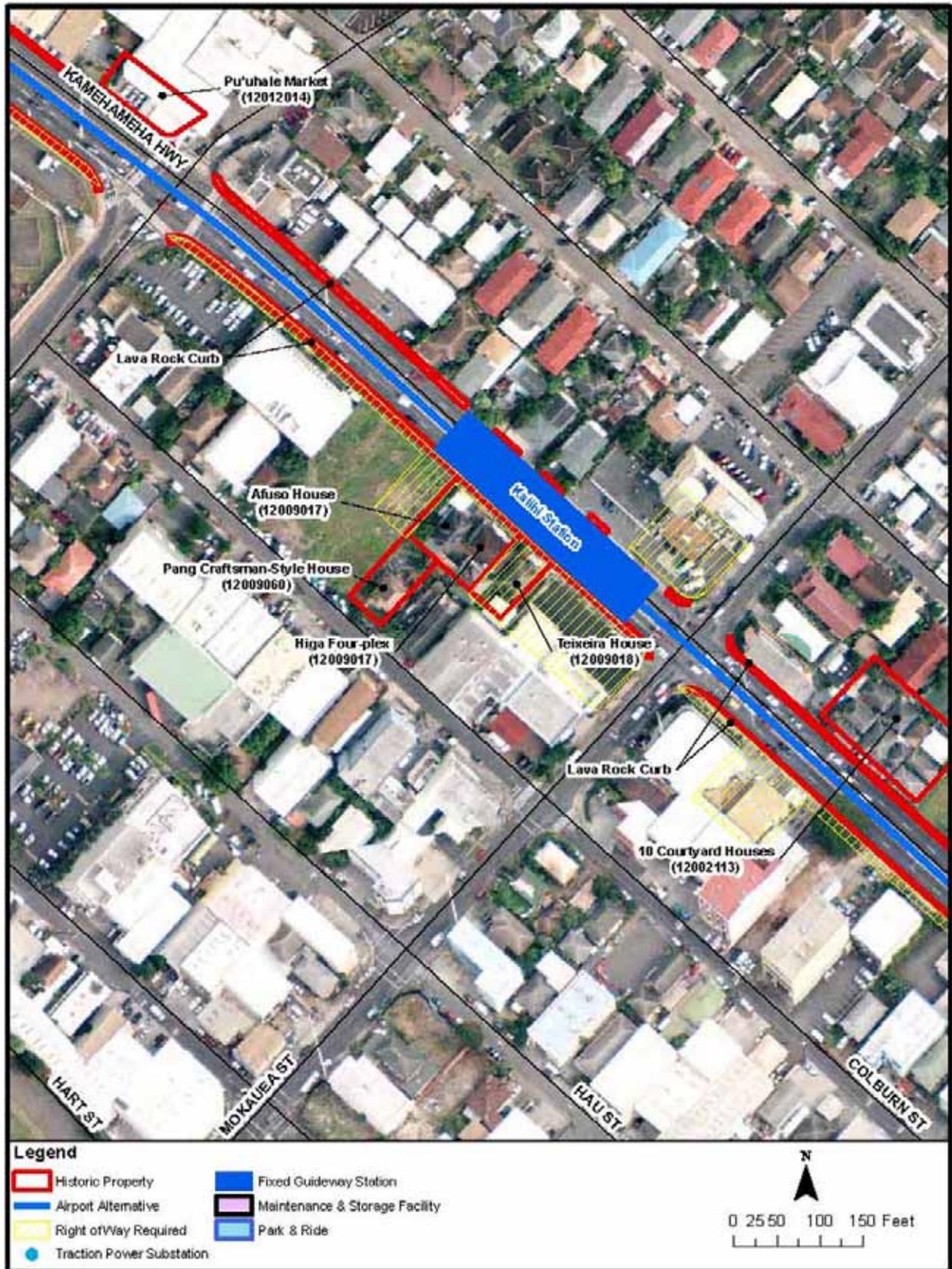


Figure 110. Teixeira House



Figure 111. Teixeira House, facing mauka/Ewa from property to Dillingham Boulevard



Figure 112. Teixeira House, facing mauka/Koko Head from property to Dillingham Boulevard

5.46 Pang Craftsman-Style House

20 Kalihi Station sector—TMK 12009060
1928 Colburn Street

Property description

The Pang Craftsman-Style House is a single-story, Craftsman-style residence constructed in 1933. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Kalihi Kai neighborhood in its period of transition during the 1930s to a residential-commercial area and under Criterion C because it embodies the distinctive characteristics of, and is a good example of, a Craftsman-style, pre-World War II, single-wall house.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform within the alignment of Dillingham Boulevard located behind and mauka of the Pang Craftsman-Style House (see Figure 113 and TMK as shown on Drawing HP019 in Appendix A). The makai edge of the guideway would be approximately 100 feet from the mauka edge of the NRHP boundary and approximately 125 feet from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-9 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Pang Craftsman-Style House.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. The property is generally surrounded on all sides by light industrial properties; the house is oriented makai to Colburn Street, which is characterized by non-historic light industrial properties. These properties within the setting and viewshed do not contribute to the historic significance of this property. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure and transit station platform mauka of the property and to its rear that would be visible from the mauka portion of

the property, which is also the property's back yard. Although the guideway's construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century house. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century house. Therefore, the Project would have no effect to the property's integrity of association.

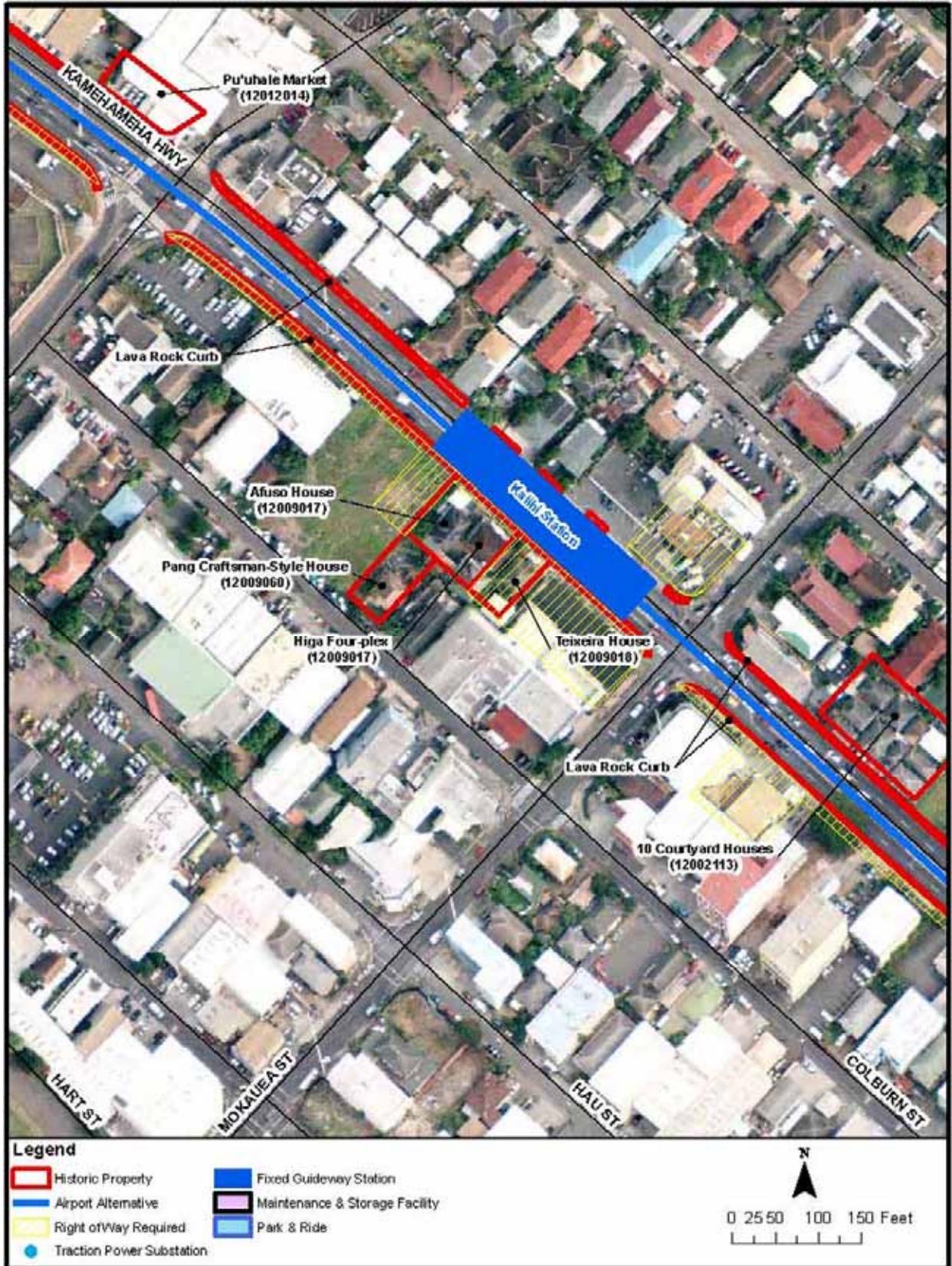


Figure 113. Pang Craftsman-Style House



Figure 114. Pang Craftsman-Style House, facing mauka/Koko Head to Dillingham Boulevard, property on right



Figure 115. Pang Craftsman-Style House, facing makai/‘Ewa from property along Colburn Street

5.47 10 Courtyard Houses

20 Kalihi Station sector—TMK 12002113
1808 Dillingham Boulevard

Property description

The 10 Courtyard Houses property comprises 10 single-story, plantation-style houses constructed in 1916 and 1927. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Kalihi Kai neighborhood in the early 1900s and under Criterion C as an example of a plantation-style dwelling set in a courtyard grouping.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the center of Dillingham Boulevard just makai of the 10 Courtyard Houses property (see Figure 116 and TMK as shown on Drawing HP019 in Appendix A). The mauka edge of the guideway would be approximately 25 feet from the makai edge of the NRHP boundary and the most makai houses. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the 10 Courtyard Houses.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial, light industrial, and residential properties. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the mauka edge of the roadway. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure just makai of the property and within the adjacent alignment of Dillingham Boulevard that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects

to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a collection of early 20th-century houses. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a collection of early 20th-century houses. Therefore, the Project would have no effect to the property's integrity of association.

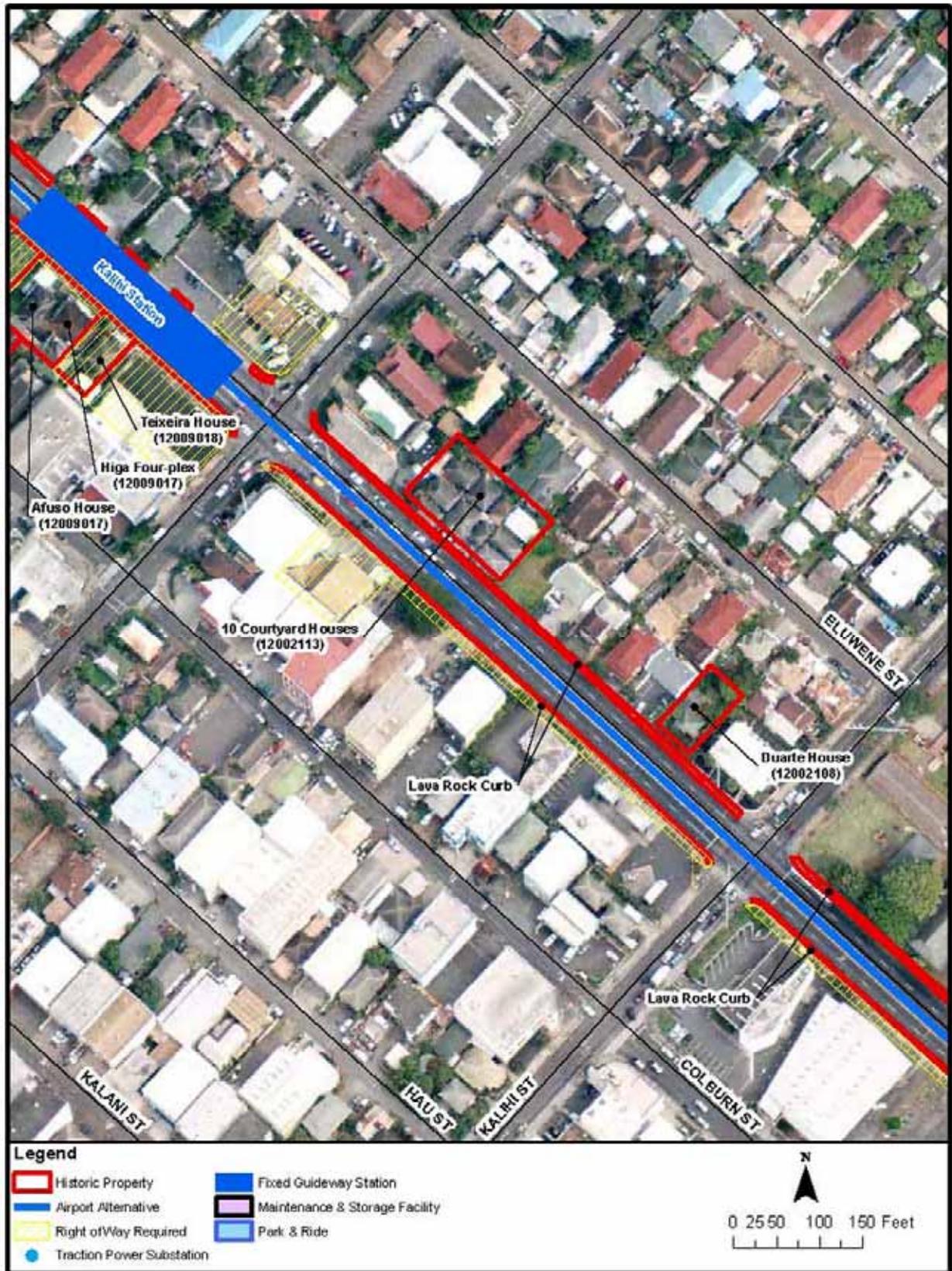


Figure 116. 10 Courtyard Houses



Figure 117. 10 Courtyard Houses, facing makai/'Ewa from property to Dillingham Boulevard



Figure 118. 10 Courtyard Houses, facing makai/Koko Head from property to Dillingham Boulevard

5.48 Duarte House

20 Kalihi Station sector—TMK 12002108
1720 Dillingham Boulevard

Property description

The Duarte House is a single-story, Craftsman-style residence constructed in 1925. The property was determined eligible for the NRHP under Criterion A for its association with the residential development of the Kalihi Kai neighborhood in the early 1900s and under Criterion C because it embodies the distinctive characteristics of, and is a good example of, a single-wall, pre-World War II, Craftsman-style house.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the center of Dillingham Boulevard just makai of the Duarte House (see Figure 119 and TMK as shown on Drawing HP019 in Appendix A). The mauka edge of the guideway would be approximately 20 feet from the makai edge of the NRHP boundary and the house. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Duarte House.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial, light industrial, and residential properties. These properties within the setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure just makai of the property and within the adjacent alignment of Dillingham Boulevard that would be visible from the makai portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features

within its setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century house. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century house. Therefore, the Project would have no effect to the property's integrity of association.

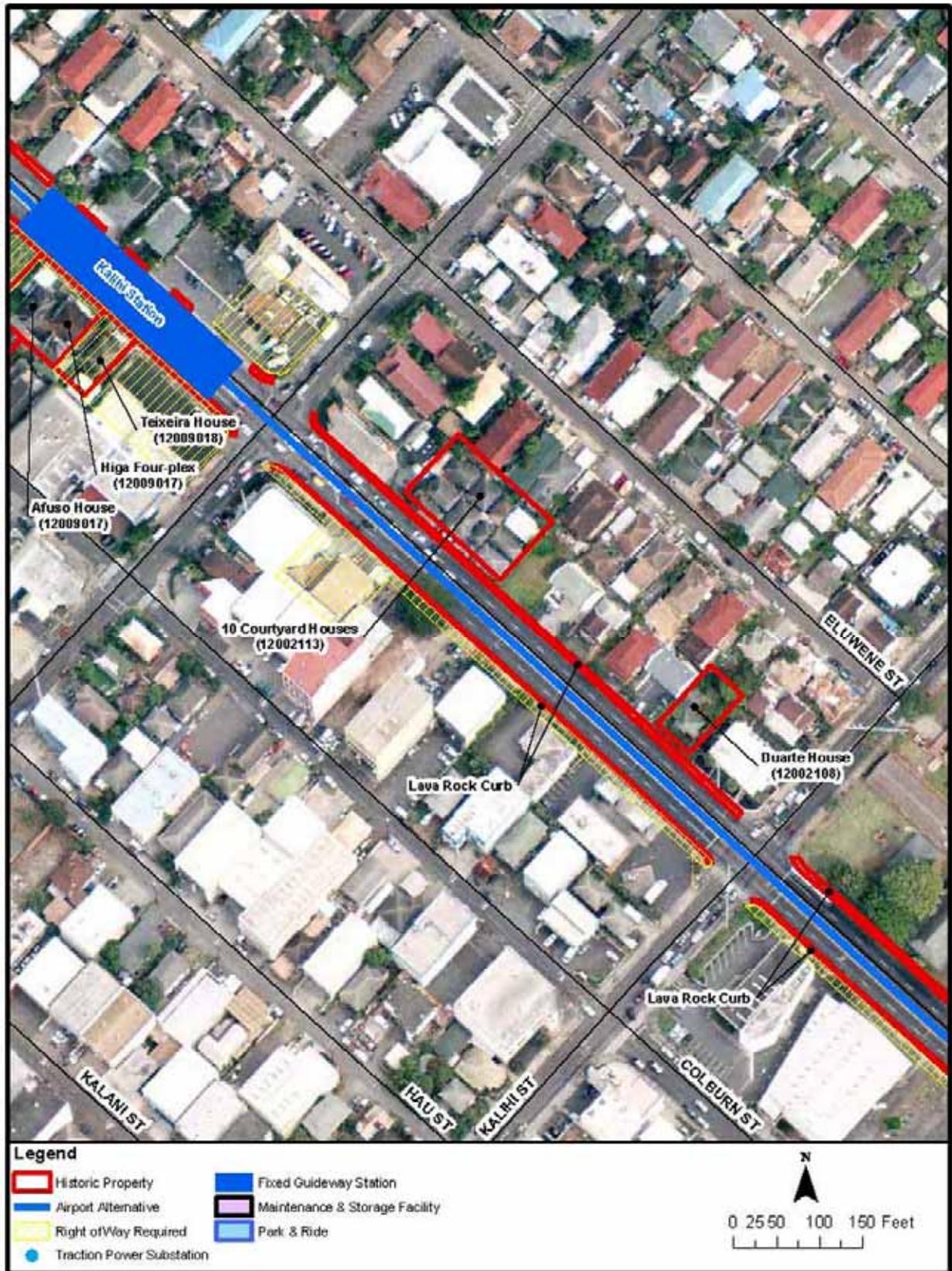


Figure 119. Duarte House



Figure 120. Duarte House, facing makai/Ewa from property to Dillingham Boulevard



Figure 121. Duarte House, facing makai/Koko Head from property to Dillingham Boulevard

5.49 Boulevard Saimin

20 Kalihi Station sector—TMK 15029060
1419 Dillingham Boulevard

Property description

Boulevard Saimin is a restaurant located in a two-story commercial building constructed in 1960. The property was determined eligible for the NRHP under Criterion A for its association with the commercialization of saimin, a plantation staple food in Hawai'i. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure just mauka of the building within which Boulevard Saimin is located (see Figure 122 and TMK as shown on Drawing HP019 in Appendix A). The makai edge of the guideway would be approximately 25 feet from the mauka edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 30 to 35 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Boulevard Saimin.

Location: No Adverse Effect. The Project would not remove the property from its historic location, but it would cause physical damage or destruction to a portion of the property. Construction would require an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard at this property; this right-of-way would be acquired from the mauka portion of the historic property. This acquisition would eliminate one vehicle parking space but would not alter any historic features of the property. Removal of the single parking space would not jeopardize the continued use of the commercial building within which the Boulevard Saimin restaurant is located. Therefore, the Project would have no adverse effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. The Project would require an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary of the historic property. However, this acquisition would not alter any historic design features of the property. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. The property is surrounded on all sides by non-historic commercial and light industrial properties. These properties within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure just mauka of the property and within the adjacent Dillingham Boulevard alignment that would be visible from the mauka portion of the property. Although the guideway's construction would introduce a new component to the property's setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historic features. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not alter any historically significant characteristics or features related to the workmanship used to create the property. The Project would require that an approximately 10-foot wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historic features. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not diminish the property's expression of its historic character. The Project would require that an approximately 10-foot wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historically significant features. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historically significant features. Therefore, the Project would have no effect to the property's integrity of association.

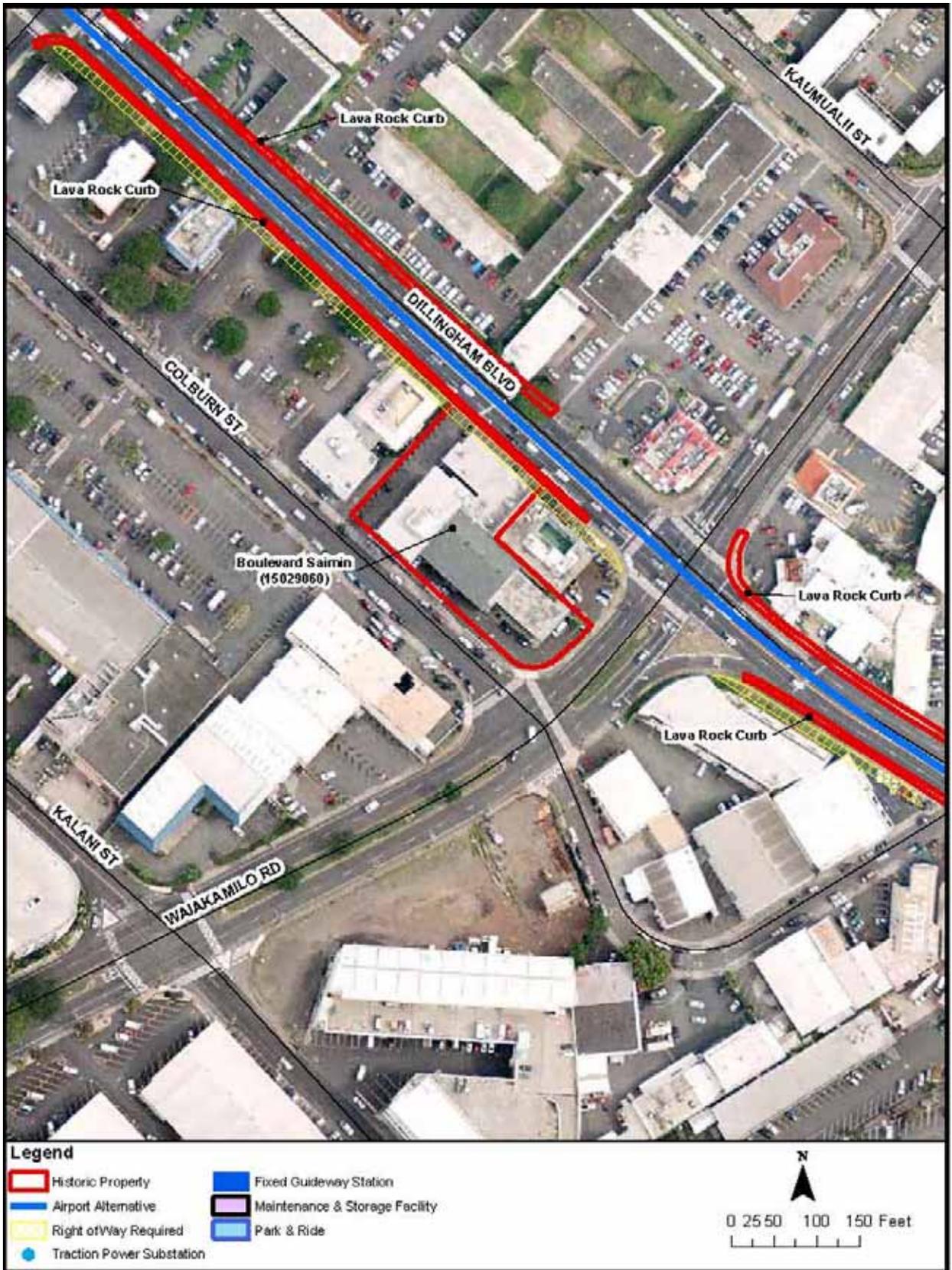


Figure 122. Boulevard Saimin



Figure 123. Boulevard Saimin, facing mauka/'Ewa from property to Dillingham Boulevard



Figure 124. Boulevard Saimin, facing mauka/Koko Head from property to Dillingham Boulevard

5.50 True Kamani Trees

21 Kapālama through 22 Iwilei Station sectors—TMK not applicable
Along Dillingham Boulevard from approximately Kapālama Drainage
Canal to Ka'aahi Street

Property description

The true kamani trees were planted along both sides of Dillingham Boulevard about 1934 and have matured; pruning for utilities has altered the canopies of many trees. The true kamani trees were determined eligible for the NRHP under Criterion A for their association with 1930s roadway infrastructure development and the history of street tree plantings in Honolulu.

Project description in vicinity of objects

The Project would include construction of an elevated, fixed-guideway rail structure within the center of Dillingham Boulevard. A transit station platform would be constructed just Koko Head of the Kapālama Drainage Canal (see Figure 125 and TMK as shown on Drawing HP020 in Appendix A). Between the Kapālama Drainage Canal and Ka'aahi Street, Dillingham Boulevard does not contain a median and would require widening along its makai edge to accommodate the guideway. This widening would require removal of the true kamani trees along the makai edge of Dillingham Boulevard. The design of the guideway along Dillingham Boulevard from Kapālama Drainage Canal to Ka'aahi Street would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 30 to 40 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-10 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the true kamani trees.

Location: Adverse Effect. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard between the Kapālama Drainage Canal and Ka'aahi Street. Therefore, the Project would have an adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard. This alteration would essentially eliminate the planned design of Dillingham Boulevard as a tree-lined street in this area. Therefore, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. Dillingham Boulevard is characterized by non-historic commercial and light industrial properties. These properties comprise the true kamani trees' setting but do not contribute to

their historic significance. However, removal of the true kamani trees from the makai side of the roadway and the guideway's placement within the center of Dillingham Boulevard would significantly alter the immediate setting of the trees that would be removed makai and those that would remain mauka following construction. The trees' immediate setting on both sides of Dillingham Boulevard would be significantly altered. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the trees are not a noise receptor, audible affects to this property were not evaluated. No atmospheric effects to this property were identified.

Materials: Adverse Effect. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard. Therefore, the Project would have an adverse effect to the property's integrity of materials.

Workmanship: Adverse Effect. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard. This would alter the design of the street tree plantings and the evidence of workmanship used to create the property. Therefore, the Project would have an adverse effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard. This would essentially eliminate the planned design of Dillingham Boulevard as a tree-lined street in this area. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter the historically significant visual relationships of the property and would obscure its historic appearance to an observer. The Project would require the removal of most of the true kamani trees on the makai edge of Dillingham Boulevard. This alteration would essentially eliminate the planned design of Dillingham Boulevard as a tree-lined street in this area. Therefore, the Project would have an adverse effect to the property's integrity of association.

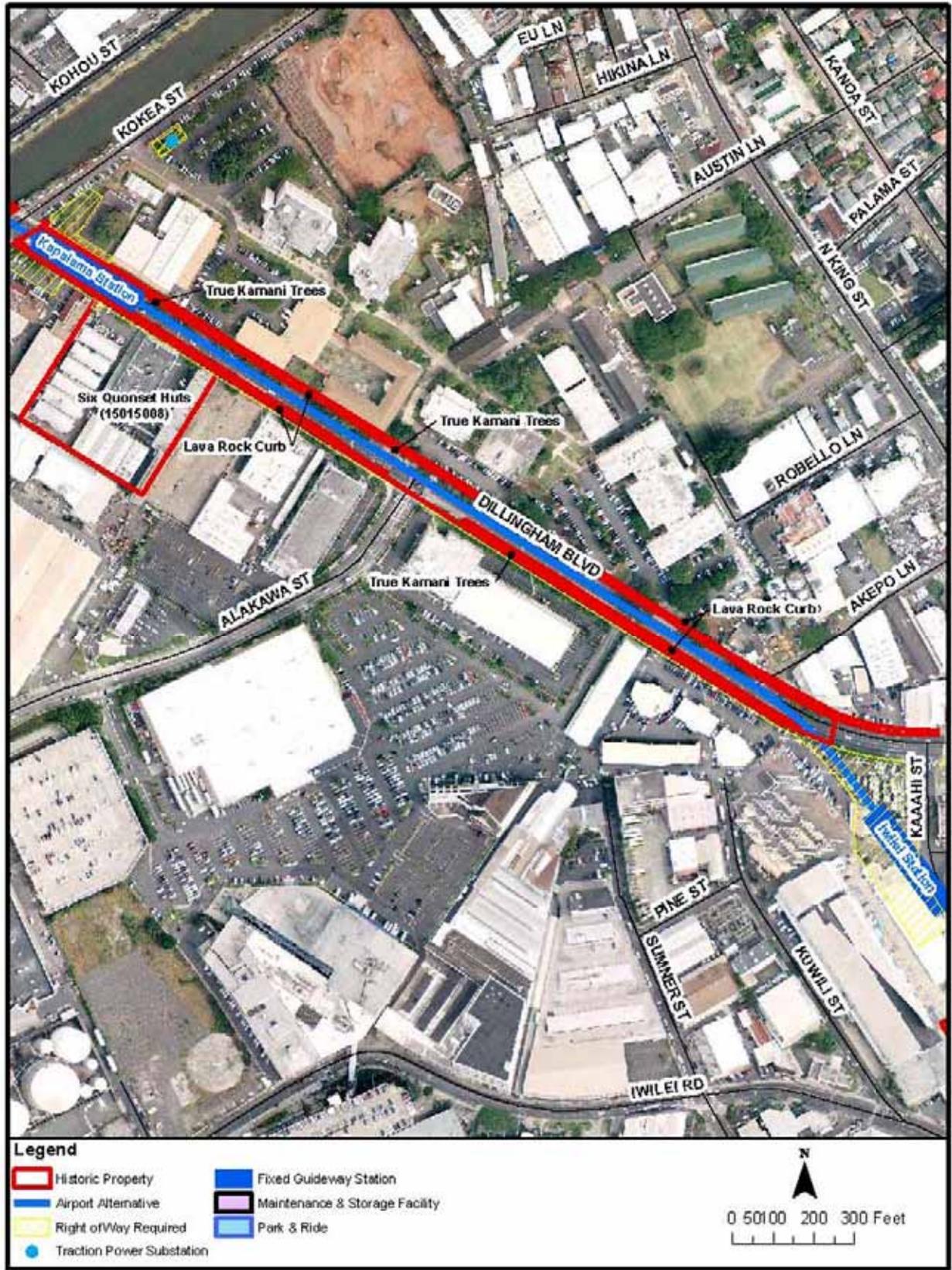


Figure 125. True Kamani Trees



Figure 126. True Kamani Trees, representative view, facing mauka/Koko Head along Dillingham Boulevard



Figure 127. True Kamani Trees, representative view, facing makai/Koko Head from along Dillingham Boulevard



Figure 128. Visual Simulation: Representative View along Dillingham Boulevard, looking 'Ewa near Honolulu Community College and Kapālama Station Area

5.51 Kapālama Canal Bridge

21 Kapālama Station sector—TMK not applicable
Dillingham Boulevard over Kapālama Drainage Canal

Property description

The Kapālama Canal Bridge is a five-span, reinforced-concrete, T-beam deck-girder bridge of 113 feet constructed in 1930. The property was determined eligible for the NRHP under Criterion A for its association with the transportation history of the area and the extension of Dillingham Boulevard and under Criterion C as an example of concrete bridge engineering and design in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure over the Kapālama Canal Bridge (see Figure 129 and Drawing HP020 in Appendix A). The guideway would span the length of the bridge. The existing bridge would require modification to accommodate both the guideway and existing traffic lanes. The bridge would be widened from 10 to 12 feet along its makai edge, and a median would be installed in the center of the bridge within which two bridge columns would be located. Policy requires that when substantial modification is made to an existing structure, then the entire structure must be improved to current design and safety standards; therefore, additional modifications, including improvement of the existing bridge rails, would be required. The mauka rail would be maintained but strengthened on the inside of the rail, and a new rail would be constructed on the makai side of the bridge that replicates the appearance of the existing rail from the outside of the bridge. A transit station platform would be constructed just Koko Head of the bridge. The guideway's design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 30 feet above the bridge. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-10 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Kapālama Canal Bridge.

Location: No Adverse Effect. The Project would not remove the property from its historic location over the Kapālama Canal. Although the Project would be constructed through a portion of the NRHP boundary and would require modifications to the bridge, construction would not cause the removal of the bridge. Therefore, the Project would have no adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would substantially alter the property's design elements and features. As described above, the Project would require modifications to the bridge's design to accommodate the guideway, to maintain the existing

number of travel lanes, and to meet current design standards. These changes include the widening of the bridge, creation of a median within which guideway columns would be placed, and the replacement and modification of the existing bridge rails. Although the bridge has been previously altered (seismically retrofitted in 2005), these additional modifications would further alter the bridge's design through widening, incorporation of guideway columns, and alterations to existing rails. Therefore, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary; within the NRHP boundary, the Kapālama Canal Bridge still spans that waterway. The bridge is surrounded by non-historic commercial development. Existing elevated structures include tall utility transmission poles carrying multiple transmission lines along the makai edge of the roadway and a relatively large communications transmission tower located makai of the property. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. As described above, the Project would include construction of an elevated, fixed-guideway rail structure over the center of the bridge and modifications to the bridge's design to accommodate the guideway, to maintain existing travel lanes, and to meet current design standards. These changes include the widening of the bridge, creation of a median within which guideway columns would be placed, and alterations to existing rails.

Although setting was not identified as a historically significant feature or characteristic of the property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction over the bridge would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the historic bridge and physical impacts to it would generally obscure the historic appearance of the bridge and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: Adverse Effect. The Project would substantially alter the physical elements that comprise the property. As described above, the Project would require modifications to the bridge's design to accommodate the guideway, to maintain existing travel lanes, and to meet current design standards. These changes include the widening of the bridge, creation of a median within which guideway columns would be placed, and alteration of the existing rails. Although the bridge has been previously altered (seismically retrofitted in 2005), these additional modifications would further alter the bridge's design and materials through widening, incorporation of guideway columns, and removal, replacement, and modification of existing rails. Therefore, the Project would have an adverse effect to the property's integrity of materials.

Workmanship: Adverse Effect. The Project would substantially alter historically significant characteristics and features related to the workmanship used to create the property. As described above, the Project would require modifications to the bridge's design to accommodate the guideway, to maintain existing travel lanes, and to meet current design standards. These changes include the widening of the bridge, creation of a median within which guideway columns would be placed, replacement of the makai rail, and alteration of the existing rails. Although additional research indicates that the bridge has been previously altered (seismically retrofitted in 2005), these additional modifications would alter the bridge's design and materials through widening; incorporation of guideway columns; and removal, replacement, and modification of existing rails. These alterations would obscure features related to the workmanship used to construct the bridge. Therefore, the Project would have an adverse effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction over the bridge and required modifications would obscure the property's historic appearance and immediate environment. The guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction over the bridge and required modifications would obscure the property's historic appearance and immediate environment. The guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

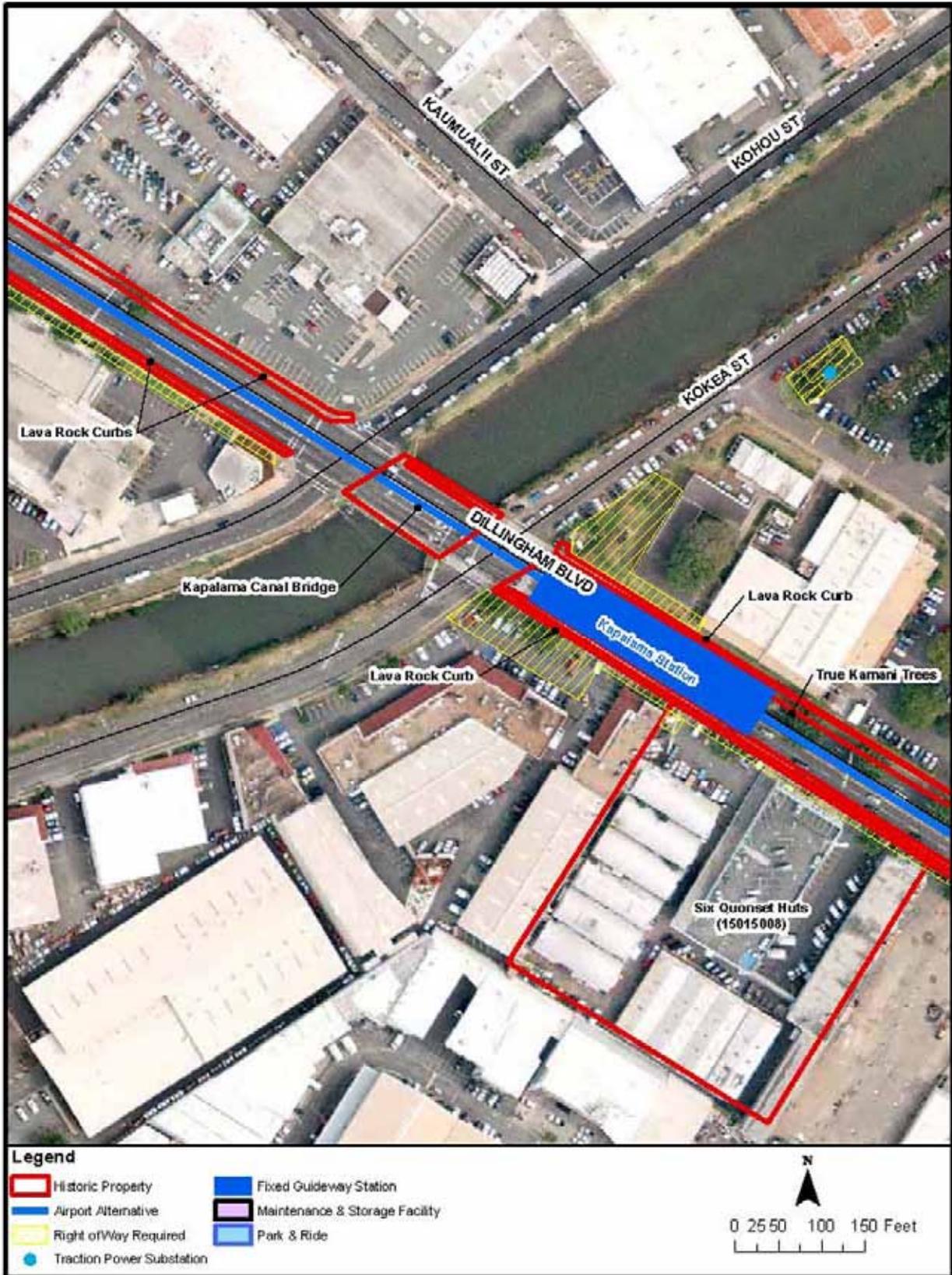


Figure 129. Kapālama Canal Bridge



Figure 130. Kapālama Canal Bridge, facing makai/Koko Head from Kōkea Street to bridge



Figure 131. Kapālama Canal Bridge, facing makai/'Ewa from bridge

5.52 Six Quonset Huts

21 Kapālama Station sector—TMK 15015008
1001 Dillingham Boulevard

Property description

The Six Quonset Huts property contains six 40-foot by 100-foot Quonset huts; a non-historic metal warehouse building is also located on the property Koko Head of the Quonset huts. The property was determined eligible for the NRHP under Criterion A because of its association with the reuse of former military buildings by small businesses and others on O‘ahu and under Criterion C because the buildings embody the distinctive characteristics of Quonset building.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the center of adjacent Dillingham Boulevard and a transit station platform just mauka of the Six Quonset Huts property (see Figure 132 and TMK as shown on Drawing HP020 in Appendix A). The makai edge of the guideway would be approximately 25 feet from the mauka edge of the NRHP boundary and approximately 80 feet from the mauka Quonset hut. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-16 in Appendix B and would be elevated approximately 30 to 35 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-1 and C-10 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Six Quonset Huts.

Location: No Adverse Effect. The Project would not cause the property’s removal from its historic location, but it would cause physical damage or destruction to a portion of the property. (The historic property has been previously moved, but its significance is directly tied to its relocation and adaptive reuse. The SHPD concurred with the eligibility determination, although listing moved properties is relatively rare.) Construction would require an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard at this property; this right-of-way would be required from the mauka portion of the historic property. A small area of additional right-of-way acquisition at the ‘Ewa corner of the property would extend makai up to approximately 25 feet. A portion of this acquisition would be converted to roadway and sidewalk use to accommodate installation of the median and guideway in Dillingham Boulevard.

The right-of-way acquisition may require reconfiguration of the existing parking and vehicle storage located along the mauka portion of the property at Dillingham Boulevard. However, the removal or alteration of this parking area would not affect the location of the Quonset huts (located approximately 50 feet from the makai edge

of the proposed right-of-way acquisition), the continued use of the buildings, or the continued commercial use of the parcel upon which they are located. Therefore, the Project would have no adverse effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary of the historic property. However, this acquisition would not alter any historic design features of the Quonset huts. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting. The property is surrounded on all sides by non-historic light industrial and commercial properties. These properties within the setting and viewshed do not contribute to the historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure mauka of the property and within the center of adjacent Dillingham Boulevard that would be visible from the mauka portion of the property and the property's parking lot. Although the guideway's construction would introduce a new component to the setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary of the historic property. However, this acquisition would not alter any historic design features of the Quonset huts. Therefore, the Project would have no effect to the property's integrity of materials. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary of the historic property. However, this acquisition would not alter any historic design features of the Quonset huts. Therefore, the Project would have no effect to the property's integrity of workmanship. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not diminish the property's expression of its historic character. The Project would require that an approximately 10-foot wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historically significant features of the relocated Quonset huts. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. The Project would require that an approximately 10-foot-wide strip of right-of-way be acquired along the makai edge of Dillingham Boulevard and within the NRHP boundary. However, this acquisition would not alter any historically significant features of the relocated Quonset huts. Therefore, the Project would have no effect to the property's integrity of association.



Figure 132. Six Quonset Huts



Figure 133. Six Quonset Huts, facing makai from Dillingham Boulevard to property



Figure 134. Six Quonset Huts, facing mauka/'Ewa from property to Dillingham Boulevard

5.53 Institute for Human Services/Tamura Building

22 Iwilei Station sector—TMK 15007033

536 Ka'aahi Street

Property description

The Institute for Human Services/Tamura Building is a three-story, International-style building constructed in 1968. The property was determined eligible for the NRHP under Criterion C as an example of an International-style building. Criterion G was not addressed as part of the eligibility assessment. Therefore, this property is not considered to be exceptionally important (supported by prior NRHP determinations for properties considered under Criterion G) but is instead considered eligible as a result of the effort to evaluate properties that will achieve the age of 50 years before project construction.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform immediately 'Ewa of the Institute for Human Services/Tamura Building and over Ka'aahi Street (see Figure 135 and TMK as shown on Drawing HP020 in Appendix A). The Koko Head edge of the guideway would be approximately 10 to 20 feet from the 'Ewa edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-17 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-11 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Institute for Human Services/Tamura Building.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: Adverse Effect. This property's setting was not identified as a historically significant feature. The property is surrounded by a mix of contemporary and non-historic light industrial properties. These properties located within the setting and viewshed do not contribute to the historic significance of this property. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure and transit station

platform just 'Ewa of the property and over Ka'aahi Street that would be visible from, and obstruct views from, the 'Ewa portion of the property and building.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's and passenger platform's placement in close proximity to the building would significantly alter the immediate physical and visual setting of the property. The guideway's proximity to the building would generally obscure its historic appearance and its relationship to its surroundings. The modern structure would also become the prominent element in the setting, and views from all 'Ewa windows would be significantly altered or obstructed. Therefore, because the Project would significantly alter and diminish the immediate setting, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's introduction would significantly alter the property's immediate setting and obscure its historic appearance. Although the building would remain physically intact, the proximity of the guideway and passenger platform to the building would substantially obscure the visual relationship between the building and its surroundings and thereby obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. The guideway's introduction would significantly alter the property's immediate setting and obscure its historic appearance. Although the building would remain physically intact, the proximity of the guideway and passenger platform to the building would substantially obscure the visual relationship between the building and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

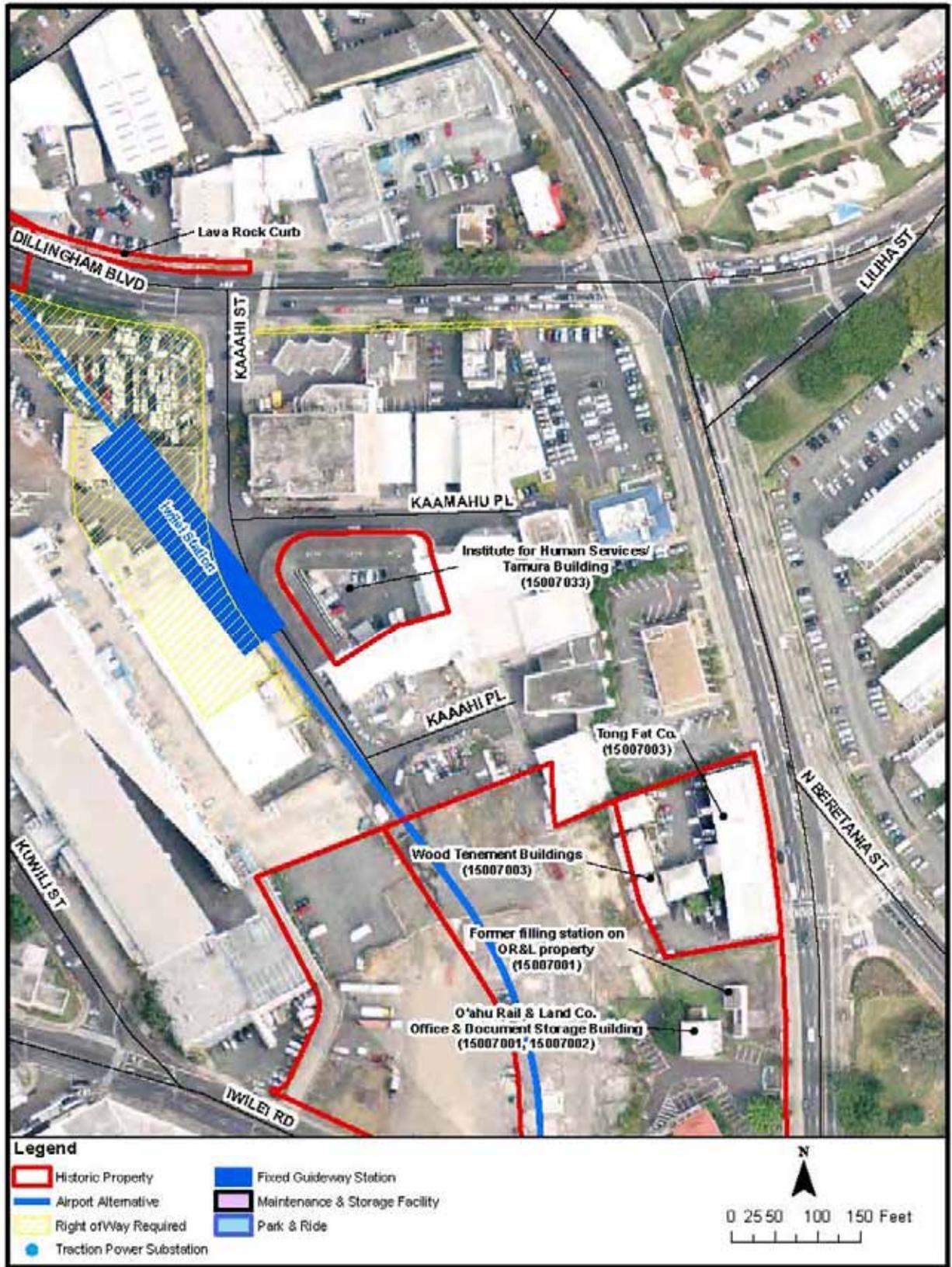


Figure 135. Institute for Human Services/Tamura Building



Figure 136. Institute for Human Services/Tamura Building, facing mauka along Ka'aahi Street, property on right



Figure 137. Institute for Human Services/Tamura Building, facing makai from property along Ka'aahi Street

5.54 Tong Fat Co.

22 Iwilei Station sector—TMK 15007003
425 North King Street

Property description

The Tong Fat Co. building is a two-story commercial building constructed in 1920. The property was determined eligible for the NRHP under Criterion A for its association with the development of the 'A'ala neighborhood and under Criterion C as a good local example of a large commercial building of the 1920s.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure behind and 'Ewa of the Tong Fat Co. building (see Figure 138 and TMK as shown on Drawing HP020 in Appendix A). The Koko Head edge of the guideway would be approximately 150 feet from the 'Ewa edge of the NRHP boundary and approximately 250 feet from the building. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Tong Fat Co.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature of the property. Non-historic commercial and residential properties are located mauka and Koko Head of the property. These properties within the setting and viewshed do not contribute to the historic significance of the Tong Fat Co. building. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure 250 feet behind and 'Ewa of the building that would be visible from the 'Ewa portion or rear of the property. However, the property would be substantially screened from the guideway by buildings and vegetation immediately behind the Tong Fat Co. building and within its TMK parcel that would not be affected by the Project. Although the guideway would alter portions of the property's setting and viewshed, its presence would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the

property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century commercial building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 138. Tong Fat Co.



Figure 139. Tong Fat Co., facing 'Ewa from King Street to property



Figure 140. Tong Fat Co., facing 'Ewa from rear of property to Iwilei Road

5.55 Wood Tenement Buildings behind Tong Fat Co.

22 Iwilei Station sector—TMK 15007003

425 North King Street

Property description

The Wood Tenement Buildings behind Tong Fat Co. are a group of three, two-story four-plex residential buildings and one single-story duplex constructed in 1914. The property was determined eligible for the NRHP under Criterion A for its association with the development of the 'A'ala neighborhood and under Criterion C as an example of the typical grouping and construction of early 20th-century tenement buildings in Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure behind and 'Ewa of the Wood Tenement Buildings behind Tong Fat Co. (see Figure 141 and TMK as shown on Drawing HP020 in Appendix A). The Koko Head edge of the guideway would be approximately 150 feet from the 'Ewa edge of the NRHP boundary and from the residential buildings. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Wood Tenement Buildings behind Tong Fat Co.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature of the property. The buildings overlook the defunct former rail yard of the O'ahu Railway & Land Co. Terminal Building. This site has been cleared, and no rail-related features are extant. Non-historic industrial buildings are located 'Ewa of the cleared area and constitute the buildings' viewshed. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure 150 feet behind and 'Ewa of the buildings that would be visible from the 'Ewa portion of the property. Although the guideway would introduce a new component to the property's setting and viewshed, its presence would not alter any historically significant views or visual features. Furthermore, no audible or

atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a group of early 20th-century residential buildings. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a group of early 20th-century residential buildings. Therefore, the Project would have no effect to the property's integrity of association.



Figure 141. Wood Tenement Buildings behind Tong Fat Co.



Figure 142. Wood Tenement Buildings behind Tong Fat Co., facing mauka/'Ewa to Ka'aahi Street, property on right



Figure 143. Wood Tenement Buildings behind Tong Fat Co., facing 'Ewa from property to Iwilei Road

5.56 O'ahu Railway & Land Co. Terminal Building

22 Iwilei Station sector—TMKs 15007001, 15007002
355 North King Street

Property description

The O'ahu Railway & Land Co. (OR&L) Terminal Building is a two-story, Spanish Mission Revival-style building constructed in 1925. The property was determined eligible for the NRHP under Criterion A for its association with the OR&L, an important force in the development of O'ahu, and under Criterion C as an example of a Spanish Mission Revival-style building with high artistic value.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure behind and 'Ewa of the OR&L Terminal Building (see Figure 144 and TMKs as shown on Drawings HP020 and HP021 in Appendix A). The guideway would pass through an existing easement on the TMK parcel (15007001) containing the OR&L Terminal Building. The area behind the building once contained its associated rail yard; however, this portion of the property has been cleared and partially paved, and no rail-related structures or equipment are extant. The Koko Head edge of the guideway would be approximately 100 to 150 feet from the building. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the OR&L Terminal Building.

Location: Adverse Effect. The Project would not remove the property from its historic location, but it would cause physical damage or destruction to a portion of the property. Construction would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. This area was a portion of the property's rail yard but has been cleared of rail-related structures and is now open space. As noted in the documentation for the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008), the "open feeling around the buildings is similar to that of its past." Thus, this open ground behind the terminal buildings is considered a historically significant feature of the property, and it would be interrupted and reduced by the guideway's introduction. Therefore, because the historic property would be reduced by the use of easement within the property's NRHP boundary for location of the guideway, the Project would have an adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would not alter any of the property's design elements or features but would require the use of a 56-foot-wide access easement

within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. The guideway would be constructed and located within this easement. Thus, the guideway would be located within the area of the former rail yard, which is now open space, and has been determined to be a historically significant feature of the property. This feature is directly related to the overall design of the property and the spatial relationships between both extant and removed historic buildings and features associated with the OR&L Terminal Building. Therefore, because the guideway's construction would alter and obscure the open area that contained the rail yard, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. A non-historic, high-rise residential complex is located makai of the property, and non-historic industrial properties are located 'Ewa. These properties within the property's setting and viewshed do not contribute to its historic significance. The Project would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be placed at this location.

Although the 'Ewa portion of the property has been cleared and rail-related features removed, this open space area is considered a historically significant feature of the property. The elevated guideway's placement would interrupt and obscure this feature, and the guideway would become the prominent feature within the viewshed from the terminal building 'Ewa to the rail yard area. This change to the property's setting within the NRHP boundary would essentially eliminate the historically significant open space 'Ewa of the terminal building and the spatial relationship of the terminal building to this area. Therefore, because a historically significant feature within the setting and NRHP boundary would be altered and obscured, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Adverse Effect. The Project would not alter any physical elements that comprise the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary of the historic property. However, no historically significant materials related to the OR&L Terminal Building or the defunct rail yard or railway would be altered. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. However, no historically significant characteristics or features related to the workmanship used to create the property would be altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's placement would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be placed at this location. Although the terminal building would remain physically intact, the elevated guideway's placement would interrupt and obscure the historically significant open space 'Ewa of the building. This change within the NRHP boundary would obscure the property's historic character by essentially eliminating the historically significant open space 'Ewa of the terminal building and the spatial relationship of the terminal building to this area. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's placement would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be introduced at this location. Although the terminal building would remain physically intact, placement of the elevated guideway would interrupt and obscure the historically significant open space located 'Ewa of the building. This change within the NRHP boundary would obscure the property's historic character by essentially eliminating the historically significant open space 'Ewa of the terminal building and the spatial relationship of the terminal building to this area. Therefore, the Project would have an adverse effect to the property's integrity of association.

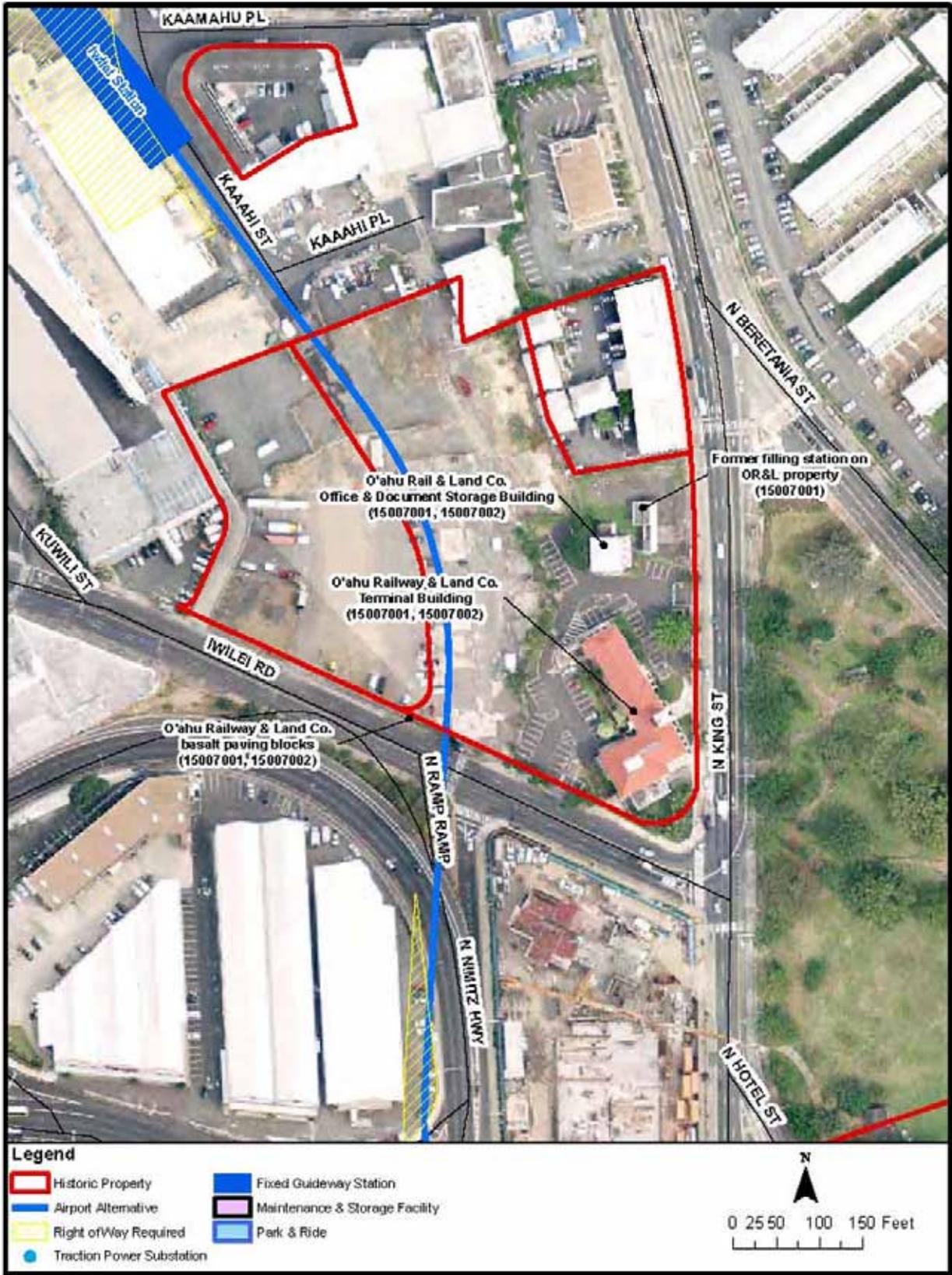


Figure 144. O'ahu Railway & Land Co. Terminal Building



Figure 145. O'ahu Railway & Land Co. Terminal Building, facing 'Ewa along Iwilei Road, property on right



Figure 146. O'ahu Railway & Land Co. Terminal Building, facing makai from property along King Street

5.57 O‘ahu Railway & Land Co. Office and Document Storage Building

22 Iwilei Station sector—TMKs 15007001, 15007002
355 North King Street

Property description

The O‘ahu Railway & Land Co. (OR&L) Office and Document Storage Building is a two-story, Colonial Revival-style building constructed in 1914. The property was determined eligible for the NRHP under Criterion A for its association with the OR&L, an important force in the development of O‘ahu, and under Criterion C as a rare surviving example of Colonial Revival architecture in Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure behind and ‘Ewa of the OR&L Office and Document Storage Building (see Figure 147 and TMKs as shown on Drawings HP019 and HP020 in Appendix A). The guideway would pass through an existing easement on the TMK parcel (15007001) containing both the OR&L Terminal Building (see above) and the Office and Document Storage Building. The ‘Ewa portion of the property once contained the rail yard associated with the terminal building; however, this portion of the property has been cleared and partially paved, and no rail-related structures or equipment are extant. The Koko Head edge of the guideway would be approximately 150 feet from the Office and Document Storage Building. The guideway’s design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the OR&L Office and Document Storage Building.

Location: Adverse Effect. The Project would not remove the property from its historic location or directly alter the property’s use, but it would cause physical damage or destruction to a portion of the property. Construction would require the use of a 56-foot-wide access easement within the rear or ‘Ewa portion of TMK 15007001 within which the guideway would be built. This area was a portion of the property’s rail yard but has been cleared of rail-related structures and is now open space. As noted in the documentation for the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008), the “open feeling around the buildings is similar to that of its past.” Thus, this open ground behind the building is considered a historically significant feature of the property, and it would be interrupted and reduced by the guideway’s introduction. Therefore, because the historic property would be reduced by the use of easement within the property’s NRHP boundary for location of the guideway, the Project would have an adverse effect to the property’s integrity of location.

Design: Adverse Effect. The Project would not alter any of the property's design elements or features but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. The guideway would be constructed and located within this easement. Thus, the guideway would be located within the area of the former rail yard which is now open space and has been determined to be a historically significant feature. This feature is directly related to the overall design of the property and the spatial relationships between both extant and removed historic buildings and features associated with the OR&L Terminal Building. Therefore, because the guideway's construction would alter and obscure the open area that contained the rail yard, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary. A non-historic, high-rise residential complex is located makai of the property, and non-historic industrial properties are located 'Ewa. These properties within the property's setting and viewshed do not contribute to its historic significance. The Project would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be placed at this location.

Although the 'Ewa portion of the property has been cleared and rail-related features removed, this area remains open and is considered a historically significant feature. The elevated guideway's placement would interrupt and obscure this feature, and the guideway would become the prominent feature within the viewshed from the Office and Document Storage Building 'Ewa to the rail yard area. This change to the property's setting within the NRHP boundary would essentially eliminate the historically significant open space 'Ewa of the terminal building and the spatial relationship of the OR&L buildings related to this area. Therefore, because a historically significant feature within the property's setting and NRHP boundary would be altered and obscured, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Adverse Effect. The Project would not alter any physical elements that comprise the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. However, the OR&L Office and Document Storage Building would not be altered. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. However, no historically significant characteristics or features related to the workmanship used to create the property would be altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's placement would require the use of a 56-foot-

wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be placed at this location. Although the Office and Document Storage Building would remain physically intact, the elevated guideway's placement would interrupt and obscure the historically significant open space located 'Ewa of the building. This change within the NRHP boundary would obscure the property's historic character by essentially eliminating the historically significant open space 'Ewa of the Office and Document Storage building and the spatial relationship of the OR&L buildings to this area. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's placement would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 and within the NRHP boundary. An elevated, fixed guideway rail structure would be placed at this location. Although the Office and Document Storage Building would remain physically intact, the elevated guideway's placement would interrupt and obscure the historically significant open space 'Ewa of the building. This change within the NRHP boundary would obscure the property's historic character by essentially eliminating the historically significant open space 'Ewa of the Office and Document Storage Building and the spatial relationship of the OR&L buildings to this area. Therefore, the Project would have an adverse effect to the property's integrity of association.

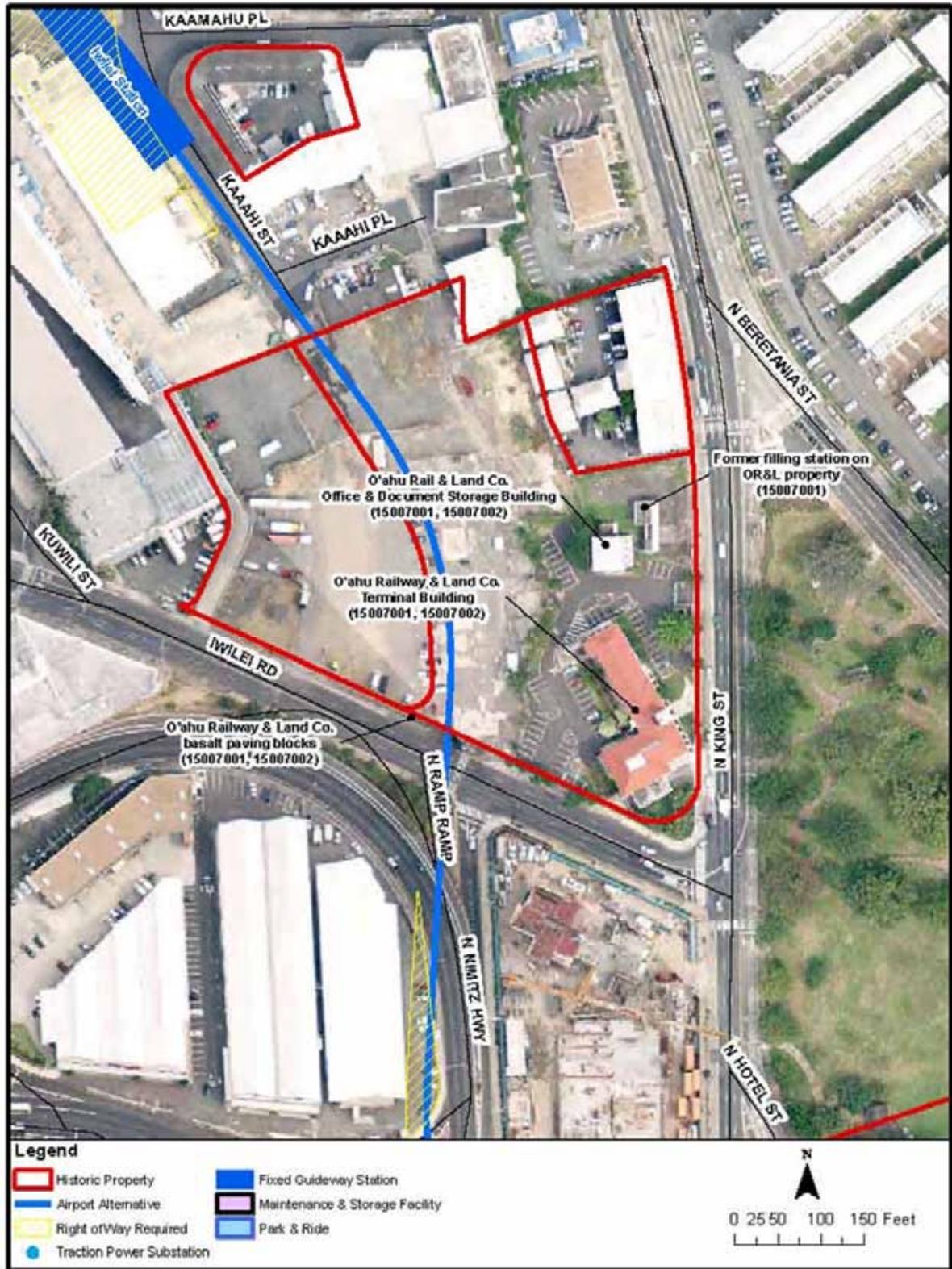


Figure 147. O'ahu Railway & Land Co. Office and Document Storage Building



Figure 148. O'ahu Railway & Land Co. Office and Document Storage Building, facing mauka/'Ewa from King Street to property



Figure 149. O'ahu Railway & Land Co. Office and Document Storage Building, facing 'Ewa from property to Ka'aahi Street

5.58 Former Filling Station on OR&L Property

22 Iwilei Station sector—TMK 15007001
355 North King Street

Property description

The former filling station on OR&L property is a single-story, flat-roofed, masonry building constructed in 1940. The property was determined eligible for the NRHP under Criterion A for its association with the development of 'A'ala neighborhood. Although it is located on the OR&L property, it is not known to be related to the other OR&L buildings and is not part of that historic complex.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure behind and 'Ewa of the former filling station on OR&L property (see Figure 150 and TMK as shown on Drawing HP021 in Appendix A). The guideway would pass through an existing easement on the TMK parcel (15007001) containing the OR&L Terminal Building (see above), the Office and Document Storage Building (see above), and the former filling station on OR&L property. The 'Ewa portion of the property once contained the rail yard associated with the Terminal Building; however, this portion of the property has been cleared and partially paved, and no rail-related structures or equipment are extant. The Koko Head edge of the guideway would be approximately 225 feet from the former filling station. The guideway's design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the former filling station on OR&L property.

Location: No Effect. The Project would not remove the property from its historic location, but it would cause physical damage or destruction to a portion of the TMK parcel on which the building is located. Construction would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. This area was a portion of the OR&L rail yard but has been cleared of rail-related structures and is now open space. However, the filling station building is separated and visually screened from this area of the property by the OR&L Office and Document Storage Building, and it is not known to be historically related to the rail yard feature. The portion of the property containing the filling station building is oriented to North King Street and would not be affected by use of the easement. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Adverse Effect. The Project would not alter any of the property's design elements or features but would require the use of a 56-foot-wide access easement

within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated from this area by the OR&L Office and Document Storage Building and is not known to be historically related to the rail yard feature. The portion of the property containing the filling station building would not be affected by use of the easement, and the filling station's design elements would not be altered. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature. A non-historic, high-rise residential complex is located and visible makai of the property, and non-historic residential properties are located Koko Head. These properties within the property's setting and viewshed do not contribute to its historic significance. The Project would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated and visually screened from this area of the property by the OR&L Office and Document Storage Building, and it is not known to be historically related to the rail yard. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property would be substantially visually screened from the Project area, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated from this area by the OR&L Office and Document Storage Building and is not known to be historically related to the rail yard. The portion of the property containing the filling station building would not be affected by use of the easement, and the filling station's physical features would not be altered. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated from this area by the OR&L Office and Document Storage Building and is not known to be historically related to the rail yard. The portion of the property containing the filling station building would not be affected by use of the easement, and no historically significant characteristics or features related to the workmanship used to create the building would be altered. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character but would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated from this area by the OR&L Office and Document Storage Building and is not known to be historically related to the rail yard. The portion of the property containing the filling station building would not be affected by use of the easement, and the property's historically significant physical features would not be altered. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. The Project would require the use of a 56-foot-wide access easement within the rear or 'Ewa portion of TMK 15007001 within which the guideway would be built. However, the filling station building is separated from this area by the OR&L Office and Document Storage Building and is not known to be historically related to the rail yard. The portion of the property containing the filling station building would not be affected by use of the easement, and the property's historically significant features would not be altered. Therefore, the Project would have no effect to the property's integrity of association.

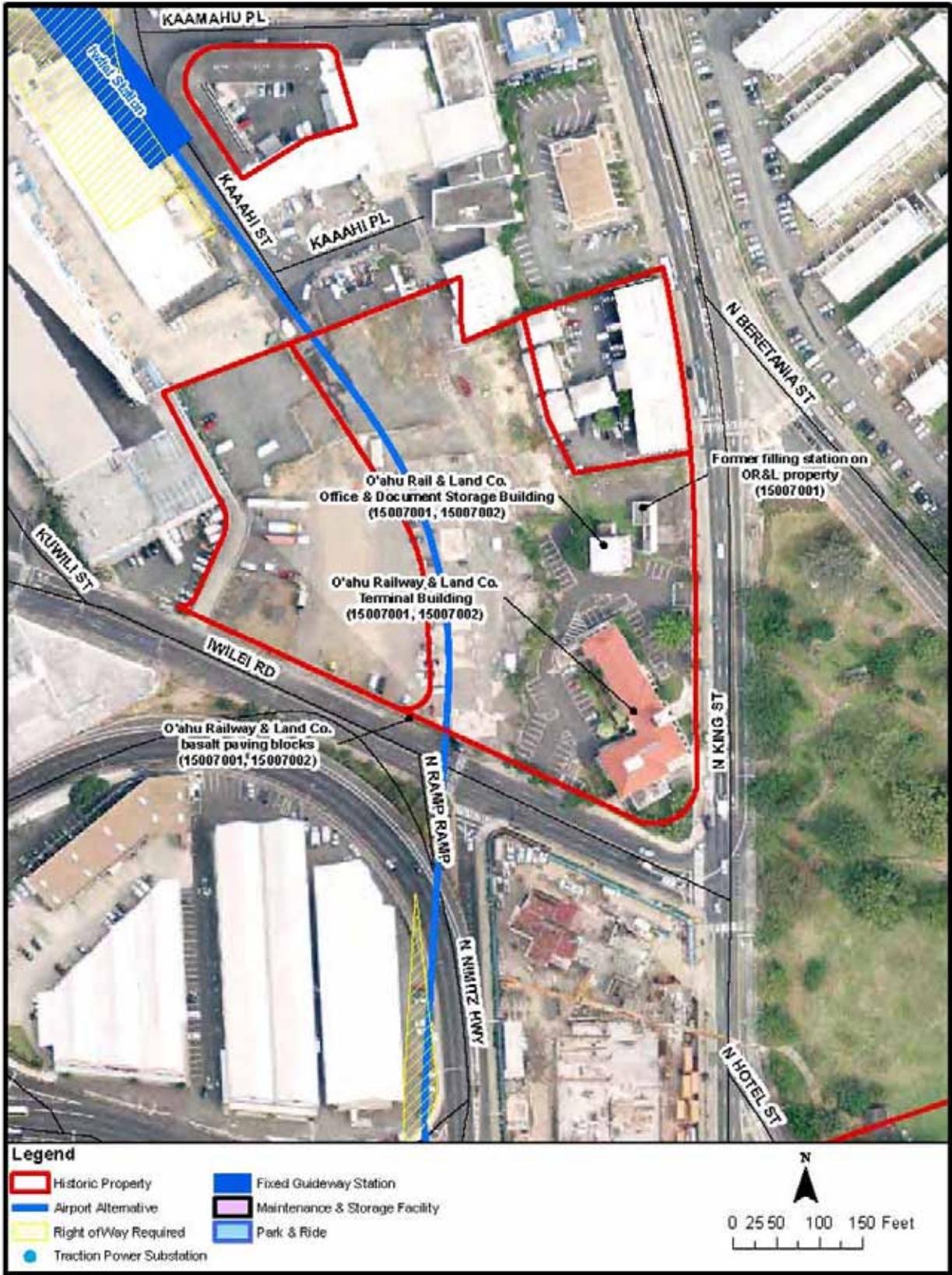


Figure 150. Former Filling Station on OR&L Property



Figure 151. Former Filling Station on OR&L Property, facing 'Ewa from King Street to property



Figure 152. Former Filling Station on OR&L Property, facing mauka/'Ewa from King Street to Ka'aahi Street, property on left

5.59 O‘ahu Railway & Land Co. Basalt Paving Blocks

22 Iwilei Station sector—TMKs 15007001, 15007002
870 Iwilei Road

Property description

The O‘ahu Railway & Land Co. (OR&L) basalt paving blocks are roughly shaped, rectangular basalt paving block installed along Iwilei Road circa 1914. These objects were determined eligible for the NRHP under Criterion A for their association with the development of Honolulu’s roadway infrastructure, under Criterion C because they demonstrate the distinctive method of using basalt paving blocks in road construction in Honolulu, and under Criterion D as a rare source of information on the technology of street paving in early Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure through an access easement on the ‘Ewa portion of TMK parcel 15007001 containing the OR&L Terminal Building (see above) and the OR&L basalt paving blocks (see Figure 153 and TMKs as shown on Drawings HP020 and HP021 in Appendix A). The ‘Ewa portion of the property once contained the rail yard associated with the terminal building; however, this portion of the property has been cleared and partially paved, and no rail-related structures or equipment are extant. The guideway would be oriented mauka to makai in this area and would cross over a portion of the remaining paving blocks. The guideway’s design in this area would correspond to that shown in the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 45 feet above grade and the paving blocks.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the OR&L basalt paving blocks.

Location: No Effect. The Project would not remove the property from its historic location. The Project would include construction of an elevated, fixed-guideway rail structure through an access easement on the ‘Ewa portion of the TMK parcel containing the OR&L Terminal Building and the paving blocks. The guideway is oriented mauka to makai in this area and would cross over the extant OR&L basalt paving blocks. No construction activity would disturb the remaining basalt paving blocks. No support columns would be located in the area containing the remaining blocks, and no other ground-disturbing activity would occur in this area. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features but would include construction of an elevated, fixed-guideway rail structure that would cross over the extant OR&L basalt paving blocks. No construction activity would disturb the remaining basalt paving blocks. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property's setting was not identified as a historically significant feature. A non-historic, high-rise residential complex is located makai of the property, and non-historic light industrial properties are located 'Ewa. The Project would introduce an elevated, fixed guideway rail structure that would cross over the location of the extant OR&L basalt paving blocks. Although the guideway's placement would alter the property's setting, the guideway would not alter or obscure any historically significant views or visual features related to the extant paving blocks. The view from the blocks to the OR&L Terminal Building would remain essentially unchanged. Therefore, because no historically significant or visual features within the property's setting were identified or would be obscured, the Project would have no adverse effect to the property's integrity of setting. In addition, because the paving blocks are neither noise receptors nor sensitive to atmospheric elements, audible and atmospheric effects were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property but would include construction of an elevated, fixed-guideway rail structure that would cross over the extant OR&L basalt paving blocks. No construction activity would disturb the remaining basalt paving blocks. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not alter any historically significant characteristics or features related to the workmanship used to create the property. The Project would include construction of an elevated, fixed-guideway rail structure that would cross over the extant OR&L basalt paving blocks. No construction activity would disturb the remaining basalt paving blocks. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as early 20th-century paving blocks. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as early 20th-century paving blocks. Therefore, the Project would have no effect to the property's integrity of association.

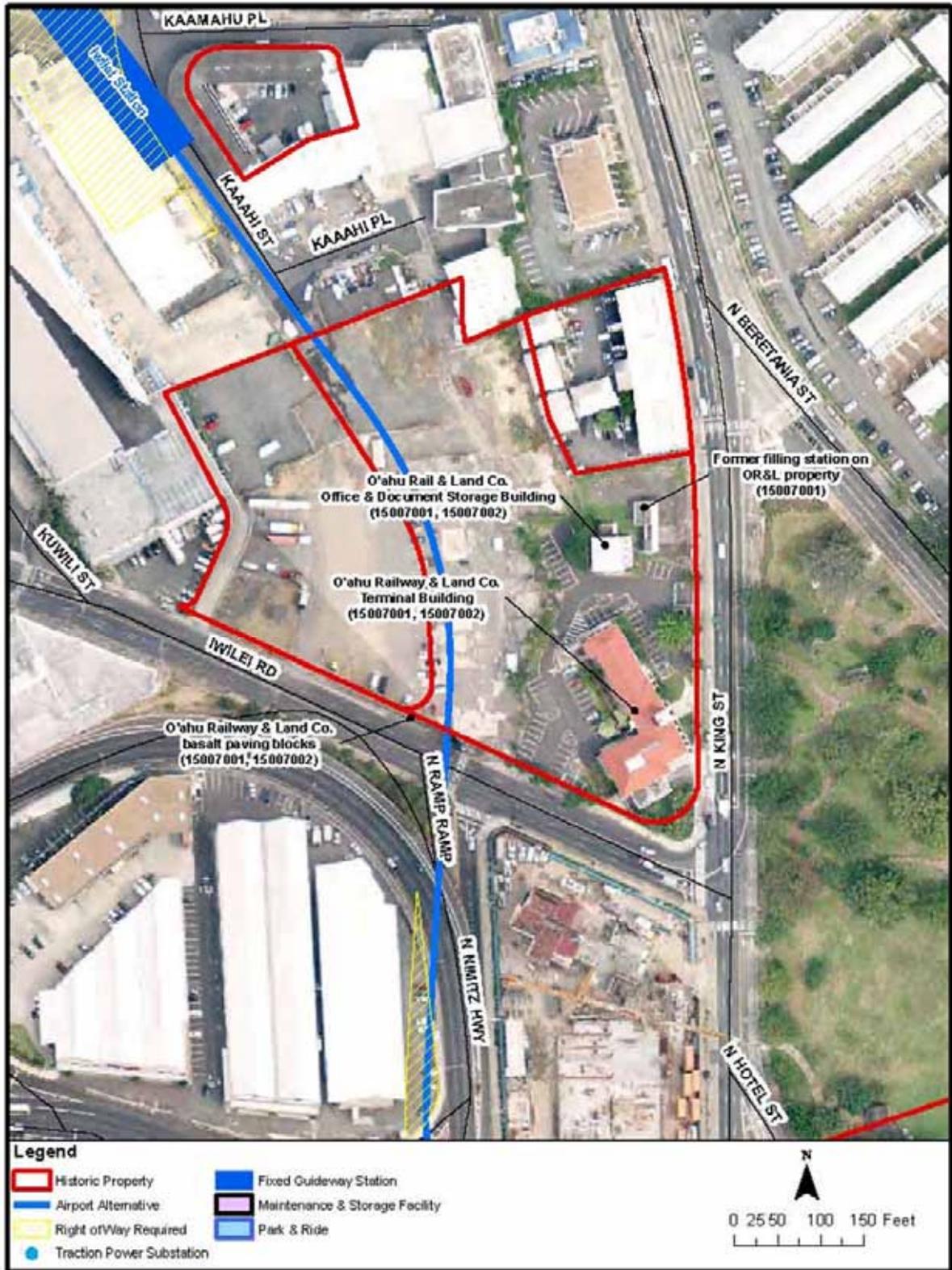


Figure 153. O'ahu Railway & Land Co. Basalt Paving Blocks



Figure 154. O'ahu Railway & Land Co. Basalt Paving Blocks, facing 'Ewa along Iwilei Road



Figure 155. O'ahu Railway & Land Co. Basalt Paving Blocks, detail of basalt pavers

5.60 Nu‘uanu Stream Bridge

23 Chinatown Station sector—TMK not applicable
Nimitz Highway over Nu‘uanu Stream

Property description

The Nu‘uanu Stream Bridge is a concrete bridge constructed in 1932. The property was determined eligible for the NRHP under Criterion A for its association with the transportation history of the waterfront and under Criterion C as a late example of a concrete bridge with solid parapet design that incorporates unusual molded detailing and a rounded top rail.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the median space of the Nu‘uanu Stream Bridge pair (see Figure 156 and Drawing HP021 in Appendix A). The guideway would span the length of the bridge; no columns would be constructed within the median of the bridge spans. The mauka span is the historic Nu‘uanu Stream Bridge. The guideway would overhang portions of the interior lane of the Nu‘uanu Stream Bridge. A passenger station would be located just Koko Head of the historic bridge, and a traction power substation would be located just ‘Ewa and mauka of the bridge. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-18 in Appendix B and would be elevated approximately 30 to 35 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-12 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Nu‘uanu Stream Bridge.

Location: No Effect. The Project would not cause the property’s removal from its historic location. Although the Project would be constructed over a portion of the NRHP boundary, construction would not cause the bridge to be removed or physically altered. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. The guideway’s construction would not physically alter the bridge or any of its design characteristics. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting outside of its NRHP boundary; within the NRHP boundary, the Nu‘uanu Stream Bridge still spans that waterway. Non-historic commercial and light industrial properties are located ‘Ewa of the bridge; a non-historic bridge is immediately makai of the historic bridge; and non-historic high-rise development is located mauka and Koko Head of the

bridge. The non-historic expansion of Nimitz Highway has also altered the property's historic setting. These properties and features within the property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would include construction of an elevated, fixed-guideway rail structure within the median of the two Nu'uaniu Stream Bridge spans and over the interior lane of the mauka historic Nu'uaniu Stream Bridge span.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's construction within the median of the bridge spans and over portions of the historic Nu'uaniu Stream Bridge span would significantly alter the immediate physical and visual setting. The guideway's proximity to the historic bridge would generally obscure the historic appearance of the bridge and its relationship to its surroundings, and the modern structure would become the prominent element in the property's setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. In addition, because the bridge is neither a noise receptor nor sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not alter any historically significant characteristics or features related to the workmanship used to create the bridge. Although the Project would be constructed over a portion of the NRHP boundary, construction would not physically alter the bridge. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway's placement would significantly alter the property's immediate setting. Furthermore, the guideway's placement immediately within the median of the bridge spans and partially over the historic Nu'uaniu Stream Bridge span would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings and thereby also obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway's introduction would significantly alter the property's immediate setting. Furthermore, the guideway's construction immediately within the median of the bridge spans and partially over the historic Nu'uaniu Stream Bridge span would obscure the property's historic appearance and immediate environment. Although the bridge would remain physically intact, the guideway's proximity to the bridge would substantially obscure the visual relationship between the bridge and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

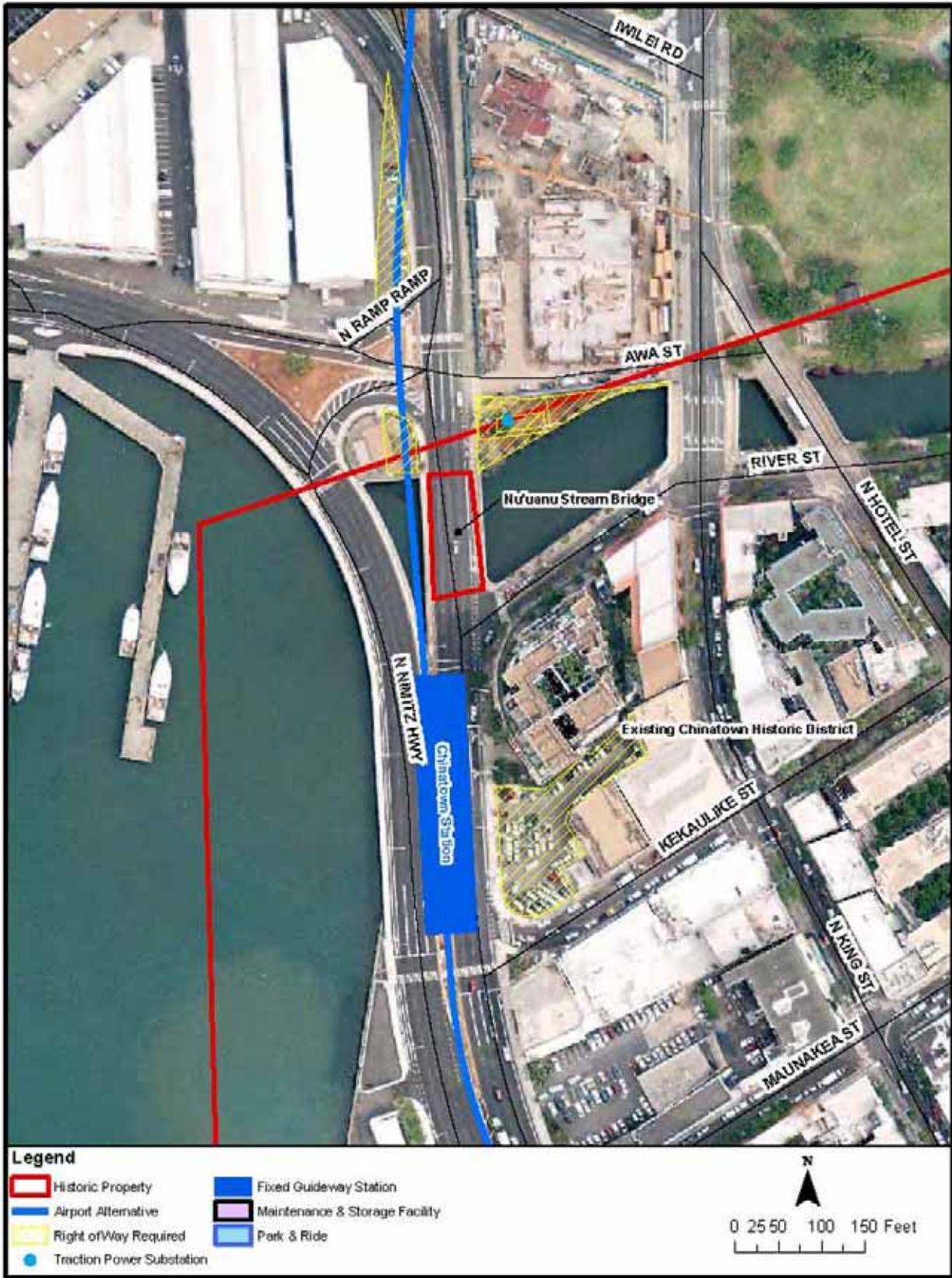


Figure 156. Nu'uanu Stream Bridge



Figure 157. Nu'uanu Stream Bridge, facing 'Ewa along Nimitz Highway to bridge



Figure 158. Nu'uanu Stream Bridge, facing 'Ewa along Nimitz Highway to bridge



Figure 159. Visual Simulation: View from King Street Bridge makai to Nu'uaniu Stream Bridge and Chinatown Station Area

5.61 Chinatown Historic District

23 Chinatown Station sector—TMK plats 17002, 17003, 17004
Roughly bounded by Nu‘uanu Stream, Beretania Street, Nu‘uanu Avenue,
and Honolulu Harbor

Property description

The Chinatown Historic District encompasses approximately 36 acres near Nu‘uanu Stream and Honolulu Harbor and just ‘Ewa of Downtown Honolulu. The area derives its historical significance from its central role in the life of the local Chinese community, including its commerce, architecture, and institutions. The immigrant Chinese community began to settle in this area in the early 19th century. Visually, the historic district is noted for its many historic shop buildings that contain public commercial spaces as well as private residences. Because of a series of catastrophic fires, most of the historic buildings date to the early 20th century or later. The NRHP nomination notes that Chinatown is “one of the few areas of Honolulu which has maintained a sense of identity as a community over the years.” The Chinatown Historic District was listed on the NRHP in 1973.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway along and within the approximate makai boundary of the Chinatown Historic District (see Figure 160 and Drawing HP021 in Appendix A). A transit station platform would be constructed just Koko Head of Nu‘uanu Stream. The guideway’s design in this area would correspond to the typical sections illustrated as Figures B-18, B-19, and B-20 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-12 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Chinatown Historic District.

Location: No Adverse Effect. The Project would not remove the property from its historic location but would cause physical damage or destruction to a portion of the historic district. The elevated guideway would be located within the median of Nimitz Highway within which support columns would be constructed; however, the median of the highway is not a historically significant feature of the Chinatown Historic District. To accommodate the transit station platform, all surface parking associated with TMK 17002026 would be acquired; this parcel lies within the Chinatown Historic District and contains two historic buildings that contribute to the historic district (Holau Market/Chinatown Marketplace, constructed in 1936; and Wing Loy Market, constructed in 1954). Most surface parking in Chinatown is on-street but includes some pay lots, such as one located on TMK 17002026, that serve areas of the neighborhood rather than specific buildings. Although acquisition of the surface

parking on TMK 17002026 may alter vehicular access at this location, it would not reduce pedestrian access, which would likely increase because of its proximity to the passenger station. Buildings in this area would remain viable for commercial use and would retain access to vehicular and pedestrian traffic via Kekaulike Street. Therefore, the Project would have no adverse effect to the property's integrity of location.

Design: Adverse Effect. The Project would alter historically significant design features within the Chinatown Historic District. As noted in the documentation for the *Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report* (RTD 2008):

The makai boundary of the district expresses the importance of Chinatown's connection with the harbor and its historic ties to the waterfront, a factor of great importance in its origin and evolution. "The major reason for its [Chinatown's] early development and continuous history as a commercial area was due to the close proximity to Honolulu Harbor" (Riconda 1973 and NRHP nomination form).

Because the Chinatown neighborhood developed in relationship and adjacent to the harbor, the area's street grid is oriented to the waterfront. The series of parallel streets from Nu'uaniu Stream to Nu'uaniu Avenue are oriented mauka to makai to provide access from the neighborhood and its businesses to the harbor and its related structures, including piers. These mauka to makai streets are not interrupted. Although Nimitz Highway has been widened, vehicle and pedestrian access to the harbor is intact. Although placement of the elevated guideway along Nimitz Highway would not interrupt access to the harbor from Chinatown, it would represent a prominent design feature that would partially visually separate the neighborhood from the waterfront. Therefore, because the guideway would alter a historic design relationship of the Chinatown Historic District, the Project would have an adverse effect to the property's integrity of design.

Setting: Adverse Effect. In the Project area within the Chinatown Historic District's NRHP boundary, the property does not retain a high degree of integrity of setting. Only one intact historic property fronts Nimitz Highway, and the highway has been expanded. The remaining properties fronting Nimitz Highway consist of non-historic high-rise buildings and surface parking lots. The waterfront appears to remain intact and still retains historic structures, including buildings, piers, and harbor walls. The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway along and within the approximate makai boundary of the Chinatown Historic District, as well as construction of a transit station platform just Koko Head of Nu'uaniu Stream.

The Chinatown neighborhood developed in relationship and adjacent to the harbor, and the area's street grid is oriented to the waterfront. Views from the makai portion of the historic district to the waterfront via mauka-to-makai oriented streets are essentially uninterrupted. This physical and visual connection between the Chinatown Historic District and the harbor is a historically significant feature of the historic district. Introduction of the elevated guideway and the station platform would visually interrupt and partially block views from areas within the Chinatown Historic District makai to the

harbor, and would introduce a design element within the NRHP boundary and the property's makai setting that is out of character with its historic setting. No audible or atmospheric effects to properties within the historic district were identified. Therefore, because historically significant visual features within the property's setting and NRHP boundary would be altered and obscured, the Project would have an adverse effect to the property's integrity of setting.

Materials: No Adverse Effect. The Project would not alter any historically significant physical materials that comprise the property but would include construction of an elevated, fixed-guideway rail structure within the Nimitz Highway alignment and within the approximate makai boundary of the historic district. The current design and material composition of Nimitz Highway is not a historically significant characteristic or feature of the historic district. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would not alter any historically significant characteristics or features related to the workmanship used to create the property but it would include construction of an elevated, fixed-guideway rail structure within the Nimitz Highway alignment and within the approximate makai boundary of the historic district. The existing composition of Nimitz Highway is not a historically significant characteristic or feature of the historic district. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The Chinatown neighborhood developed in relationship and adjacent to the harbor, and the area's street grid is oriented to the waterfront. Views from the makai portion of the historic district to the waterfront via mauka-to-makai oriented streets are essentially uninterrupted. This physical and visual connection between the Chinatown Historic District and the harbor is a historically significant feature of the historic district. Introduction of the elevated guideway and the station platform would visually interrupt and partially block views from areas within the Chinatown Historic District makai to the harbor. The guideway would substantially obscure the visual relationship between the neighborhood and the harbor and thereby obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The Chinatown neighborhood developed in relationship and adjacent to the harbor, and the area's street grid is oriented to the waterfront. Views from the makai portion of the historic district to the waterfront via mauka-to-makai oriented streets are essentially uninterrupted. This physical and visual connection between the Chinatown Historic District and the harbor is a historically significant feature of the historic district. Introduction of the elevated guideway and the station platform would visually interrupt and partially block views from areas within the Chinatown Historic District makai to the harbor. The guideway would partially obscure the visual relationship between the neighborhood and the harbor. Therefore, the Project would have an adverse effect to the property's integrity of association.

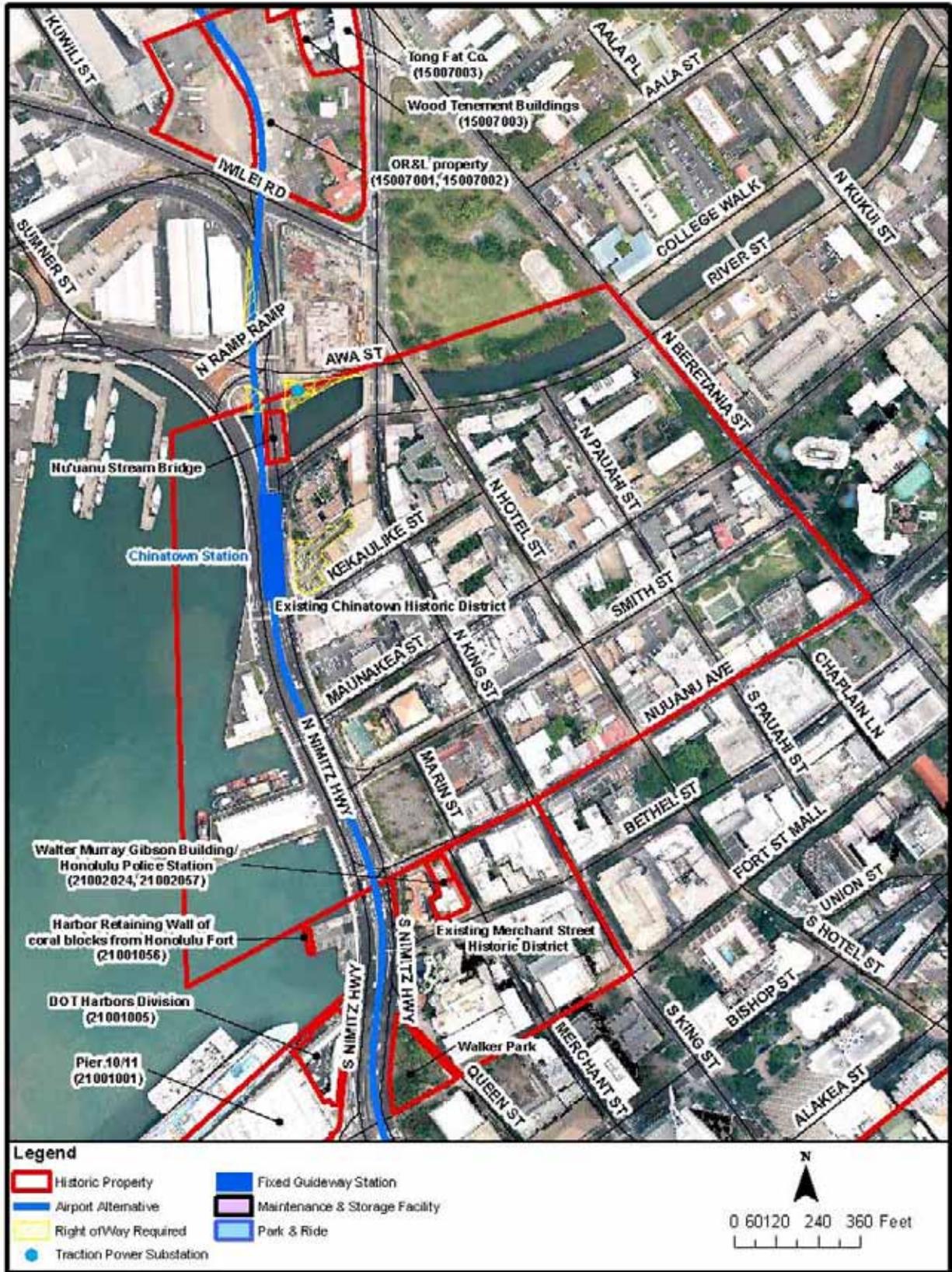


Figure 160. Chinatown Historic District



Figure 161. Chinatown Historic District, facing 'Ewa along Nimitz Highway from Kekaulike Street



Figure 162. Chinatown Historic District, facing Koko Head along Nimitz Highway from Kekaulike Street



Figure 163. Chinatown Historic District, facing makai along Maunakea Street



Figure 164. Chinatown Historic District, facing mauka/Ewa from Smith Street along Nimitz Highway



Figure 165. Visual Simulation: View within Chinatown Historic District, O'ahu Market at King Street, looking makai



Figure 166. Visual Simulation: View within Chinatown Historic District, Maunakea Street looking makai

5.62 Harbor Retaining Wall of Coral Blocks from Honolulu Fort
23 Chinatown Station sector—TMK 21001056
Pier 12 at Honolulu Harbor

Property description

The harbor retaining wall is comprised of coral blocks of varying sizes from Honolulu Fort (demolished 1857) that were reused to construct a harbor retaining wall circa 1859. The property was determined eligible for the NRHP under Criterion D as a primary source of information on early harbor retaining wall and wharf construction in Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway mauka of the harbor retaining wall (see Figure 167 and Drawing HP021 in Appendix A). The makai edge of the guideway would be approximately 175 feet from the mauka edge of the NRHP boundary and from the structure. The guideway's design in this area would correspond to the typical section illustrated as Figure B-19 in Appendix B and would be elevated approximately 35 to 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Harbor retaining wall of coral blocks from Honolulu Fort.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property is predominantly submerged. Above the waterline, it does not retain integrity of setting, and the property's setting was not identified as a historically significant feature. Non-historic high-rise development and the expanded alignment of Nimitz Highway are located mauka of the property and in the project area. Historic buildings and structures within the harbor are not associated with this property's period of significance. These properties and features within the property's setting do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified. The Project would introduce an elevated, fixed guideway rail structure mauka of the property and within the center of Nimitz Highway that might be visible from the top of the harbor wall. Although the guideway's placement could alter the property's mauka setting and viewshed, the guideway's presence would not alter any historically significant views or visual features. Therefore, because the property

does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting. In addition, because the harbor wall is neither a noise receptor nor known to be sensitive to atmospheric elements, audible and atmospheric effects to this property were not evaluated.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-nineteenth-century harbor retaining wall. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-19th-century harbor retaining wall. Therefore, the Project would have no effect to the property's integrity of association.

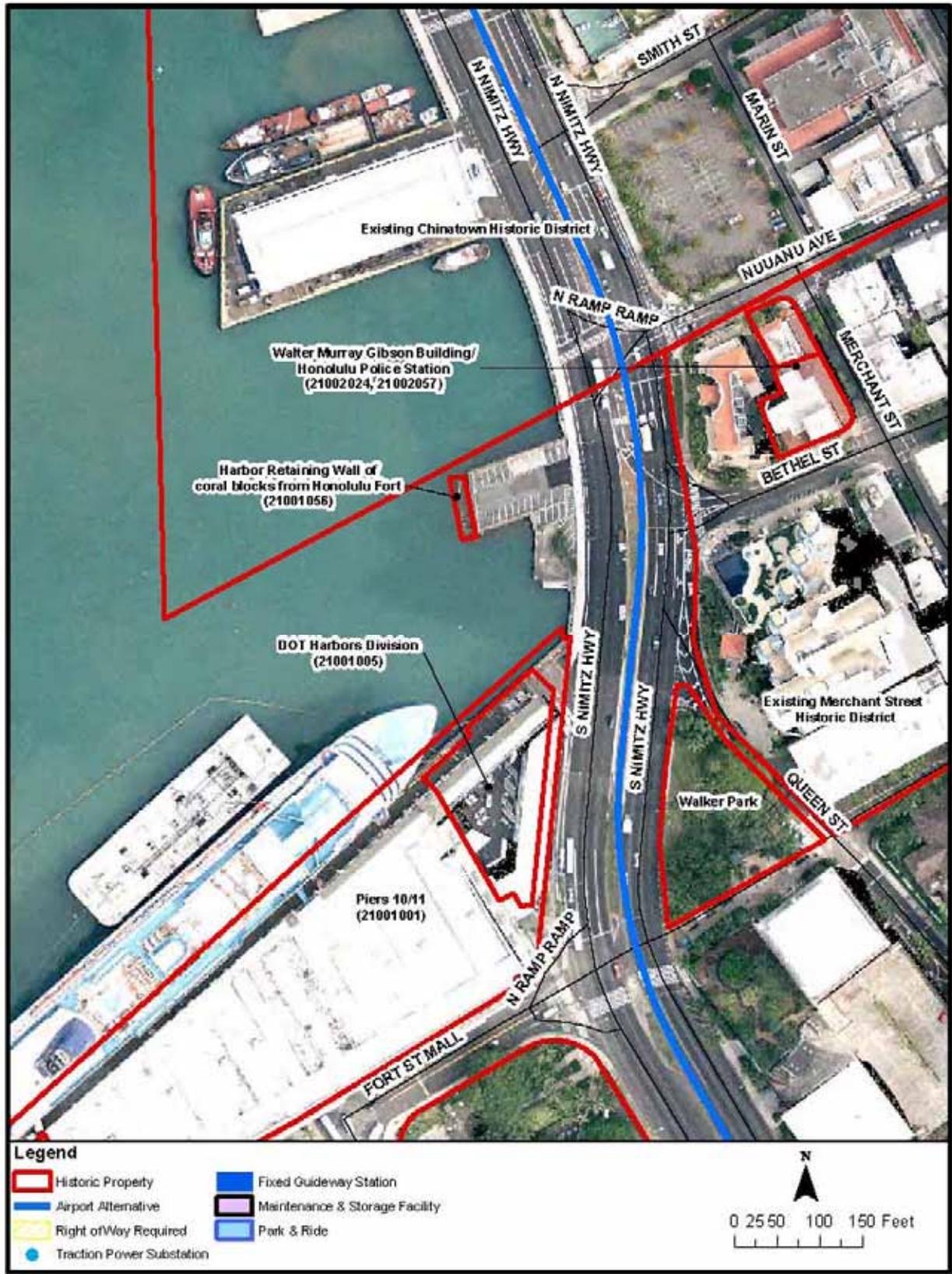


Figure 167. Harbor Retaining Wall of Coral Blocks from Honolulu Fort



Figure 168. Harbor Retaining Wall of Coral Blocks from Honolulu Fort, facing makai to property from Nimitz Highway at Bethel Street



Figure 169. Harbor Retaining Wall of Coral Blocks from Honolulu Fort, facing makai to property from Nimitz Highway at Nu'uanu Avenue

5.63 Merchant Street Historic District (including Walter Murray Gibson Building/Honolulu Police Station)

23 Chinatown and 24 Downtown Station sectors—TMKs 21002057,
21002024
842 Bethel Street

Property description

The Walter Murray Gibson Building/Honolulu Police Station is the only contributing resource within the Merchant Street National Register Historic District identified as being in the Project's APE. Although the historic district's boundaries still encompass properties along Nimitz Highway, the existing properties on this roadway are non-historic and do not contribute to the historic significance of the district. The Mediterranean-style, reinforced-concrete Walter Murray Gibson Building/Honolulu Police Station building consists of four stories and was constructed in 1930 and 1939. The property was also independently evaluated as eligible for the NRHP under Criterion A for its association with the history of the Honolulu Police Department and under Criterion C as an excellent example of Hawaiian Mediterranean-style architecture of the 1930s.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway makai of the Walter Murray Gibson Building/Honolulu Police Station, a contributing historic resource within the Merchant Street Historic District (see Figure 170 and Drawing HP021 in Appendix A). The mauka edge of the guideway would be approximately 150 feet from the makai edge of the historic district's NRHP boundary and from the former Police Station building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-20 in Appendix B and would be elevated approximately 35 to 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the Walter Murray Gibson Building/Honolulu Police Station, the only contributing resource within the Merchant Street Historic District located within the Project's APE, and thus there would be **No Adverse Effect** to the historic district.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the historic district's NRHP

boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting within or outside of the historic district's NRHP boundary. Although the historic district's boundaries still encompass properties along Nimitz Highway, the existing properties on this roadway are non-historic high-rise development; these buildings are located makai of the Walter Murray Gibson Building/Honolulu Police Station. Additional non-historic and high-rise development is located in the vicinity and within the viewshed of the historic district. These properties within the NRHP boundary, setting, and viewshed do not contribute to the historic significance of this property. The Project would introduce an elevated, fixed guideway rail structure makai of the property and within the center of the Nimitz Highway alignment two blocks makai of the Walter Murray Gibson Building/ Honolulu Police Station. The guideway would only be visible within views makai along North Bethel Street from the Koko Head sidewalk adjacent to that building. Although the guideway's placement would alter a portion of the property's setting and viewshed, its presence would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting in the project area and no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the historic district's NRHP boundary that would alter the Walter Murray Gibson Building/Honolulu Police Station's historic character as a Hawaiian Mediterranean-style building of the 1930s. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the historic district's NRHP boundary that would obscure the Walter Murray Gibson Building/Honolulu Police Station's historic appearance to an observer of this Hawaiian Mediterranean-style building of the 1930s. Therefore, the Project would have no effect to the property's integrity of association.

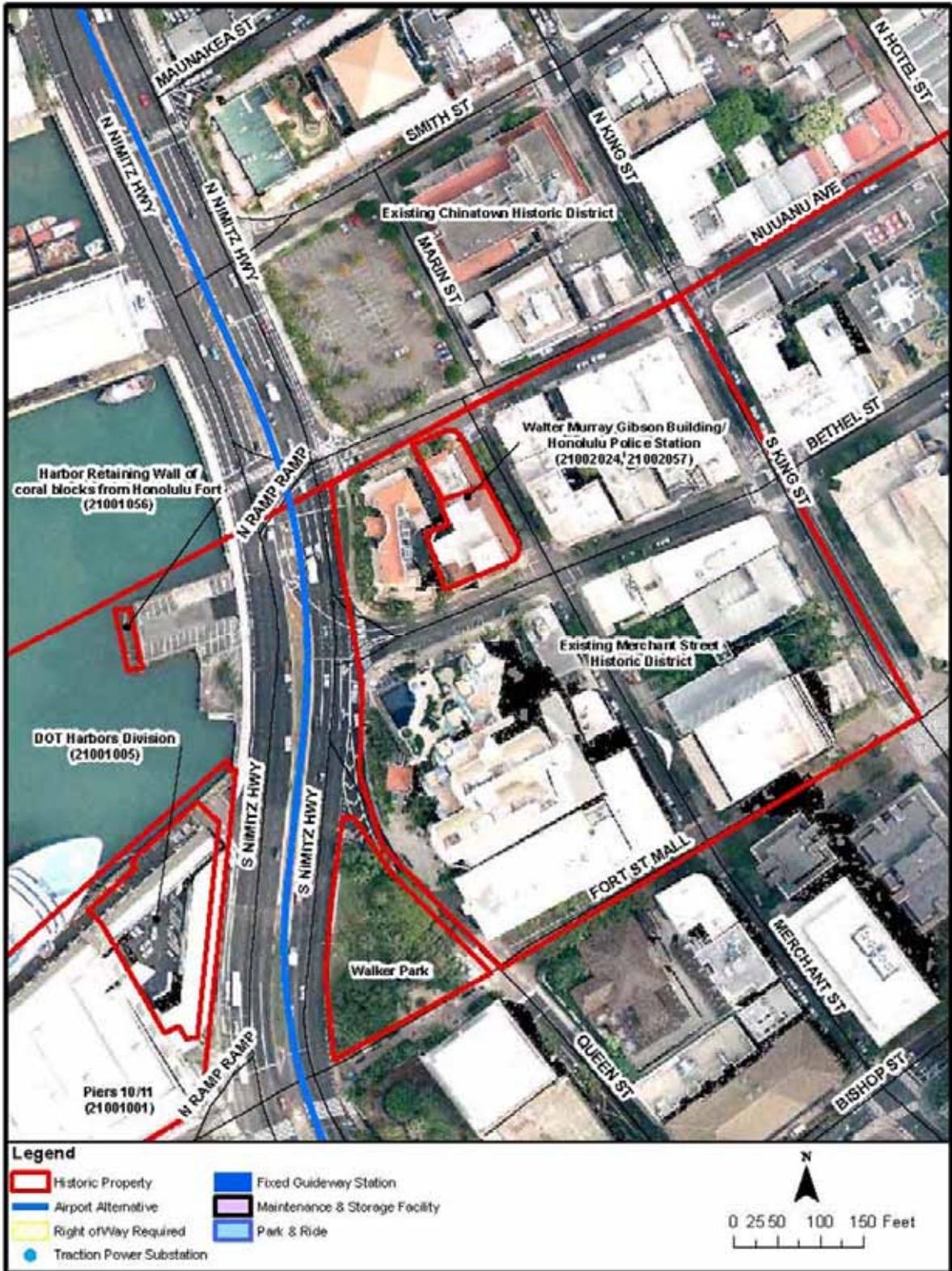


Figure 170. Merchant Street Historic District (including Walter Murray Gibson Building/Honolulu Police Station)



Figure 171. Merchant Street Historic District (including Walter Murray Gibson Building/Honolulu Police Station), facing makai from property to Nimitz Highway



Figure 172. Merchant Street Historic District (including Walter Murray Gibson Building/Honolulu Police Station), facing makai from property to Nimitz Highway

5.64 DOT Harbors Division

24 Downtown Station sector—TMK 21001005
South Nimitz Highway at Fort Street

Property description

The DOT Harbors Division building is a three-story building with an International-style façade constructed in 1952. The property was determined eligible for the NRHP under Criterion A for its association with the Harbor Commission of the Territory of Hawai'i in the period after World War II and before the 1959 advent of jetliners.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway just mauka of the DOT Harbors Division building (see Figure 173 and TMK as shown on Drawing HP021 in Appendix A). The makai edge of the guideway would be approximately 70 feet from the mauka edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-21 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the DOT Harbors Division.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. Although the property's setting was not initially identified as a historically significant feature, architectural historians have determined that the property's 'Ewa/makai viewshed to Honolulu Harbor is historically significant. The property's location on the harbor is directly related and integral to its historic use, and therefore the harbor is a historically significant feature of the property's setting. Views from the property to the harbor are intact. The setting mauka and Koko Head of the property contains the expanded alignment of Nimitz Highway and non-historic high-rise development in Downtown Honolulu. These properties within the property's mauka and Koko Head setting and viewshed do not contribute to its historic significance. The Project would introduce an elevated, fixed guideway rail structure just mauka of the property and within the center of Nimitz Highway that would be visible from the mauka portion of the property. Although the guideway's placement would introduce a new component to the property's setting and viewshed, its

presence would not alter any historically significant views or visual features. The harbor would not be altered, and historically significant views from the property makai to the harbor would not be interrupted or obscured. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century, International-style building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century, International-style building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 173. DOT Harbors Division



Figure 174. DOT Harbors Division, facing 'Ewa from Nimitz Highway at Fort Street to property



Figure 175. DOT Harbors Division, facing 'Ewa from Nimitz Highway at Bishop Street to property

5.65 Pier 10/11

24 Downtown Station sector—TMK 21001001
600 Fort Street

Property description

The Pier 10/11 building is a single-story passenger terminal building and extends most of the length of Piers 10 and 11. The building is approximately 550 feet long and was constructed in 1926. The property was determined eligible for the NRHP under Criterion A for its association with the maritime passenger industry and under Criterion C as an example of neo-classical architecture of the 1920s in Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway mauka of Pier 10/11 (see Figure 176 and TMK as shown on Drawing HP021 in Appendix A). The makai edge of the guideway would be approximately 100 feet from the mauka edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-21 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Pier 10/11.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. Although the property's setting was not initially identified as a historically significant feature of the property, architectural historians have determined that the property's 'Ewa/makai viewshed to Honolulu Harbor is historically significant. The property's location on the harbor is directly related and integral to its historic use, and therefore the harbor is a historically significant feature of the property's setting. Views from the property to the harbor are intact. The setting Koko Head of the property contains historic Aloha Tower, historic Irwin Park, and a non-historic, two-story shopping center. Aloha Tower and Irwin Park represent adjacent historic properties and visual features within the property's setting. The shopping center does not represent a historic feature; however, its scale is not out of proportion with Pier 10/11, and it does not comprise a visual intrusion that obscures views to other historic features. Therefore, architectural historians also determined that the property's Koko Head setting is a historically significant feature of the

property and is also intact. The setting mauka of the property contains the expanded alignment of Nimitz Highway and extensive non-historic high-rise development in Downtown Honolulu. These properties within the mauka setting and viewshed do not contribute to the historic significance of this property.

The Project would introduce an elevated, fixed guideway rail structure mauka of the property and within the center of Nimitz Highway that would be visible from the mauka portion of the property. Although the guideway would be a new component to mauka portions of the property's setting and viewshed, its presence would not alter any historically significant views or visual features. The harbor would not be altered, and historically significant views from the property makai to the harbor would not be interrupted or obscured. Likewise, views Koko Head to Aloha Tower and Irwin Park would not be altered or obscured. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century passenger terminal building. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century passenger terminal building. Therefore, the Project would have no effect to the property's integrity of association.



Figure 176. Pier 10/11



Figure 177. Pier 10/11, facing mauka from mauka portion of property to Nimitz Highway



Figure 178. Pier 10/11, facing mauka from makai portion of property along Fort Street to Nimitz Highway

5.66 Walker Park

24 Downtown Station sector—TMK not applicable
Nimitz Highway between Fort and Queen Streets

Property description

Walker Park is a triangular-shaped park consisting of a grassed lawn planted with multiple coconut palm trees and four mature monkeypod trees. The park was developed circa 1951. The property was determined eligible for the NRHP under Criterion A for its association with the development of the Downtown Honolulu waterfront and Central Business District and under Criterion C as an “early example of a created greenspace in the Central Business District.”

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway just makai of Walker Park (see Figure 179 and Drawing HP021 in Appendix A). The mauka edge of the guideway would be approximately 20 to 25 feet from the makai edge of the NRHP boundary. The guideway’s design in this area would correspond to the typical section illustrated as Figure B-21 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Walker Park.

Location: No Effect. The Project would not remove the property from its historic location, would not cause physical damage or destruction to all or part of the property, and would not cause any direct alteration to the property’s use. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of location.

Design: No Effect. The Project would not alter any of the property’s design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property’s integrity of design.

Setting: No Adverse Effect. This property does not retain integrity of setting. Within the NRHP boundary, non-historic elements, including a late 20th-century paved plaza and fountain and various late 20th-century monuments, characterize the property’s Koko Head side. Various non-historic monuments are also within the ‘Ewa portion of the park. Several of these monuments reuse historic materials, but their compositions date to the late 20th century. Outside of the NRHP boundary, the property’s setting was not identified as a historically significant feature of the property. Non-historic high-rise development lies immediately mauka and Koko Head of the property. The expanded alignment of heavily travelled Nimitz Highway forms the property’s makai boundary. These properties and features within the

property's setting and viewshed do not contribute to its historic significance. Furthermore, no historically significant viewsheds to or from this property were identified.

The Project would introduce an elevated, fixed guideway rail structure just makai of the property and within the center of the adjacent Nimitz Highway. Although the guideway's placement would introduce a new component to the property's setting and viewshed, its presence would not alter any historically significant views or visual features. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property does not retain integrity of setting and no historically significant or visual features within the property's setting were identified, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or further diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a mid-20th-century park with significant non-historic features. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or further obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a mid-20th-century park with significant non-historic features. Therefore, the Project would have no effect to the property's integrity of association.



Figure 179. Walker Park



Figure 180. Walker Park, facing 'Ewa from property along Nimitz Highway



Figure 181. Walker Park, facing makai from property to Nimitz Highway

5.67 Irwin Park

24 Downtown Station sector—TMK 21013007

Nimitz Highway between Fort Street and Bishop Street

Property description

Irwin Park is a rectangular-shaped park consisting primarily of paved automobile parking with grass medians and multiple mature monkeypod trees. The park was originally developed circa 1930. The property was determined eligible for the NRHP under Criterion A for its association with the history of beautification efforts in the Honolulu waterfront passenger terminal area, under Criterion B for its association with William G. Irvin, a noted Hawaiian businessman and philanthropist of the late 19th and early 20th centuries, and under Criterion C for representing the work of leading Honolulu landscape architect Robert O. Thompson.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway just mauka of Irwin Park (see Figure 182 and TMK as shown on Drawing HP022 in Appendix A). The makai edge of the guideway would be approximately 60 to 70 feet from the makai edge of the NRHP boundary. The guideway's design in this area would correspond to the typical section illustrated as Figure B-22 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Irwin Park.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. Although the property's setting was not initially identified as a historically significant feature of the property, architectural historians have determined that several areas within the property's setting are historically significant. The setting immediately 'Ewa of the property contains historic Pier 10/11, and Aloha Tower is just makai of and is visible from the park. Because the park, in conjunction with Aloha Tower, was originally conceived as a place of reception and congregation at Honolulu's waterfront passenger terminal, the park's location adjacent to Pier 10/11 and in close proximity to Aloha Tower is directly related and integral to its historic use. Therefore, Pier 10/11 and Aloha Tower are historically significant features within the property's setting and within views from the property 'Ewa and

makai. The makai setting also includes a non-historic, two-story shopping center just Koko Head of Aloha Tower. The shopping center does not represent a historic feature; however, its scale is not out of proportion with other historic properties within the property's setting, and it does not comprise a visual intrusion that obscures views to other historic features. In addition, the historic HECO Downtown Plant and Leslie A. Hicks Building lies within the property's Koko Head setting. Although this property is not historically associated with the park, it was developed at the same time as the park and has been a historic feature within the property's setting. Therefore, architectural historians determined that the property's 'Ewa, makai, and Koko Head setting is a historically significant feature of the property and is intact. The setting mauka of the property contains the expanded alignment of heavily travelled Nimitz Highway and extensive non-historic high-rise development in Downtown Honolulu. These properties within the property's mauka setting and viewshed do not contribute to its historic significance. Notably, non-historic paving has been installed within a substantial portion of this park and within its NRHP boundary to accommodate automobile parking; thus, the property does not retain integrity of setting within its NRHP boundary.

The Project would introduce an elevated, fixed guideway rail structure just mauka of the property and within the center of Nimitz Highway that would be visible from the mauka portion of the property. Although the guideway's placement would introduce a new component to the property's mauka setting and viewshed, the presence of the guideway would not alter any historically significant views or visual features. Pier 10/11, Aloha Tower, and the HECO plant would not be altered, and historically significant views from the property 'Ewa, makai, and Koko Head would not be interrupted or obscured. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or further diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as a 1930s park with significant non-historic features. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or further obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as a 1930s park with significant non-historic features. Therefore, the Project would have no effect to the property's integrity of association.



Figure 182. Irwin Park



Figure 183. Irwin Park, facing 'Ewa from property along Nimitz Highway



Figure 184. Irwin Park, facing mauka from Aloha Tower Dr. at Bishop Street to property and Nimitz Highway



Figure 185. Visual Simulation: Nimitz Highway/Fort Street Intersection, looking Koko Head, Irwin Park visible on right

5.68 Aloha Tower

24 Downtown Station sector—TMK 21001013
Fort Street at Aloha Tower Drive

Property description

Aloha Tower is a 184-foot, Art Deco-style tower constructed in 1926. The property was determined eligible for the NRHP under Criterion A for its association with the development of Hawai'i as a tourist destination for travelers from the Mainland and for its role as a harbor-control tower during World War II, and under Criterion C as an example of 1920s Art Deco architecture in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Nimitz Highway mauka of Aloha Tower (see Figure 186 and TMK as shown on Drawing HP022 in Appendix A). The makai edge of the guideway would be approximately 650 feet from the mauka edge of the NRHP boundary and from the building. The guideway's design in this area would correspond to the typical sections illustrated as Figures B-21 and B-22 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to Aloha Tower.

Location: No Effect. The Project would not cause the property's removal from its historic location. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Adverse Effect. Although the property's setting was not initially identified as a historically significant feature of the property, architectural historians have determined that select areas and visual features within the property's setting are historically significant. Because of the nature of the property's historic use, this property's setting may be considered and evaluated in two ways: its local setting at or near grade, and its setting and viewsheds from the observation area at the top of the tower.

At or near grade, the local setting immediately 'Ewa and makai of the property is Honolulu Harbor, and the setting mauka contains historic Pier 10/11 and Irwin Park. Because Aloha Tower, in conjunction with Irwin Park, was originally conceived as a place of reception and congregation at Honolulu's waterfront passenger terminal, the park's location adjacent to the harbor, Pier 10/11, and in close proximity to Irwin Park is directly related and integral to its historic use. Therefore, Honolulu Harbor,

Pier 10/11, and Irwin Park are historically significant features within the property's 'Ewa, makai, and mauka setting. The property's Koko Head setting includes a non-historic, two-story shopping center that does not contribute to the historic significance of this property.

Expansive views to O'ahu Island and the Pacific Ocean are available from the observation area at the top of Aloha Tower: makai views include Honolulu Harbor and the Pacific Ocean beyond; Koko Head views include the harbor, the aforementioned shopping center, and additional piers; mauka views include extensive non-historic high-rise development in Downtown Honolulu; and 'Ewa views include the harbor, industrial areas, Honolulu's 'Ewa neighborhoods, and surrounding mountain ranges. Because of extensive ongoing and mid-to-late 20th-century development of O'ahu, views from the tower encompass predominantly non-historic features, including non-historic high-rise development in Downtown Honolulu and elsewhere, non-historic harbor improvements and surrounding industrial development, and non-historic commercial and residential development. Nonetheless, Aloha Tower was constructed to provide a visual icon to incoming travelers on passenger ships to Honolulu, and to provide views from the tower to Honolulu, O'ahu, and the Pacific Ocean (Sandler, Mehta, and Haines 2008). Thus, views from the ocean to the tower and views from the tower to the ocean and island are a historic visual feature of the property.

The Project would introduce an elevated, fixed guideway rail structure approximately 650 feet mauka of the property and within the center of Nimitz Highway. At ground level, the guideway would be substantially screened from the property by distance and the surrounding properties, including Pier 10/11 and vegetation within Irwin Park. The guideway would be partially visible in the distance from the mauka portion of the property at the intersection of Fort Street and Nimitz Highway. Although the guideway would be a new component to the property's mauka setting and viewshed at ground level, its presence would not alter any historically significant views or visual features. Pier 10/11 and Irwin Park would not be altered, and grade-level historically significant views from the property 'Ewa, makai, and mauka would not be interrupted or obscured. Although the guideway would be visible from the observation area at the top of the tower, the Project would represent a minor change to the property's mauka setting. From the elevated position at the top of the tower and from an approximately 650-foot distance, the guideway would be a minor feature set within the extensive non-historic development in Downtown Honolulu and the surrounding areas. 'Ewa, makai, and Koko Head views from the top of the tower would not be altered.

Views to the tower were also assessed. From various points around Aloha Tower from Nimitz Highway and surrounding parcels, the tower is often not visible because of the presence of vegetation or built resources that block views for pedestrians and motorists. While it is obviously a landmark in Honolulu, it is not universally visible because of these numerous modern intrusions. Views from incoming maritime traffic in Honolulu Harbor to the tower would also not be interrupted or obscured. In addition, no audible or atmospheric effects to this property were identified.

Therefore, because no historically significant or visual features within the property's

setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the NRHP boundary that would alter its historic character as an early 20th-century, Art Deco-style tower. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the NRHP boundary that would obscure its historic appearance to an observer as an early 20th-century, Art Deco-style tower. Therefore, the Project would have no effect to the property's integrity of association.



Figure 186. Aloha Tower



Figure 187. Aloha Tower, facing mauka from property along Fort Street



Figure 188. Aloha Tower, facing mauka from top of tower



Figure 189. Aloha Tower, facing makai/Koko Head from top of tower



Figure 190. Aloha Tower, facing makai from top of tower



Figure 191. Aloha Tower, facing 'Ewa from top of tower



Figure 192. Aloha Tower, facing mauka/'Ewa from top of tower

5.69 Dillingham Transportation Building
24 Downtown Station sector—TMK 21014003
735 Bishop Street

Property description

The Dillingham Transportation Building is a four-story, Italian Renaissance Revival-style building constructed in 1930. The property was determined eligible for the NRHP under Criterion A for its association with the commercial development of Honolulu and the Dillingham family businesses, and under Criterion C as a good example of the Italian Renaissance Revival-style building with an ornate Art Deco-style lobby.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and a transit station platform in the median of Nimitz Highway immediately makai and Koko Head of the Dillingham Transportation Building (see Figure 193 and TMK as shown on Drawing HP022 in Appendix A). The mauka edge of the guideway structure would be approximately 25 to 40 feet from the makai edge of the NRHP boundary and from the building. Associated features of the transit station platform, including a grade-level entryway, escalator, and elevator shaft, would be located immediately behind and adjacent to the building at its Koko Head/makai corner and within its NRHP boundary. An approximately 2,000-square-foot area within the NRHP boundary would be acquired within which an entry, escalators, and elevator shaft would be constructed. The guideway's design in this area would correspond to the typical section illustrated as Figure B-23 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-13 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have an **Adverse Effect** to the Dillingham Transportation Building.

Location: Adverse Effect. The Project would not remove the property from its historic location but would cause physical damage or destruction to a portion of the property. To accommodate an entry area with an escalator and elevator to the transit station platform, right-of-way would be acquired within the TMK parcel and NRHP boundary containing the Dillingham Transportation Building. An approximately 2,000-square-foot area behind the building and adjacent to Nimitz Highway would be acquired; existing vegetation would be removed; and an entry, escalator, and elevator shaft would be constructed. The vegetation is a remnant of the original interior garden (Sandler, Mehta, and Haines 2008). However, the garden area has been substantially altered with modern design features, including planters and paving, and it no longer represents the historic garden design. Nonetheless, because significant

right-of-way acquisition and use of a portion of the property would be required, the Project would have an adverse effect to the property's integrity of location.

Design: No Adverse Effect. The Project would require an approximately 2,000-square-foot acquisition within the NRHP boundary of the historic property. This acquisition would require removal of vegetation in a portion of the building's interior garden. As described above, the vegetation is a remnant of the original interior garden design that has been substantially altered with modern design features, including planters and paving. It no longer represents the historic garden design. No other design features would be physically impacted. Therefore, the Project would have no adverse effect to the property's integrity of design.

Setting: Adverse Effect. This property does not retain integrity of setting, and the property's setting was not identified as a historically significant feature of the property. Non-historic high-rise development is located immediately 'Ewa, mauka, and Koko Head of the property. The expanded alignment of heavily travelled Nimitz Highway forms the property's makai boundary. These properties and features within the property's setting and viewshed do not contribute to its historic significance. The Project would introduce an elevated, fixed guideway rail structure and transit station platform just makai and Koko Head of the property and within the median of the adjacent Nimitz Highway. An approximately 2,000-square-foot area behind the building and adjacent to Nimitz Highway would be acquired within the property's TMK parcel and within its NRHP boundary. Existing vegetation would be removed, and an entry, escalator, and elevator shaft would be constructed. The vegetation, which includes mature coconut palm trees, is a visual buffer between the rear of the Dillingham Transportation Building and an adjacent non-historic high-rise building.

Although setting was not identified as a historically significant feature or characteristic of the historic property that contributes to its NRHP eligibility, and although the property does not retain integrity of setting outside of its NRHP boundary, the guideway's placement within the median of Nimitz Highway would significantly alter the immediate physical and visual setting. The guideway's proximity to the historic building would generally obscure the historic appearance of the building and its relationship to its surroundings, and the modern structure would become an additional, prominent non-historic element in the property's setting. Mauka views from upper stories of the building would consist of the guideway structure. In addition, portions of the property's historic interior garden would be removed. Vegetation that partially screens the rear of the building from an adjacent non-historic high-rise building would be removed and entry facilities constructed at this location. The reduction of the interior garden, removal of vegetation, and construction of an escalator and elevator shaft would introduce additional non-historic features in the property's immediate setting. Therefore, because the Project would significantly alter and diminish the property's immediate setting, the Project would have an adverse effect to the property's integrity of setting. No audible or atmospheric effects to this property were identified.

Materials: No Adverse Effect. The Project would require an approximately 2,000-square-foot acquisition within the NRHP boundary of the historic property.

This acquisition would require removal of vegetation in a portion of the building's interior garden. As described above, the vegetation is a remnant of the original interior garden design. However, the garden area has been substantially altered with modern design features, including planters and paving, and it no longer represents the historic garden design. No other features would be physically impacted. Therefore, the Project would have no adverse effect to the property's integrity of materials.

Workmanship: No Adverse Effect. The Project would require an approximately 2,000-square-foot acquisition within the NRHP boundary. This acquisition would require removal of vegetation in a portion of the building's interior garden. As described above, the vegetation is a remnant of the original interior garden design. However, the garden area has been substantially altered with modern design features, including planters and paving, and it no longer represents the historic garden design. No other features would be physically impacted. Therefore, the Project would have no adverse effect to the property's integrity of workmanship.

Feeling: Adverse Effect. The Project would diminish the property's expression of its historic character. The guideway would significantly alter the property's immediate setting and views from the building mauka, as well as reduce the interior garden and screening vegetation. Placement of the guideway, passenger platform, and support structures in close proximity to the building would also obscure the property's historic appearance. Although the building would remain physically intact, the proximity of the guideway and related structures to the building would substantially obscure the visual relationship between the building and its surroundings and thereby obscure the property's historic character. Therefore, the Project would have an adverse effect to the property's integrity of feeling.

Association: Adverse Effect. The Project would alter historically significant visual relationships of the property and would obscure its historic appearance to an observer. The guideway would significantly alter the property's immediate setting and views from the building mauka, as well as reduce the interior garden and screening vegetation. Construction of the guideway, passenger platform, and support structures in close proximity to the building would also obscure the property's historic appearance. Although the building would remain physically intact, the proximity of the guideway and related structures to the building would substantially obscure the visual relationship between the building and its surroundings. Therefore, the Project would have an adverse effect to the property's integrity of association.

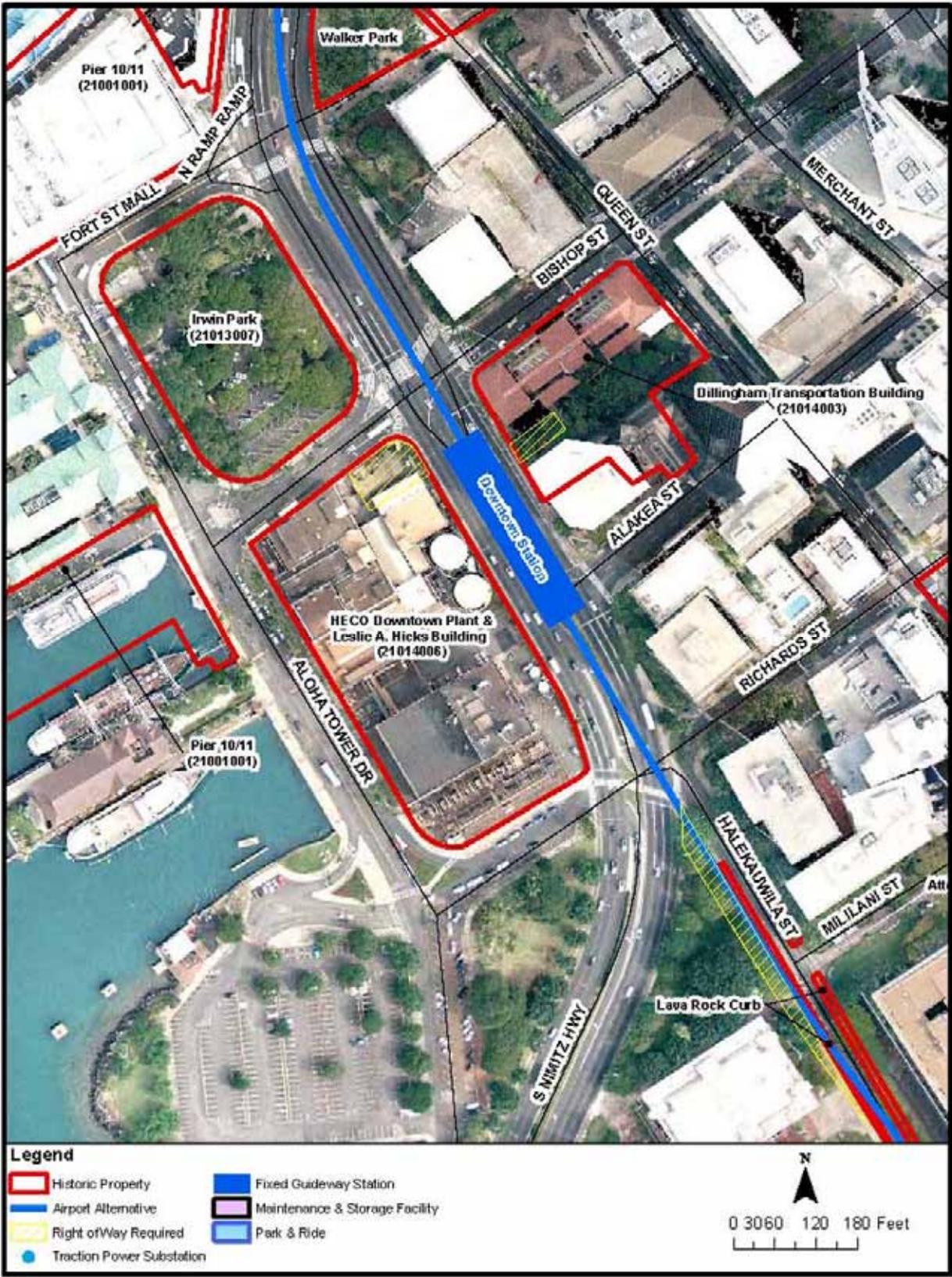


Figure 193. Dillingham Transportation Building



Figure 194. Dillingham Transportation Building, facing mauka from Bishop Street to Nimitz Highway and property



Figure 195. Dillingham Transportation Building, facing mauka/Koko Head from Nimitz Highway at Bishop Street, property on left



Figure 196. Visual Simulation: Nimitz Highway/Fort Street Intersection, looking Koko Head, Dillingham Transportation Building visible on left

5.70 HECO Downtown Plant and Leslie A. Hicks Building

24 Downtown Station sector—TMK 21014006

222 Ala Moana Boulevard

Property description

The HECO Downtown Plant and Leslie A. Hicks Building is a power generation plant consisting primarily of two buildings constructed in 1929 and 1955, respectively. The property was determined eligible for the NRHP under Criterion A for its association with the history of electric power in and the development of Honolulu.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure and transit station platform in the median of Ala Moana Boulevard just mauka of the HECO Downtown Power Plant and Leslie A. Hicks Building (see Figure 197 and TMK as shown on Drawing HP022 in Appendix A). The makai edge of the guideway and platform structure would be approximately 40 to 45 feet from the mauka edge of the NRHP boundary. Associated features of the transit station platform, including a grade-level entry, escalator, and elevator shaft, would be located immediately mauka of and adjacent to the property's 1929 building at its 'Ewa/mauka corner and within its NRHP boundary. An approximately 6,750-square-foot area within the NRHP boundary would be acquired, within which an entry, escalator, and elevator shaft would be constructed. The guideway's design in this area would correspond to the typical section illustrated as Figure B-23 in Appendix B and would be elevated approximately 40 to 45 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-2 and C-13 in Appendix C.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Adverse Effect** to the HECO Downtown Plant and Leslie A. Hicks Building.

Location: No Adverse Effect. The Project would not remove the property from its historic location but would cause physical damage or destruction to a portion of the property. To accommodate the entry area, escalator, and elevator to the transit station platform, right-of-way would be acquired within the TMK parcel and NRHP boundary containing the HECO Downtown Plant and Leslie A. Hicks Building. An approximately 6,750-square-foot area in front of or mauka of the 1929 building and adjacent to Ala Moana Boulevard would be acquired; the existing parking lot and coconut palm trees would be removed; and elevator shafts would be constructed. The parking lot and trees are not historically significant features associated with the property. Removal of these features would not remove any historically significant features or characteristics of the property or alter the continued use of the property. Therefore, although right-of-way acquisition and use of a portion of the property

would be required, the Project would have no adverse effect to the property's integrity of location.

Design: No Adverse Effect. The Project would not alter any of the property's historic design elements or features. The Project would require an approximately 6,750-square-foot acquisition within the NRHP boundary of the property. However, this acquisition would not alter any historic design features of the property. The parking lot and vegetation that would be removed were not identified as, and are not known to be, historic landscape design features of the property. Therefore, the Project would have no adverse effect to the property's integrity of design.

Setting: No Adverse Effect. Although the property's setting was not initially identified as a historically significant feature, architectural historians have determined that several areas within the property's setting are historically significant. The power plant has always been located on Honolulu Harbor, and the property's 'Ewa setting contains historic Irwin Park, which was developed at approximately the same time. Therefore, views from the property to the harbor and to Irwin Park are historically significant visual features within the property's setting. Non-historic high-rise development is located mauka and across the expanded and heavily travelled alignment of Ala Moana Boulevard from the property. These properties and features within the property's setting and viewshed do not contribute to its historic significance. The Project would introduce an elevated, fixed guideway rail structure and transit station platform just mauka of the property and within the center of the adjacent Nimitz Highway. An approximately 6,750-square-foot area behind the building and adjacent to Ala Moana Boulevard would be acquired within the property's TMK parcel and within its NRHP boundary. A parking lot and existing vegetation would be removed, and an entry, escalator, and elevator shaft would be constructed at this location. This parking lot and vegetation were not identified as, and are not known to be, historic features of the property or its setting.

Although the guideway would be a new component in the property's setting and viewshed, its presence would not alter any historically significant views or visual features. Makai views from the property to the harbor and 'Ewa views to Irwin Park would not be interrupted or obscured. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because no historically significant or visual features within the property's setting would be altered, the Project would have no adverse effect to the property's integrity of setting.

Materials: No Adverse Effect. The Project would not alter any physical elements that comprise the property. The Project would require an approximately 6,750-square-foot acquisition within the NRHP boundary. However, this acquisition would not alter any historic design features. The parking lot and vegetation that would be removed were not identified as, and are not known to be, historic landscape design features of the property. Therefore, the Project would have no adverse effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. The Project would require an approximately 6,750-square-foot acquisition within the NRHP boundary but would

not alter any historically significant characteristics or features related to the workmanship used to create the property. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would require an approximately 6,750-square-foot acquisition within the NRHP boundary. However, this acquisition would not alter any historically significant physical features. The parking lot and vegetation that would be removed were not identified as, and are not known to be, historic features. The Project would not diminish the property's expression of its historic character. Therefore, the Project would have no adverse effect to the property's integrity of feeling.

Association: No Adverse Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. The Project would require an approximately 6,750-square-foot acquisition within the NRHP boundary. However, this acquisition would not alter any historically significant features. The parking lot and vegetation that would be removed were not identified as, and are not known to be, historic features. Therefore, the Project would have no adverse effect to the property's integrity of feeling or association.

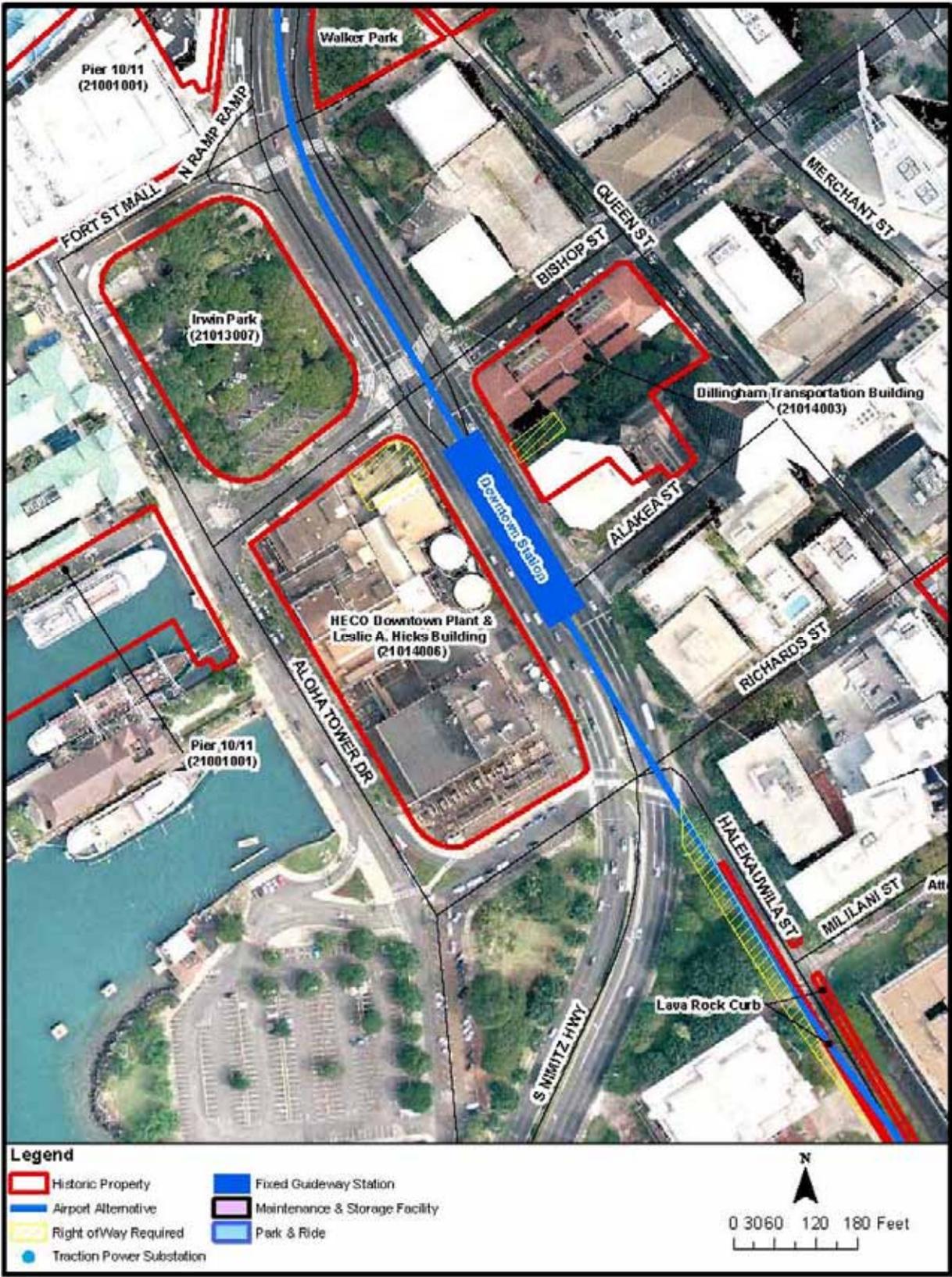


Figure 197. HECO Downtown Plant and Leslie A. Hicks Building



Figure 198. HECO Downtown Plant and Leslie A. Hicks Building, facing Koko Head along Nimitz Highway from Bishop Street to property



Figure 199. HECO Downtown Plant and Leslie A. Hicks Building, facing mauka from Richards Street to Ala Moana Boulevard, property on left

5.71 Hawai'i Capital Historic District (including Attorney General's Office/Hale 'Auhau)

24 Downtown Station sector—TMK 21026022
403 Queen Street

Property description

The Attorney General's Office/Hale 'Auhau is the only contributing resource within the Hawai'i Capital Historic District identified as being in the Project's APE. The Mission Revival-style building consists of three stories and was constructed in 1939. The property was also independently evaluated as eligible for the NRHP under Criterion C as a late example of Mission Revival-style architecture once popular for public buildings in Hawai'i.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure within the alignment of Halekauwila Street makai of the Attorney General's Office/Hale 'Auhau, a contributing historic resource within the Hawai'i Capital Historic District (see Figure 200 and Drawing HP022 in Appendix A). The mauka edge of the structure would be approximately 250 feet from the makai edge of the historic district's NRHP boundary and 300 feet from the Attorney General's Office/Hale 'Auhau. The guideway's design in this area would correspond to the typical sections illustrated as Figures B-24 and B-25 in Appendix B and would be elevated approximately 40 to 45 feet above grade.

Application of criteria of adverse effect

Summary finding: Based on the evaluation below, the Project would have **No Effect** to the Attorney General's Office/Hale 'Auhau, the only contributing historic resource within the Hawai'i Capital Historic District located within the Project's APE, and thus no adverse effect would occur to the historic district.

Location: No Effect. The Project would not result in removal of the property from its historic location. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of location.

Design: No Effect. The Project would not alter any of the property's design elements or features. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of design.

Setting: No Effect. This property would be completely screened from project activity by an intervening larger building. In addition, the building is substantially set back on its lot. The Project would include construction of an elevated, fixed-guideway rail structure within the alignment of Halekauwila Street approximately 300 feet makai of the Attorney General's Office/Hale 'Auhau. Although views from the Koko Head

edge of the property's lawn makai along Punchbowl Street to Halekauwila Street may include a small portion of the guideway in this area, this change to the property's setting and makai viewshed would be negligible. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, because the property would be visually screened from the Project, the Project would have no effect to the property's integrity of setting.

Materials: No Effect. The Project would not alter any physical elements that comprise the property. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of materials.

Workmanship: No Effect. The Project would not affect any characteristics or features related to the workmanship used to create the property. No project activity would occur within the historic district's NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. No project activity would occur within the historic district's NRHP boundary that would alter the Attorney General's Office/Hale 'Auhau's historic character as a Mission Revival-style government building of the 1930s. Therefore, the Project would have no effect to the property's integrity of feeling.

Association: No Effect. The Project would not affect any of the property's physical features or obscure or interrupt any historically significant visual features. No project activity would occur within the historic district's NRHP boundary that would obscure the Attorney General's Office/Hale 'Auhau's historic character as a Mission Revival-style government building of the 1930s. Therefore, the Project would have no effect to the property's integrity of association.

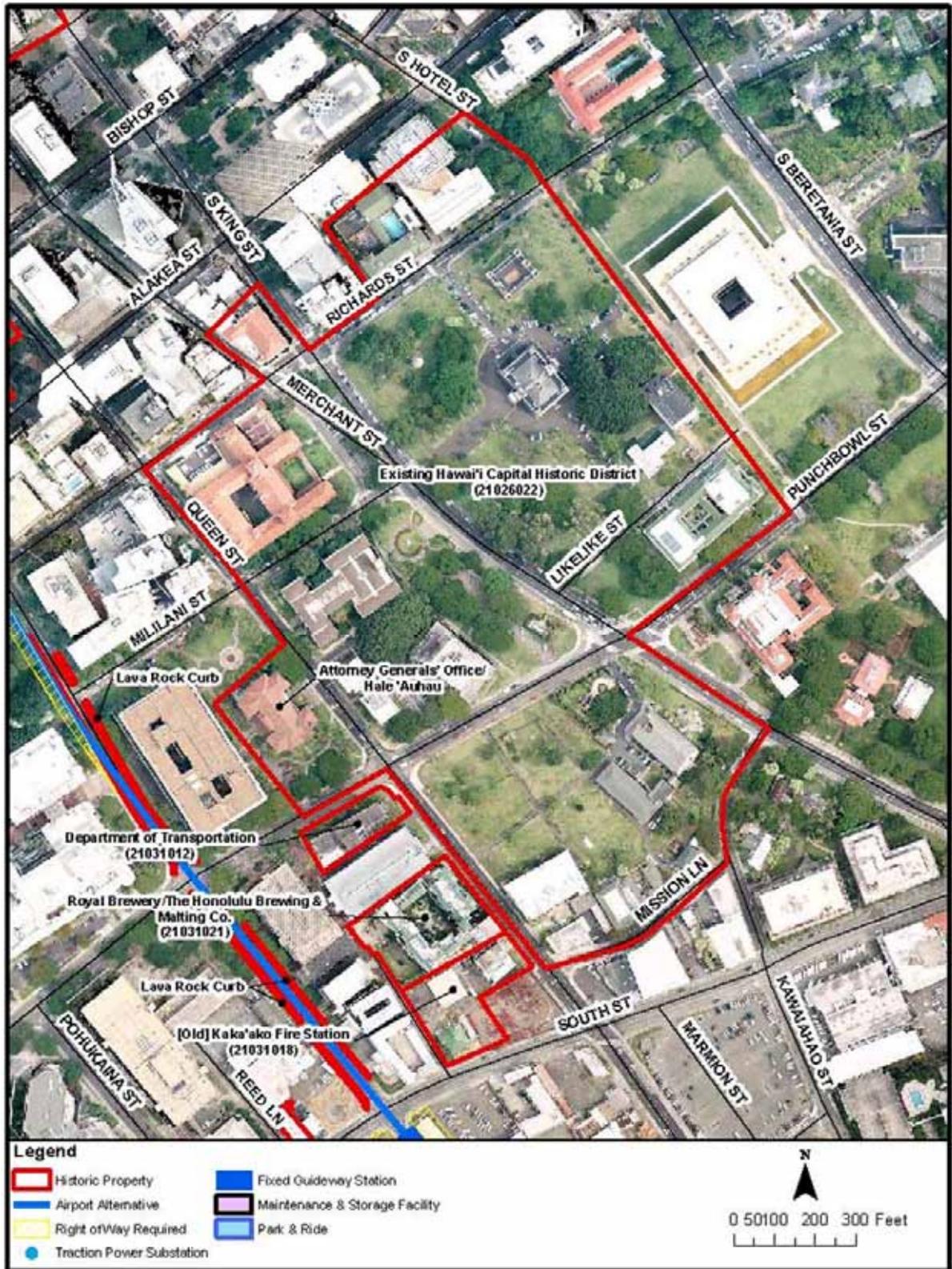


Figure 200. Hawai'i Capital Historic District (including Attorney General's Office/Hale 'Auhau)



Figure 201. Hawai'i Capital Historic District (including Attorney General's Office/Hale 'Auhau), facing 'Ewa from Punchbowl Street to property



Figure 202. Hawai'i Capital Historic District (including Attorney General's Office/Hale 'Auhau), facing makai along Punchbowl Street from property to Halekauwila Street

5.72 Department of Transportation

25 Civic Center Station sector—TMK 21031012
869 Punchbowl Street

Property description

The Department of Transportation Building was built in 1959 and designed by Law & Wilson. It is five stories in height with full-height vertical louvers that form a sunscreen. The property was determined to be eligible for the NRHP under Criterion C as an early example of the use of pre-stressed concrete in large, vertically oriented architectural elements, which was considered a breakthrough at the time.

Project description in vicinity of property

The Project would be located one-half block makai of the Department of Transportation Building on Halekauwila Street (see Figure 203 and TMK as shown on Drawing HP022 in Appendix A). The mauka edge of the guideway would be approximately 225 to 250 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-25 in Appendix B and would be elevated approximately 45 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Department of Transportation Building. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside of the NRHP boundary.

Location: No Effect. The Department of Transportation Building retains integrity of location and this would not be altered by the Project. The building will remain in its current location.

Design: No Effect. The Department of Transportation Building retains a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside of the building's NRHP boundaries.

Setting: No Adverse Effect. The Department of Transportation Building is located in a developed urban area that contains surrounding new construction as well as contemporary built resources. The Project would be visible from select areas of the Department of Transportation Building, but would not be visible from other portions because of the building's orientation and its substantial setback. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility under Criterion C, which appears to be solely based on the building's construction material and method. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the Department of

Transportation Building's setting, there would be an effect, but these changes are minimal and would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Department of Transportation Building retains a high level of integrity of materials, which is particularly critical for this building because its innovative use of materials is the sole reason it was determined to be eligible for NRHP listing, according to the recent determination of eligibility. The Project would not alter the current integrity of materials. All project work would occur outside of the Department of Transportation Building's NRHP boundary, and the Project would not affect its integrity of materials.

Workmanship: No Effect. The Department of Transportation Building retains a high level of integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Department of Transportation Building has a high level of integrity of feeling. It conveys its origins as a late 1950s mid-century Modern building that used concrete, a popular material of the era, to achieve its design intent. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character.

Association: No Effect. The Department of Transportation Building also has a high level of integrity of association. It continues to demonstrate its role in the construction of mid-century Modern commercial buildings in Honolulu at a time when the City was striving to establish itself as a forward-thinking, urban area by using innovative construction techniques. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building itself and not the surrounding environment.

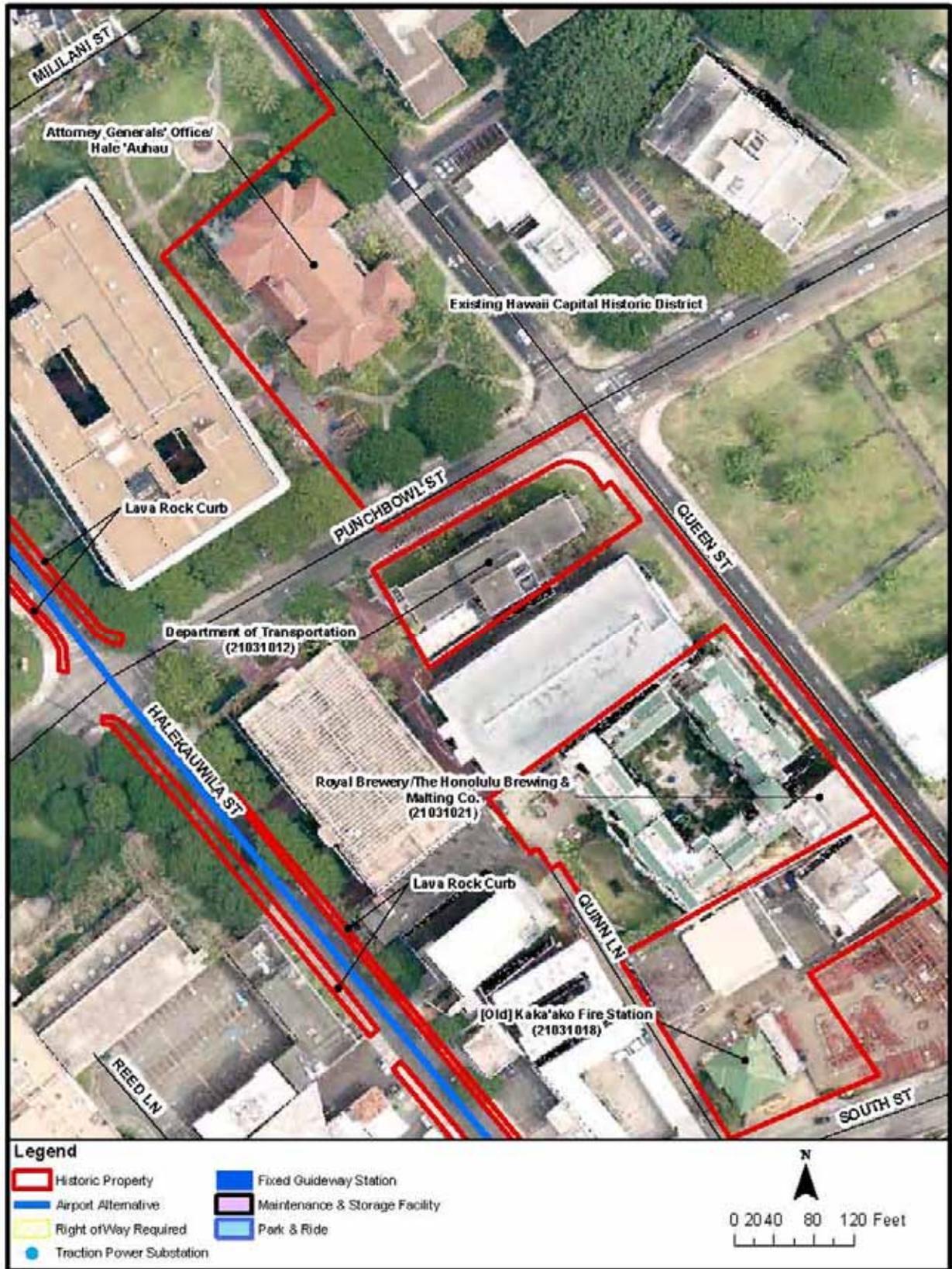


Figure 203. Department of Transportation



Figure 204. Department of Transportation, facing makai along Punchbowl Street from property to Halekauwila Street



Figure 205. Department of Transportation, facing makai along Punchbowl Street from Queen Street to Halekauwila Street, property on left

5.73 Royal Brewery/Honolulu Brewing & Malting Co.
25 Civic Center Station sector—TMK 21031021
547-553 Queen Street

Property description

The Royal Brewery is a red-brick, Romanesque Revival building that exemplifies Victorian-era building trends that were commonly applied to mainland breweries. The Royal Brewery was listed in the National Register of Historic Places in 1972. The original form does not specify criteria, but updates completed as part of the current project state that the property meets Criterion A for its association with beer-making in Hawai'i, and Criterion C for the distinctive architecture that is unique to Hawai'i. The Royal Brewery is located within a mixed-use, developed area of Honolulu and is surrounded by extensive new construction. It is a community landmark.

Project description in vicinity of property

Implementation of the Project would include construction of an elevated, fixed-guideway rail structure in the median of Halekauwila Street, one block makai from the NRHP boundaries (see Figure 206 and TMK as shown on Drawing HP022 in Appendix A). The mauka edge of the guideway would be approximately 175 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-25 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Effect** on the brewery. No historic properties would be affected.

Location: No Effect. The brewery retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The brewery has integrity of design on three of its four elevations. The entire rear of the building has been recently replaced and re-clad (possibly where an attached building may have been removed to accommodate adjacent new construction). The Project would not alter the overall remaining integrity of design. All work would occur outside of the brewery's NRHP boundary.

Setting: No Effect. The brewery is located in a developed urban area that contains surrounding new construction. The Project would be approximately 175 feet to the rear of the building, with a new condominium building of equal height to the brewery separating the brewery from the Project. The Project would not be visible from the brewery, most notably because the rear elevation is devoid of openings. The brewery's integrity of setting would not be affected by the guideway.

Materials: No Effect. Three of the brewery's four elevations retain integrity of materials, although the rear elevation has been substantially altered with recent renovations. All Project work would occur outside of the brewery's NRHP boundary, and the building's integrity of materials would not be affected.

Workmanship: No Effect. The building retains exterior integrity of workmanship, with the exception of the rear elevation. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. Despite the encroaching new construction and alterations to the rear elevation, the brewery retains a high level of integrity of feeling. It conveys its origins as an early 20th-century beer manufacturing facility and is particularly interesting within Honolulu because it imitates Mainland breweries, which were often executed in similar materials and styles. Despite minor changes, the historic character-defining features remain intact. Because the Project would not be visible from the brewery, the building's integrity of feeling would not be diminished.

Association: No Effect. The brewery retains high integrity of association as a historic brewery building. The building type is distinctly associated with its historic function, an attribute that is still conveyed today because of its high level of integrity, even though it no longer functions as a brewery. The Project would not alter the integrity of association of the building because it would not be visible from the property and no work would occur within the station's NRHP boundary.

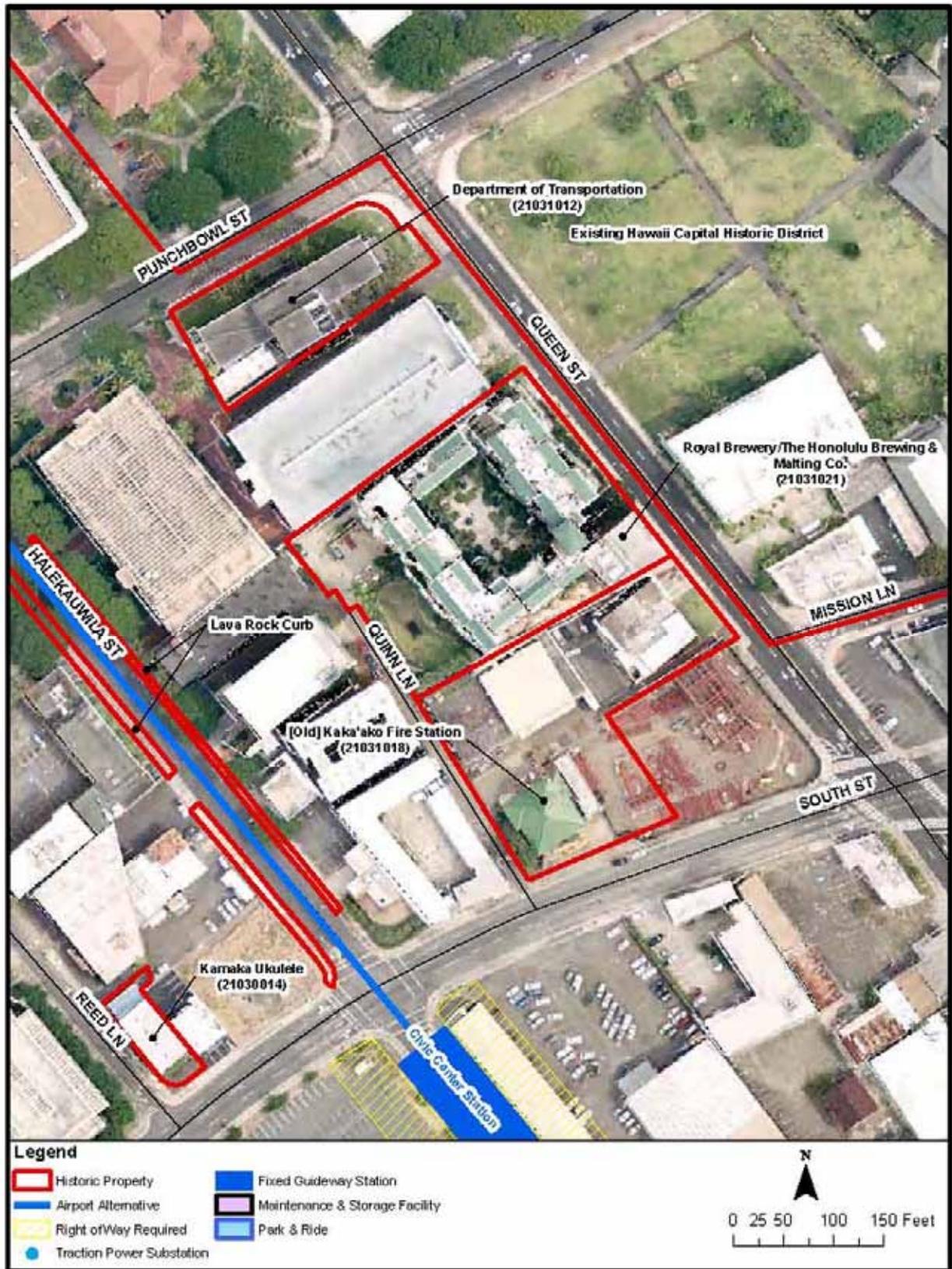


Figure 206. Royal Brewery/Honolulu Brewing & Malting Co.

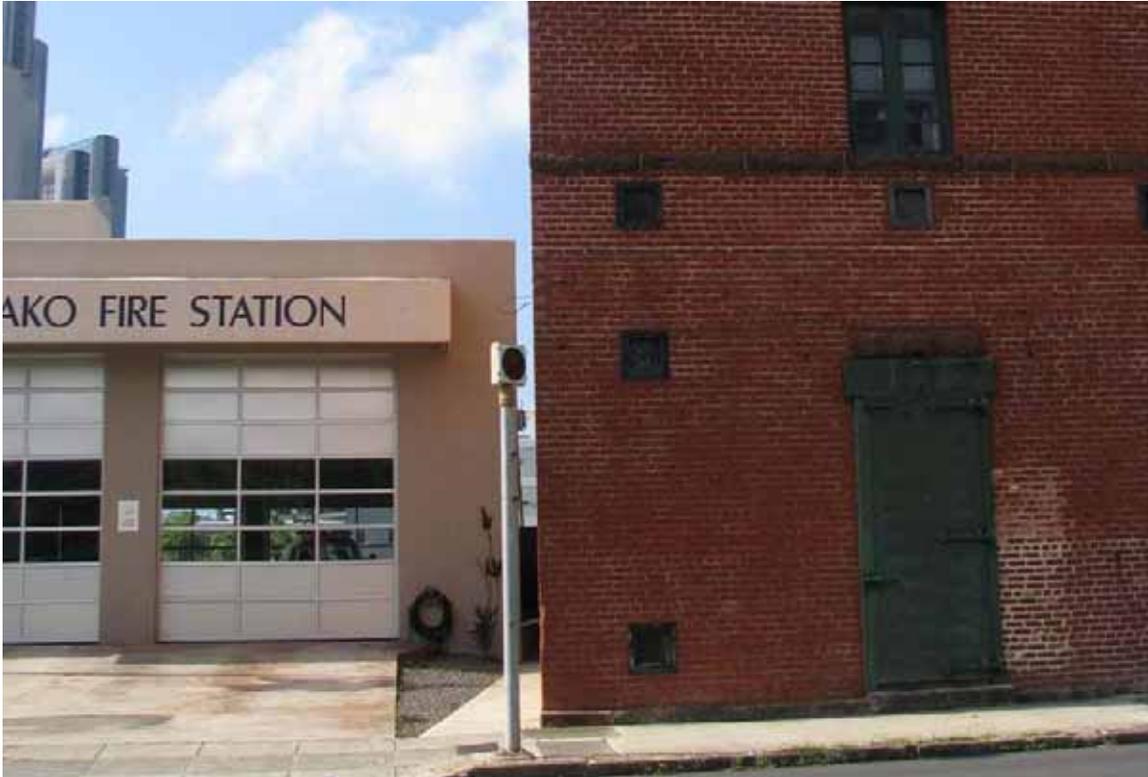


Figure 207. Royal Brewery/Honolulu Brewing & Malting Company, facing makai from Queen Street to Halekauwila Street, property on right



Figure 208. Royal Brewery/Honolulu Brewing & Malting Company, facing mauka/'Ewa from South Street to property

5.74 [Old] Kaka‘ako Fire Station

25 Civic Center Station sector—TMK 21031018
620 South Street

Property description

Constructed in 1929, the Old Kaka‘ako Fire Station has a hipped roof and attached tower that ascends above the roofline. Segmental-arch openings formerly used as fire truck entrances have been filled in with windows and muntins. The building is now used as a museum. The fire station was listed in the National Register of Historic Places in 1980 as part of the “Fire Stations of O‘ahu” Thematic Registration. Although the criteria are not specifically addressed in the National Register documentation, the fire station is eligible under Criterion A for its association with the Honolulu Fire Department, and Criterion C as an example of the Spanish Colonial architectural style used on a fire station building. The building is in excellent condition. Outside of its NRHP boundary, the fire station lies within a mixed-use, developed area of Honolulu. The station is surrounded by extensive new construction. It serves as a community landmark.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Halekauwila Street, one block makai from the of the NRHP boundary (see Figure 209 and TMK as shown on Drawing HP022 in Appendix A). The Civic Center Station would be located in the fire station’s vicinity, but according to current plans, it would not be visible from the fire station. The mauka edge of the guideway would be approximately 200 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-25 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Effect** on the Kaka‘ako Fire Station. The guideway and station would not be visible from the fire station.

Location: No Effect. The fire station retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The fire station retains moderate integrity of design. Prior changes to the property include alterations to the fire truck entrance doors. The Project would not alter the integrity of design, and all project-related work would occur outside of its boundaries.

Setting: No Effect. The fire station is located in a developed urban area that contains surrounding new construction. The project would not be visible from the fire station because of surrounding tall new construction and because of the orientation of the building on its parcel. The Civic Center Station would also not be visible from the fire

station because its location would be screened by tall surrounding buildings. Furthermore, no audible or atmospheric effects to this property were identified. The fire station's setting would not be affected by the guideway.

Materials: No Effect. The fire station retains moderate integrity of materials. Its former doors, however, have been altered to function as windows. The Project would not alter the current integrity of materials. All project work would occur outside of the fire station's NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The fire station retains integrity of workmanship, with the exception of the alterations discussed above. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The fire station has a high level of integrity of feeling. It conveys its origins as a 1920s fire station executed in a popular Spanish Colonial architectural style, and its historic character-defining features remain intact. Although the fire station is now used as a museum, the integrity of feeling has not been greatly compromised by this change in use. Because the Project would not be visible from the parcel, no alterations to the property's integrity of feeling would occur.

Association: No Effect. The fire station also has a high level of integrity of association. It continues to demonstrate its link to the historic network of early 20th-century fire stations on O'ahu. The Project would not alter the integrity of association because it would not be visible from the property and no work would occur within the station's NRHP boundary.

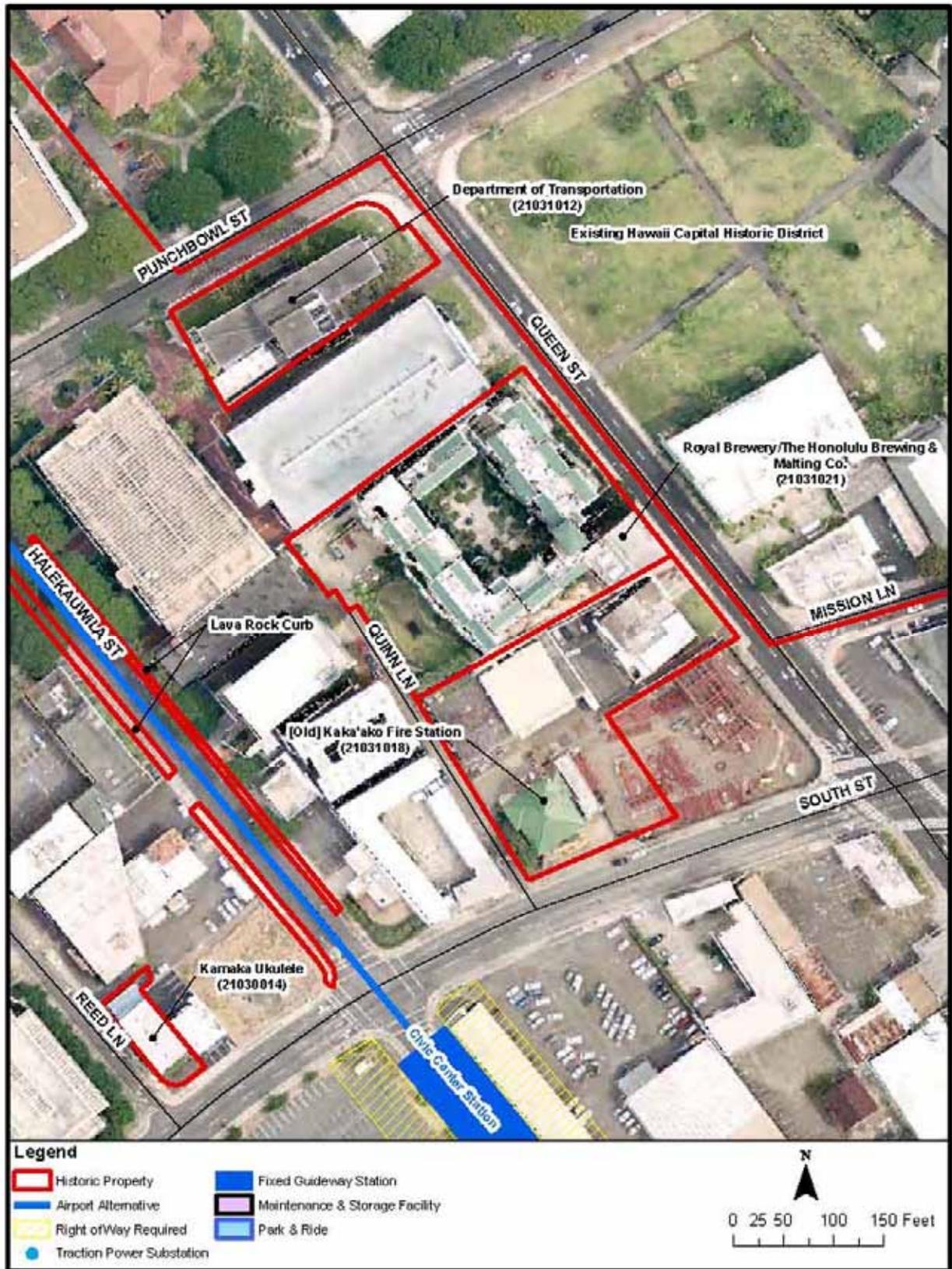


Figure 209. [Old] Kaka'ako Fire Station



Figure 210. [Old] Kaka'ako Fire Station, facing makai along South St from property to Halekauwila Street



Figure 211. [Old] Kaka'ako Fire Station, facing Koko Head from property

5.75 Kamaka Ukulele

25 Civic Center Station sector—TMK 21030014
550 South Street

Property description

Kamaka Ukulele was built in 1958. It is a modest, single-story building constructed of concrete hollow tiles. The surrounding area contains high-rise residential buildings, low-rise light industrial buildings, and surface parking lots. The resource was determined to be eligible under Criterion A for its association with ukulele manufacturing in Hawai'i.

Project description in vicinity of property

The Project would be located one block mauka of the Kamaka Ukulele on Halekauwila Street (see Figure 212 and TMK as shown on Drawing HP022 in Appendix A). The makai edge of the guideway would be approximately 150 to 175 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-25 in Appendix B and would be elevated approximately 40 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on Kamaka Ukulele. Construction of the guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside of the NRHP boundary.

Location: No Effect. Kamaka Ukulele retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. Kamaka Ukulele retains integrity of design, although the property has few distinguishing characteristics. The Project would not alter the integrity of design, and all project-related work would occur outside of the building's NRHP boundaries.

Setting: No Adverse Effect. Kamaka Ukulele is located in a developed urban area that contains surrounding light industrial and high-rise residential buildings. The project would be visible from select areas of Kamaka Ukulele, but would not be visible from other portions. The building has few exterior openings, and none are oriented directly toward the guideway's path. The guideway would run a block away from the building and would only be visible from Kamaka Ukulele's parking area. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility under Criterion A. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into Kamaka

Ukulele's setting, there would be an effect, but these changes would be extremely minor and would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. Kamaka Ukulele retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All Project work would occur outside of Kamaka Ukulele's NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. Kamaka Ukulele retains integrity of workmanship, although the modest building is largely devoid of character-defining workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the building. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. Kamaka Ukulele has minimal integrity of feeling, primarily because it does not convey its historic use as a ukulele manufacturer. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character and would not affect the integrity of feeling.

Association: No Effect. Kamaka Ukulele also has a minimal level of integrity of association. It does not visibly demonstrate its role as a ukulele manufacturer in Honolulu. The Project would not affect any of the property's historically significant physical features—primarily because there are not any exterior features that tie the resource to ukulele manufacturing. The Project would not alter Kamaka Ukulele's integrity of association.

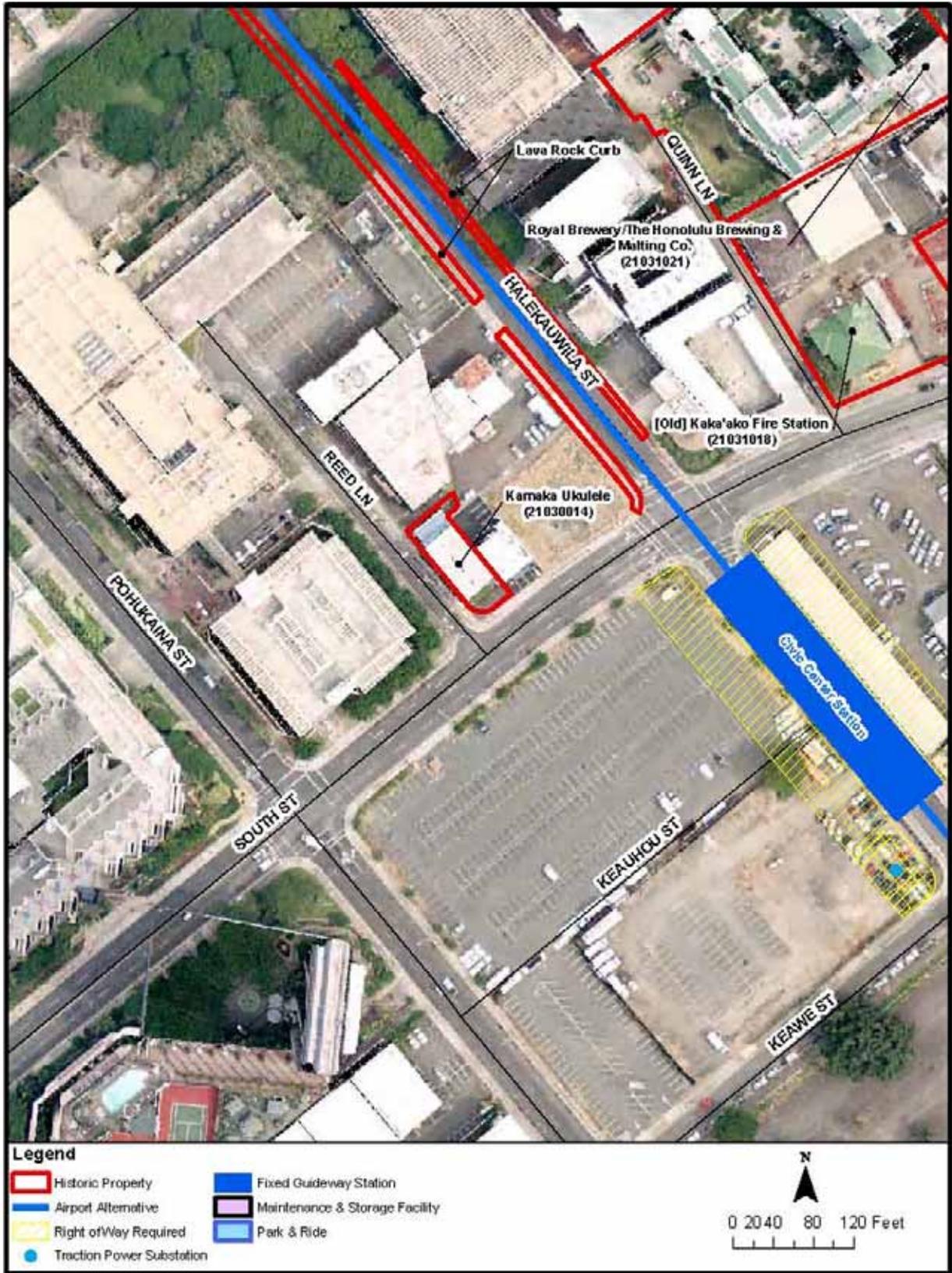


Figure 212. Kamaka Ukulele



Figure 213. Kamaka Ukulele, detail of property's façade



Figure 214. Kamaka Ukulele, facing mauka/Koko Head from property to Halekauwila Street

5.76 Mother Waldron Playground

25 Civic Center Station sector—TMKs 21051005, 21051006
Halekauwila, Coral, and Pohukaina Streets

Property description

Mother Waldron Playground is a nearly 2-acre park that features impressive Art Deco architectural and landscape design elements. Earlier documentation reports that the park, designed by Harry Sims Bent and built in 1937, was listed in the National Register of Historic Places on June 9, 1988, as part of the City and County of Honolulu Art Deco Parks and Playground Thematic Registration. However, a review of National Register listings on the National Park Service database on February 24, 2009, does not show the playground as being formally listed. However, the property is listed in the Hawai'i Register of Historic Places. It is eligible under Criterion A for its association with the nationwide playground movement, and Criterion C as an excellent example of Art Deco architectural and landscape design by Bent.

Project description in vicinity of property

The Project would be located directly mauka of Mother Waldron Playground on Halekauwila Street. The Civic Center Station would be located more than one block 'Ewa of the playground (see Figure 215 and TMKs as shown on Drawing HP022 in Appendix A). The makai edge of the guideway would be approximately adjacent to the property's mauka NRHP boundary. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-26 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have an **Adverse Effect** on Mother Waldron Playground. Placement of the guideway would adversely affect the property's integrity of setting, although the Project would be outside the NRHP boundary. Although the park does contain built resources, its primary use is as an outdoor recreational facility, and the Project's guideway would adversely affect the playground.

Location: No Effect. Mother Waldron Playground retains integrity of location and this would not be altered by the Project. The playground and its built resources would remain in its current location.

Design: No Effect. Mother Waldron Playground retains a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the NRHP boundary.

Setting: Adverse Effect. Mother Waldron Playground is in a developed urban area that contains surrounding new construction and contemporary built resources. Unlike many other resources within the Project's APE, Mother Waldron Playground is

primarily an outdoor designed space, although it does contain a comfort station. Generally, the effects on building settings are different than those on a resource that is primarily an outdoor facility. While these recently constructed adjacent buildings detract from the playground's overall historic setting, the surrounding buildings are separated from the playground by the streets that encircle the playground. Because the guideway would introduce a new element into Mother Waldron Playground's setting in a close proximity, an affect that is particularly apparent to an outdoor resource, there would be an adverse effect. No audible or atmospheric effects to this property were identified.

Materials: No Effect. Mother Waldron Playground retains integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. Mother Waldron Playground retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: Adverse Effect. Mother Waldron Playground has a high level of integrity of feeling. It conveys its origins as a New Deal-era park/playground. The Project would not affect any of the property's physical features, but it would diminish the property's expression of its historic character, primarily because of the close proximity of the guideway to the open air resource. The project would alter the property's integrity of feeling.

Association: Adverse Effect. Despite surrounding new construction, Mother Waldron Playground has a high level of integrity of association. It continues to demonstrate its role in the construction of New Deal and progressive-era public improvement projects in Honolulu. The Project would not affect any of the property's physical features, but it would alter the integrity of association because it would be out of character with the historic appeal of the 1930s Mother Waldron Playground.



Figure 215. Mother Waldron Playground



Figure 216. Mother Waldron Playground, facing mauka/Ewa from property to Halekauwila Street



Figure 217. Mother Waldron Playground, facing Koko Head from property along Halekauwila Street



Figure 218. Visual Simulation: Halekauwila Street/Cooke Street Intersection, looking mauka from Mother Waldron Playground



Figure 219. Visual Simulation: Halekauwila Street/Cooke Street Intersection, looking 'Ewa along Halekauwila Street, Mother Waldron Playground visible on left

5.77 Fuji Sake Brewing Co.

26 Kaka'ako Station sector—TMK 21052008
539 Cooke Street

Property description

The Fuji Sake Brewing Co. building, constructed in 1938, is a modest light industrial building constructed of masonry covered with stucco. It features minimal Art Moderne architectural details, and the historic portion appears to have been constructed in two phases. The brewery was determined to be eligible for listing in the National Register of Historic Places under Criterion C as an example of Art Moderne styling applied to a light industrial building. However, a large addition that surpasses the size of the brewery's historic portion is attached. It appears that only the earlier portions of the being are designated. Outside of the proposed NRHP boundary, the surrounding area is developed with high-rise and low-rise condominiums and other light industrial buildings. Most of these buildings have been recently constructed. Mother Waldron Park is adjacent to the brewery.

Project description in vicinity of property

The Project would include construction of an elevated, fixed-guideway rail structure in the median of Halekauwila Street, directly adjacent to the brewery, mauka of the NRHP boundary (see Figure 220 and TMK as shown on Drawing HP023 in Appendix A). The makai edge of the guideway would be approximately 75 feet from the mauka edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-27 in Appendix B and would be elevated approximately 30 to 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Fuji Sake Brewing Co. building. The setting surrounding the brewery is substantially compromised by recently constructed buildings, and all project work would occur outside of the NRHP boundary.

Location: No Effect. The brewery retains integrity of location that would not be affected by the Project. The building would remain in its current location.

Design: No Effect. The building's integrity of design has been compromised by the large addition that is attached to the historic portion. The guideway would not alter the building's current integrity of design. The Project would be located outside of the brewery's NRHP boundary.

Setting: No Adverse Effect. The brewery's historic setting is substantially compromised by numerous recently constructed buildings, including high-rise and low-rise condominiums. The guideway would be located in the middle of Halekauwila Street, running adjacent to the brewery. Although placement of the guideway would alter the property's setting and viewshed, the brewery site does not contain

historically significant views or vistas that would be altered by the guideway's presence. Furthermore, no audible or atmospheric effects to this property were identified. Therefore, no historically significant visual features within the property's setting would be altered. Because the historic setting of the building is compromised, and because the setting is not a significant aspect of the reason that this property was determined to be eligible for listing in the National Register, the Project would have no adverse effect on the brewery's integrity of setting.

Materials: No Effect. The brewery's integrity of materials has been diminished by the replacement of select doors and windows. The Project would not alter the current integrity of materials. All project work would occur outside of the brewery's NRHP boundary.

Workmanship: No Effect. The brewery has a moderate level of integrity of workmanship as a result of alterations. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Adverse Effect. The building's light industrial use remains apparent, although it is not readily identifiable as a brewery. The integrity of feeling is minimal because of alterations to the building and its surrounding area. The introduction of the guideway will not affect the property's ability to convey its remaining historic character and would not alter the building's integrity of feeling.

Association: No Adverse Effect. The property does not have a high level of integrity of association. Changes to the building and complex, as well as to the surrounding area, have compromised this aspect of integrity. The Project would not affect the brewery's limited ability to convey its historic use and would not affect the integrity of association.



Figure 220. Fuji Sake Brewing Co.



Figure 221. Fuji Sake Brewing Co., facing mauka/'Ewa from property to Halekauwila Street



Figure 222. Fuji Sake Brewing Co., facing mauka/Koko Head from property along Halekauwila Street

5.78 Ching Market and House

26 Kaka'ako Station sector—TMK 21050049
449 Kamani Street

Property description

Ching Market and House were built in 1912 and comprise a shop and plantation house. Both are of two-story frame construction. The market is an interesting example of the Italianate style applied to a wood commercial building, and the residence is an example of a plantation-style house. The property was determined to be eligible for the NRHP under Criterion A for its role in the surrounding neighborhood's development, and under Criterion C for its stylistic elements applied to a frame structure.

Project description in vicinity of property

The Project would be located one-and-one-half blocks makai of the Ching Market and House on Halekauwila Street (see Figure 223 and TMK as shown on Drawing HP023 in Appendix A). The mauka edge of the guideway would be approximately 350 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Effect** on the Ching Market and House. The guideway would not affect the property's integrity, although the Project would introduce new elements into areas outside of the NRHP boundary and its viewshed.

Location: No Effect. The Ching Market and House retain integrity of location, and this will not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Ching Market and House retain a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside of the building's NRHP boundaries.

Setting: No Effect. The Ching Market and House are in a developed urban area that contains surrounding new construction and contemporary built resources. The Project would not be visible from the Ching Market and House. The guideway would be located one-and-one-half blocks from the buildings. The guideway would run opposite of the buildings' main orientation, and would be screened by numerous intervening structures. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility; field views during the assessment of effects verified this. Furthermore, no audible or atmospheric effects to this property were identified. Because the

guideway and station would not introduce a new element into the Ching Market and House's setting, there would be no effect to the setting.

Materials: No Effect. The Ching Market and House retain a high level of integrity of materials. The Project would not alter the current integrity of materials. All project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Ching Market and House retain integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Ching Market and House have a high level of integrity of feeling. They convey their origins as an early 20th-century market and residence. Few similar frame shops or houses from this era appear to survive in Honolulu. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. Numerous other changes to the building's environment have not affected the Ching Market and House's integrity of feeling, nor would the Project.

Association: No Effect. The Ching Market and House have a high level of integrity of association. They continue to demonstrate their origins as an early 20th-century shop and residence in Honolulu. The Project would not affect any of the property's historically significant physical features. The Project also would not alter this integrity of association, which is directly related to the building and not the surrounding environment.



Figure 223. Ching Market and House



Figure 224. Ching Market and House, facing makai/Ewa along Queen Street from Ward Avenue to property



Figure 225. Ching Market and House, facing makai from Queen Street to Ilaniwai Street, property on left

5.79 American Savings Bank/Liberty Bank— Queen-Ward Branch

26 Kaka'ako Station sector—TMK 21050052
929 Queen Street

Property description

The American Savings Bank/Liberty Bank was built in 1962 and designed by Kenneth W. Roehrig. It is two stories in height with tall vertical columns that form a peristyle and support a grille that shields full-height windows on the second story. The property was determined to be eligible for the NRHP under Criterion C as an example of architecture that evokes elements used in the Hawai'i State Capitol Building, which spawned several homages.

Project description in vicinity of property

The Project would be located one-and-one-half blocks makai of the American Savings Bank/Liberty Bank on Halekauwila Street as it crosses Ward Avenue (see Figure 226 and TMK as shown on Drawing HP023 in Appendix A). Kaka'ako Station would be located in a current parking lot near this intersection. The edge of the guideway would be approximately 300 to 350 feet from the edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-1 in Appendix B and would be elevated approximately 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the American Savings Bank/Liberty Bank. The guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside of the NRHP boundary.

Location: No Effect. The American Savings Bank/Liberty Bank retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The American Savings Bank/Liberty Bank retains a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the building's NRHP boundary.

Setting: No Adverse Effect. The American Savings Bank/Liberty Bank is in a developed urban area that contains surrounding new construction and contemporary built resources. The project would be visible from select areas of the American Savings Bank/Liberty Bank, but would not be visible from other portions. However, because of the distance and surrounding construction, it would not dominate views. The guideway would run a block-and-a-half from the building's secondary elevation, opposite of its main orientation. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating

to its eligibility under Criterion C, and additional field views verified that the setting does not relate to the building's significance. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the American Savings Bank/Liberty Bank's setting, there would be an effect, but these changes are minimal and would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The American Savings Bank/Liberty Bank retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All Project work would occur outside of the NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The American Savings Bank/Liberty Bank retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The American Savings Bank/Liberty Bank retains its integrity of feeling. It conveys its origins as a 1950s mid-century Modern building and specifically evokes the State capitol's architecture. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The project would introduce a new component into the adjacent setting. However, numerous other changes to the building's environment have not affected the American Savings Bank/Liberty Bank's integrity of feeling, nor would the Project.

Association: No Effect. The American Savings Bank/Liberty Bank has a high level of integrity of association. It continues to demonstrate its role in the construction of mid-century Modern buildings in Honolulu at a time when the City was striving to establish itself as a forward-thinking, urban area. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.



Figure 226. American Savings Bank/Liberty Bank—Queen-Ward Branch



Figure 227. American Savings Bank/Liberty Bank—Queen-Ward Branch, facing makai/Ewa from Ward Avenue to property



Figure 228. American Savings Bank/Liberty Bank—Queen-Ward Branch, facing makai from Queen Street to Ward Avenue, property on right

5.80 Pacific Development Office Building
27 Ala Moana Station sector—TMK 23007029
1202 Waimanu Street Property description

The Pacific Development Office Building was built in 1954 and designed by Chan Jay Kim, a civil engineer. It is one story in height with a flat roof. It has a rounded corner, canted windows, and Roman brick. The property was determined to be eligible for the NRHP under Criterion C as an example of a mid-century office building.

Project description in vicinity of property

The Project would be located 'Ewa of the Pacific Development Office Building as it crosses an adjacent block diagonally, and then moves mauka on Kona Street (see Figure 229 and TMK as shown on Drawing HP024 in Appendix A). The mauka edge of the guideway would be approximately 75 to 150 feet from the makai edge of the NRHP boundary and the building. The design of the guideway in this area would correspond to the typical sections illustrated as Figures B-1 and B-28 in Appendix B and would be elevated approximately 30 to 35 feet above grade.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Pacific Development Office Building. The guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Pacific Development Office Building retains integrity of location that would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Pacific Development Office Building retains a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the building's NRHP boundary.

Setting: No Adverse Effect. The Pacific Development Office Building is in a developed urban area that contains surrounding new construction and contemporary built resources. The Project would be visible from select areas of the building, but would not be visible from other portions, and it would remain a substantial distance from the building. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility under Criterion C, and field assessments verified that the setting is not a character-defining feature of the building. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the building's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Pacific Development Office Building retains a moderate level of integrity of materials. Doors have been replaced, and the interior has been completely altered. The Project would not alter the current integrity of materials. All project work would occur outside the building's NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Pacific Development Office Building retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Pacific Development Office Building has a high level of integrity of feeling. It conveys its origins as a modest 1950s mid-century Modern building with unique character. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The project would introduce a new component into the adjacent setting. However, other changes to the building's environment have not affected the building's integrity of feeling, and the Project would also not affect this.

Association: No Effect. The Pacific Development Office Building has a high level of integrity of association. It continues to demonstrate its role in the construction of modest mid-century Modern buildings in Honolulu at a time when the City was striving to establish itself as a forward-thinking, urban area. The Project would not affect any of the property's historically significant physical features. The Project would not alter this integrity of association, which is directly related to the building and not the surrounding environment.

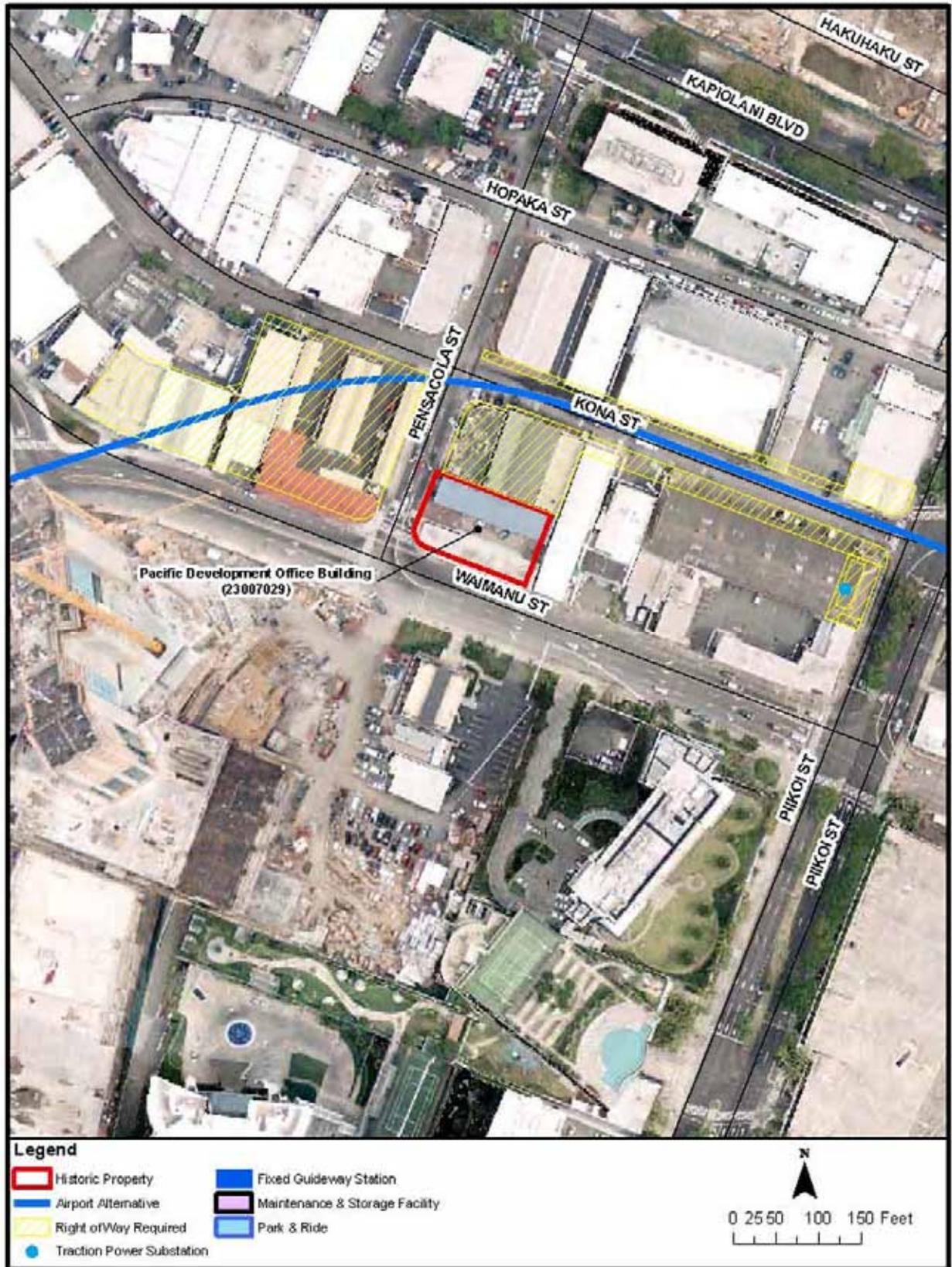


Figure 229. Pacific Development Office Building



Figure 230. Pacific Development Office Building, facing mauka along Pensacola Street from Waimanu Street, property on right



Figure 231. Pacific Development Office Building, facing mauka/Koko Head from Waimanu Street at Pensacola Street to property

5.81 Hawaiian Life Building

27 Ala Moana Station sector—TMK 23039023
1319 Kapi'olani Boulevard

Property description

The Hawaiian Life Building was built in 1951 by master architect Vladimir Ossipoff. It is six stories in height with tall vertical louvers that form a sunscreen and are painted in various hues. Several cantilevered components extend from the building. The property was determined to be eligible for the NRHP under Criterion C as an example of master architect Ossipoff's early commercial design work.

Project description in vicinity of property

The Project would be located directly makai of the Hawaiian Life Building on Kona Street (see Figure 232 and TMK as shown on Drawing HP024 in Appendix A). The mauka edge of the guideway would be approximately 25 feet from the makai edge of the NRHP boundary and 75 feet from the building. The design of the guideway in this area would correspond to the typical section illustrated as Figure B-29 in Appendix B and would be elevated approximately 40 to 50 feet above grade.

Application of criteria of adverse effect

Summary of finding: The Project would have **No Adverse Effect** on the Hawaiian Life Building. The guideway would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Hawaiian Life Building retains integrity of location and this would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Hawaiian Life Building retains a high level of integrity of design. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the building's NRHP boundary.

Setting: No Adverse Effect. The Hawaiian Life Building is in a developed urban area that contains surrounding new construction and contemporary built resources. The Project would be visible from select areas of the building, but would not be visible from other portions. The guideway would run adjacent to the building's secondary elevation, which contains a loading dock, opposite its main orientation, and would be screened by existing trees. The recently completed NRHP determination of eligibility does not discuss the building's setting as an important component relating to its eligibility under Criterion C. However, efforts to avoid affects to the existing tree canopy, which appears to extend beyond the parcel boundary, would be made, and no work on the parcel itself is anticipated. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the building's setting, there would be an effect, but these changes would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Hawaiian Life Building retains a high level of integrity of materials. The Project would not alter the current integrity of materials. All Project work would occur outside of the building's NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Hawaiian Life Building retains integrity of workmanship. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Hawaiian Life Building has a high level of integrity of feeling. It conveys its origins as a 1950s mid-century Modern building and exhibits a higher quality of design than many other buildings of its era. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The Project would introduce a new component into the adjacent setting. However, numerous other changes to the building's environment have not affected the Hawaiian Life Building's integrity of feeling, nor would the Project.

Association: No Effect. The Hawaiian Life Building also has a high level of integrity of association. It continues to demonstrate its role in the construction of mid-century Modern commercial buildings in Honolulu at a time when the City was striving to establish itself as a forward-thinking, urban area. It is among the City's more successful forays into Modern design, surely the result of Ossipoff's design skill. The Project would not affect any of the property's historically significant physical features. The Project also would not alter the integrity of association, which is directly related to the building and not the surrounding environment.

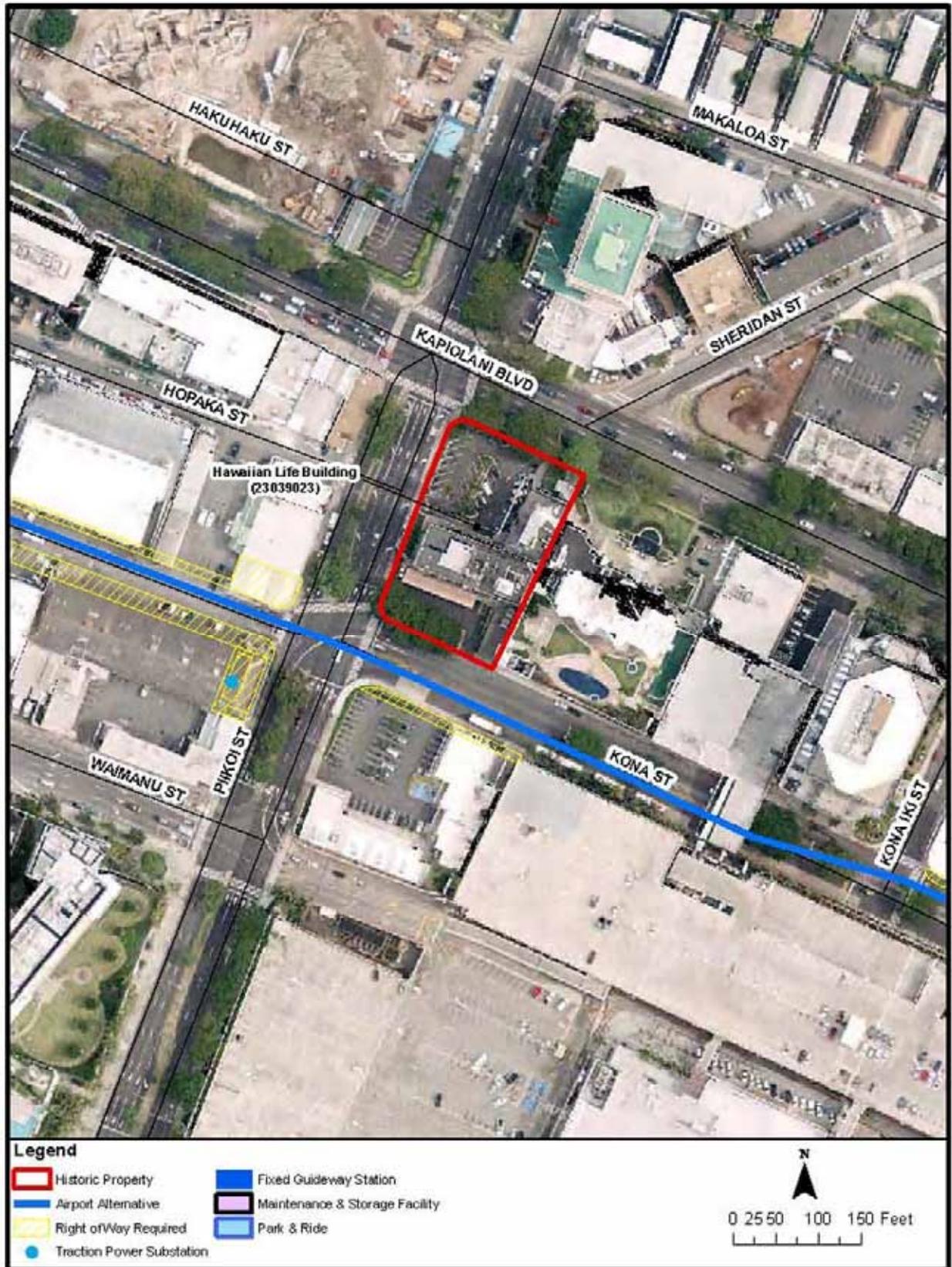


Figure 232. Hawaiian Life Building



Figure 233. Hawaiian Life Building, facing makai to property and entrance from Kapi'olani Boulevard



Figure 234. Hawaiian Life Building, facing mauka to rear of property from Kona Street

5.82 Ala Moana Building

27 Ala Moana Station sector—TMK 23039001
1441 Kapi'olani Boulevard

Property description

The Ala Moana Building, constructed in 1959, was reportedly the world's tallest pre-stressed concrete building when it was built. The rectangular tower is 23 stories in height and is topped by a round room that used to serve as a revolving restaurant. Original 12-foot sun louvers that formerly covered the building's many windows were removed circa 2000 and replaced with horizontal sun screens. The Ala Moana Building was determined to be eligible for listing in the National Register of Historic Places under Criterion C as an example of a pioneering pre-stressed concrete building and also for its association with master architect John Graham & Co. The surrounding area contains numerous recently constructed resources, most notably the massive Ala Moana Center shopping complex as well as many tall residential buildings.

Project description in vicinity of property

The Project would be located directly makai of the Ala Moana Building on Kona Street. Current project plans show the Ala Moana Center Station would be just 'Ewa of the building, also on Kona Street (see Figure 235 and TMK as shown on Drawing HP024 in Appendix A). The Koko Head terminal end of the guideway would be approximately 10 feet from the makai/'Ewa edge of the NRHP boundary and the building. The guideway's design in this area would correspond to the typical section illustrated as Figure B-30 in Appendix B and would be elevated approximately 50 feet above grade. The design of the station in this area would correspond to the plan and typical section illustrated as Figures C-3 and C-14 in Appendix C.

Application of criteria of adverse effect

Summary finding: The Project would have **No Adverse Effect** on the Ala Moana Building. The guideway and station would not adversely affect the property's integrity, although the Project would introduce new elements into areas outside the NRHP boundary.

Location: No Effect. The Ala Moana Building retains integrity of location that would not be altered by the Project. The building would remain in its current location.

Design: No Effect. The Ala Moana Building retains moderate integrity of design. Changes made to the property include the removal of character-defining louvers that were replaced by non-historic sunscreens. The Project would not alter the remaining integrity of design, and all project-related work would occur outside the building's NRHP boundary.

Setting: No Adverse Effect. The Ala Moana Building is in a developed urban area that contains surrounding new construction and contemporary built resources. The

Project would be visible from select areas of the Ala Moana Building but would be extremely low in height when compared to the building itself. The building's setting is not a significant component relating to its NRHP eligibility. This is not specifically described in the recently completed eligibility assessment, nor is it evident upon additional field views. Numerous areas of the building would have no view of the Project. The Ala Moana Center Station would be visible from select areas of the building, but would be small in scale when compared to the building. Furthermore, no audible or atmospheric effects to this property were identified. Because the guideway and station would introduce a new element into the Ala Moana Building's setting, there would be an effect, but the scale of the Project, coupled with the lack of integrity of the existing setting, would result in a determination of No Adverse Effect to the setting.

Materials: No Effect. The Ala Moana Building retains moderate integrity of materials. Former character-defining louvers have been removed. The Project would not alter the current integrity of materials. All project work would occur outside the building's NRHP boundary, and the Project would not affect the integrity of materials.

Workmanship: No Effect. The Ala Moana Building retains integrity of workmanship, with the exception of the alterations described above. The Project would not affect any characteristics or features related to the workmanship used to create the property's historic elements. No project activity would occur within the NRHP boundary. Therefore, the Project would have no effect to the property's integrity of workmanship.

Feeling: No Effect. The Ala Moana Building has a moderate level of integrity of feeling. It conveys its origins as a mid-century Modern building, although removal of character-defining features has compromised its integrity of feeling. The Project would not affect any of the property's physical features or diminish the property's expression of its historic character. The project would be a relatively minor component in the building's built environment and would not alter its integrity of feeling.

Association: No Effect. The Ala Moana Building has a moderate level of integrity of association. It continues to demonstrate its role in the construction of mid-century Modern buildings in Honolulu at a time when the City was striving to establish itself as a forward-thinking, urban area. The Project would not affect any of the property's physical features. The Project also would not alter the integrity of association because it would not be out of character with the modern appeal of the Ala Moana Building.

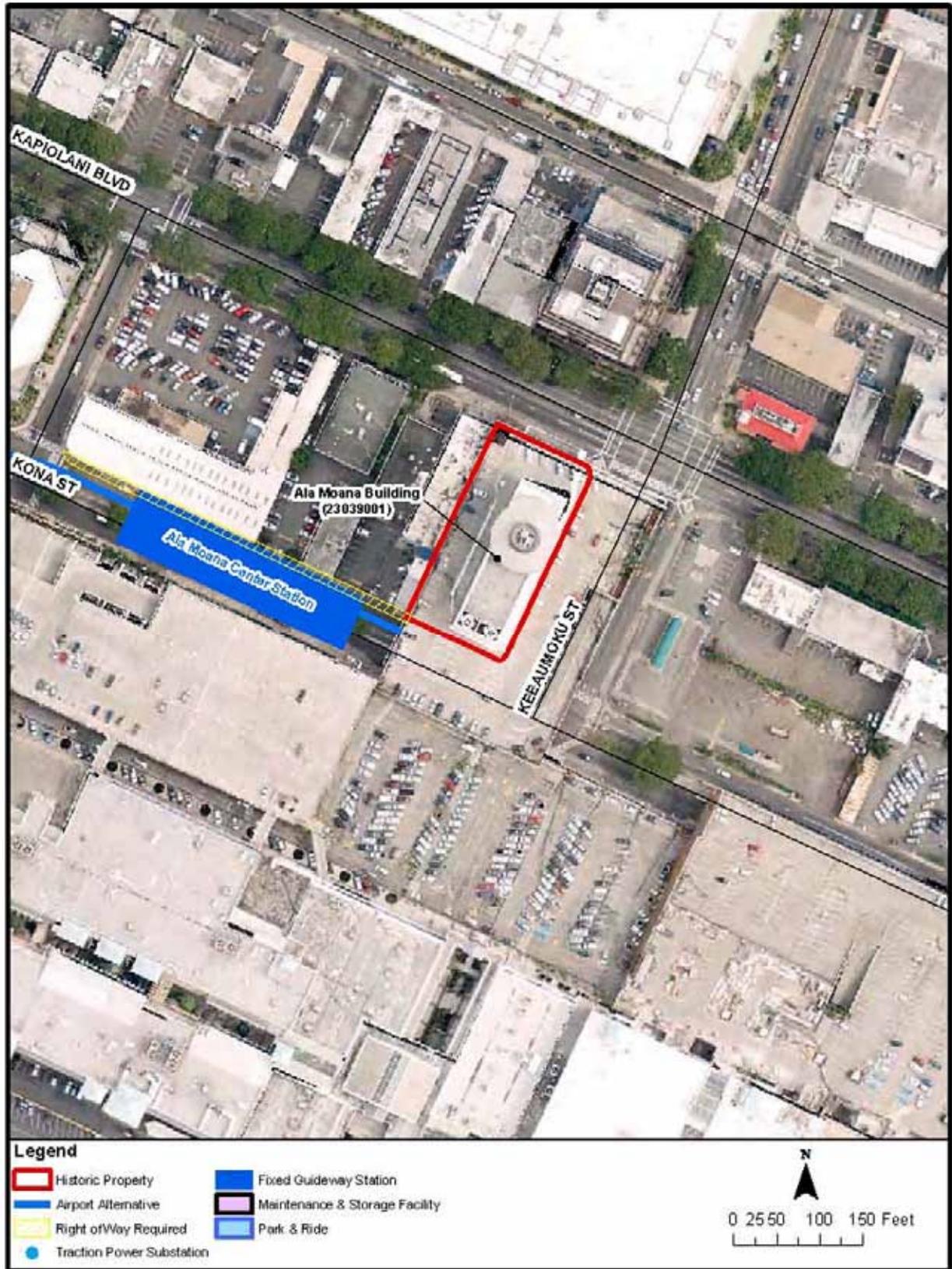


Figure 235. Ala Moana Building



Figure 236. Ala Moana Building, facing Koko Head from Kona Iki Street at Kapi'olani Boulevard to property

6.0 Summary of Effect Determinations

Of 81 NRHP-listed and NRHP-eligible historic properties, the Project would have an adverse effect to 22 properties. Of the remaining properties, the Project would have no adverse effect to 51 properties and no effect to 8 properties. Table 2 lists all historic properties and the determination of effect for each.

Table 2: Summary of Effect Determinations

Property Name	TMK Parcel No.	Effect
Station Sectors 6 through 8: East Kapolei, UH West O'ahu, and Ho'opili from Kalo'i Gulch to Fort Weaver Road/Kunia Road		
Hono'uli'uli Stream Bridge	N/A	Adverse Effect
Station Sector 9: West Loch from Fort Weaver Road/Kunia Road to Waikele Stream		
Lum-Terahira Three-story Apartments	94039082	No Adverse Effect
West O'ahu Christian Church/former American Security Bank (round plan)	94027127	No Adverse Effect
Tanaka-Ishihara House	94025008	No Adverse Effect
Station Sector 10: Waipahu Transit Center from Waikele Stream to Waipi'o Point Access Road/Kahualii Street		
Waikele Stream Bridge eastbound span and Bridge over OR&L spur	N/A	Adverse Effect
Ohara & Okahara Two-story Apartments	94019021, 94019020	No Effect
Codera-Carvalho Two-story Apartments/Waipahu Hale	94017043	No Adverse Effect
Waipahu Hawai'i Stake, Church of Jesus Christ of Latter-Day Saints	94036071	No Adverse Effect
Station Sector 11: Leeward Community College from Waipi'o Point Access Road/Kahualii Street to the H-1 Freeway (at Farrington Highway overpass)		
Watercress of Hawai'i	96003026	No Adverse Effect
Station Sector 12: Pearl Highlands from the H-1 Freeway (at Farrington Highway overpass) to 'Ewa side of Hawaiian Electric Company (HECO) Waiau		
Waiawa Stream Bridge 1932 (westbound lanes)	N/A	Adverse Effect
Waiawa Separation Bridge	N/A	No Adverse Effect
Waiawa Stream Bridge 1952 (eastbound lanes)	N/A	No Adverse Effect
Waiawa Booster Pump Station	96003045	No Effect

Property Name	TMK Parcel No.	Effect
Station Sector 13: Pearlridge from 'Ewa side of HECO Waiiau to 'Ewa side of 'Aiea Cemetery		
HECO Waiiau Plant	98003010	No Adverse Effect
Nishi Service	98006024	No Adverse Effect
Waimalu Stream Bridge	N/A	Adverse Effect
Waimalu Shopping Center	98022074, 98022081	No Adverse Effect
Sumida Watercress Farm	98016047	No Adverse Effect
Kalauao Spring Bridge	N/A	Adverse Effect
Kalauao Stream Bridge	N/A	Adverse Effect
Forty Niner Saimin	98018042	No Adverse Effect
Akiona House (Quonset)	98018041	No Effect
Station Sector 14: Aloha Stadium from 'Ewa side of 'Aiea Cemetery to road to CINCPAC Landing		
'Aiea Cemetery/Honolulu Plantation Cemetery	99012006, 99012001	No Adverse Effect
Station Sector 35: Richardson Recreation Center from road to CINCPAC Landing to Arizona Street/Hālawā Drive intersection with Kamehameha Highway		
United States Naval Base Pearl Harbor National Historic Landmark	Various	No Adverse Effect
Bombproof Switch Station – Fac. B-6	99003038	No Effect
Richardson Recreation Center Pool Complex (Swimming Pool – Fac. S-21; Recreation Facility – Fac. 1; Bath House/Locker Room – Fac. 2; Handball Court – Fac. S-20)	99003029	No Adverse Effect
Kamehameha Highway Bridge over Hālawā Stream (mauka span)	N/A	No Adverse Effect
Commander-in-Chief Pacific Fleet (CINCPACFLT) Headquarters – Fac. 250 National Historic Landmark	99002004	No Adverse Effect
Fuel Oil Pump-out Pump House, – Fac. S-386	99001001	No Adverse Effect

Property Name	TMK Parcel No.	Effect
Station Sector 36: Pearl Harbor Naval Base (Airport Alternative) from Arizona Street/Hālawā Drive to Valkenburgh Street		
Publications Printing Office and Plant – Fac. 550/District Printing Plant	99001008	No Adverse Effect
Potential Makalapa Navy Housing Historic District	99002004	Adverse Effect
Navy Upper Tank Farm (fuel storage)	99001008	No Adverse Effect
Ossipoff's Aloha Chapel, SMART Clinic, and Navy-Marine Corps Relief Society, – Fac. 1514	99001008	Adverse Effect
Potential Little Makalapa Navy Housing Historic District	99002004	No Adverse Effect
Navy WWII splinterproof shelter – Fac. S-51	99001008	No Adverse Effect
Rehab Center/Former Navy Fire Station – Fac. 199	99001008	No Adverse Effect
Station Sector 38: Lagoon Drive (Airport Alternative) from Paiea to Middle Streets		
Hawai'i Employers Council	11016004	Adverse Effect
Station Sector 19: Middle Street from Middle to Laumaka Streets/'Ewa Side of OCCC		
Gaspro Store	12013007	No Adverse Effect
Foremost Dairy	12013006	No Adverse Effect
Station Sector 20: Kalihi from Laumaka Street/'Ewa Side of OCCC to Waiakamilo Road		
Lava Rock Curbs	N/A	Adverse Effect
Pu'uhale Market	12012014	No Adverse Effect
Afuso House	12009017	Adverse Effect
Higa Four-plex	12009017	Adverse Effect
Teixeira House	12009018	Adverse Effect
Pang Craftsman-style House	12009060	No Adverse Effect
10 Courtyard Houses	12002113	No Adverse Effect
Duarte House	12002108	No Adverse Effect
Boulevard Saimin	15029060	No Adverse Effect
Station Sector 21: Kapālama from Waiakamilo Road to Akepo Lane		
True Kamani Trees	N/A	Adverse Effect
Kapālama Canal Bridge	N/A	Adverse Effect
Six Quonset Huts	15015008	No Adverse Effect

Property Name	TMK Parcel No.	Effect
Station Sector 22: Iwilei from Akepo Lane to Iwilei Road		
Institute for Human Services/Tamura Building	15007033	Adverse Effect
Tong Fat Co.	15007003	No Adverse Effect
Wood Tenement Buildings behind Tong Fat Co.	15007003	No Adverse Effect
O'ahu Railway & Land Co. Terminal Building	15007001, 15007002	Adverse Effect
O'ahu Railway & Land Co. Office and Document Storage Building	15007001, 15007002	Adverse Effect
Former filling station on OR&L property	15007001	No Adverse Effect
O'ahu Railway & Land Co. basalt paving blocks	15007001, 15007002	No Adverse Effect
Station Sector 23: Chinatown from Iwilei Road to Queen Street		
Nu'uuanu Stream Bridge	N/A	Adverse Effect
Chinatown Historic District	Portions of plat maps 17002, 17003, 17004	Adverse Effect
Harbor retaining wall of coral blocks from Honolulu Fort	21001056	No Adverse Effect
Merchant Street Historic District (incl. Walter Murray Gibson Building/Honolulu Police Station)	21002024, 21002057	No Adverse Effect
Station Sector 24: Downtown from Queen to Punchbowl Streets		
DOT Harbors Division	21001005	No Adverse Effect
Pier 10/11	21001001	No Adverse Effect
Walker Park	N/A	No Adverse Effect
Irwin Park	21013007	No Adverse Effect
Aloha Tower	21001013	No Adverse Effect
Dillingham Transportation Building	21014003	Adverse Effect
HECO Downtown Plant and Leslie A. Hicks Building	21014006	No Adverse Effect
Hawai'i Capital Historic District (incl. Attorney General's Office/Hale 'Auhau)	21026022	No Effect
Station Sector 25: Civic Center from Punchbowl to Cooke Streets		
Department of Transportation	21031012	No Adverse Effect
Royal Brewery/The Honolulu Brewing & Malting Co.	21031021	No Effect
[Old] Kaka'ako Fire Station	21031018	No Effect
Kamaka Ukulele	21030014	No Adverse Effect
Mother Waldron Playground	21051005, 21051006	Adverse Effect

Property Name	TMK Parcel No.	Effect
Station Sector 26: Kaka'ako from Cooke to Waimanu Streets		
Fuji Sake Brewing Co.	21052008	No Adverse Effect
Ching Market and House	21050049	No Effect
American Savings Bank/Liberty Bank— Queen-Ward Branch	21050052	No Adverse Effect
Station Sector 27: Ala Moana Center from Waimanu to Mahukona Streets		
Pacific Development Office Building	23007029	No Adverse Effect
Hawaiian Life Building	23039023	No Adverse Effect
Ala Moana Building	23039001	No Adverse Effect

Acronyms, Abbreviations, and Definitions

Adverse Effect	The effect of a Federal undertaking that may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association.
Area of Potential Effect (APE)	The geographic area within which a Federal undertaking may directly or indirectly alter the character or use of historic properties.
Association	Aspect of integrity. Association is the direct link between an important historic event or person and a historic property.
Code of Federal Regulations (CFR)	The codification of administrative laws as published in the <i>Federal Register</i> by the executive departments and agencies of the Federal government.
Contemporary	A property of approximate similar age.
Criteria of Adverse Effect	The standard by which a historic property is evaluated within the context of a proposed Federal undertaking to determine if that undertaking would result in an adverse effect to the historic property.
Design	Aspect of integrity. Design is the combination of elements that create the form, plan, space, structure, and style of a property.
'Ewa	Direction, toward the west.
Feeling	Aspect of integrity. Feeling is a property's expression of the aesthetic or historic sense of a particular period of time.
Guideway	The grade-separated, fixed guideway rail structure that will be the primary structure of the transit system.
Historic District	A group of historically related buildings, structures, and/or objects that are listed in the National Register of Historic Places as a unit.

Historic Property	A property or group of properties that have been listed in or have been determined eligible for listing in the National Register of Historic Places. For the purpose of this report, a historic property may be a building, a group of buildings, a historic district, a structure, or a group of objects. Historic properties were previously identified and recorded on individual forms and in the <i>Honolulu High-Capacity Transit Corridor Project Historic Resources Technical Report (DTS, 2008)</i> . Each form represents one historic property, unless forms documented multiple properties within an existing National Register or National Historic Landmark historic district, in which case the historic district is the historic property.
Integrity	The ability of a property to convey its historic significance. The aspects of integrity include location, design, setting, materials, workmanship, feeling, and association.
Koko Head	Direction, toward the east
Location	Aspect of integrity. Location is the place where the historic property was constructed or the place where the historic event occurred.
Makai	Direction, toward the ocean.
Materials	Aspect of integrity. Materials are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property.
Mauka	Direction, toward the mountains.
National Historic Landmark (NHL)	Historic properties considered nationally significant and possessing exceptional value or quality in illustrating or interpreting the heritage of the United States. NHLs are also concurrently listed in the National Register of Historic Places. Section 800.10 of the National Historic Preservation Act of 1966 (as amended) describes special requirements for protecting NHLs.
National Register Criteria for Evaluation	Criteria that define the scope of the National Register of Historic Places. They identify the range of resources and kinds of significance that will qualify properties for listing in the National Register. The criteria are written broadly to recognize the wide variety of historic properties associated with our prehistory and history.

National Register of Historic Places (NRHP)	The Federal government's list of historic places deemed worthy of preservation. The list is maintained by the National Park Service and contains districts, sites, buildings, structures, and objects determined to be of historic, cultural, architectural, archeological, or engineering significance at the national, state, or local level.
No Adverse Effect	The finding that the Project would alter a specific aspect of integrity for an individual historic property but the effect would not alter a characteristic that qualifies that property for inclusion in the NRHP in a manner that diminishes the significant aspect of integrity. Also the summary finding when a finding of "No Adverse Effect" is determined for any aspect of integrity for an individual historic property, but no effects are determined to be adverse.
No Effect	The finding that the proposed project would not alter a specific aspect of integrity for an individual historic property. Also the summary finding when no aspect of integrity for an individual historic property is altered.
Non-historic	Properties, structures, objects, features, and/or characteristics located within the setting of a historic property that were built after 1968 and do not contribute to the historic significance of the property.
National Register of Historic Places (NRHP)	The Federal government's list of historic places deemed worthy of preservation. The list is maintained by the National Park Service and contains districts, sites, buildings, structures, and objects determined to be of historic, cultural, architectural, archeological, or engineering significance at the national, state, or local level.
NRHP Boundary	The boundary within which a historic property is located. Prior documentation indicated that the boundaries for buildings and historic districts are generally legal parcel boundaries, or Tax Map Key (TMK) parcels, and the boundaries for structures and objects are the surface space or footprint the structures or objects occupy.
Project Team	Planners, designers, and technical specialists employed to develop the Project.
Resource	See Historic Property.

Section 106	Section 106 of the National Historic Preservation Act of 1966 (as amended) requires any Federal agency having direct or indirect jurisdiction over a Federal or federally assisted undertaking to consider the effect of that undertaking on any district, site, building, structure, or object that is included in or eligible for inclusion in the NRHP. Regulations and guidelines for ensuring compliance with Section 106 are set forth in 36 CFR 800, “Section 106 Regulations, Protection of Historic Properties.”
Setting	Aspect of integrity. Setting is the physical environment of a historic property.
State Historic Preservation Division (SHPD)	Hawai‘i’s state historic preservation office within the Department of Land and Natural Resources.
State Historic Preservation Officer (SHPO)	Reflects the interests of Hawai‘i and its citizens in the preservation of their cultural heritage. The SHPO advises and assists Federal agencies in carrying out Section 106 responsibilities and cooperates with such agencies, local governments, and organizations and individuals to ensure that historic properties are considered at all levels of planning and development. Hawai‘i’s SHPO is Laura H. Thielen, Chairperson of the Department of Land and Natural Resources.
Station Sector	An administrative subdivision of the transit corridor into segments to organize identification and documentation of historic properties. Station sectors were defined in previous documentation to include “properties in the blocks (or within a 500-foot radius) around each [transit] station and the parcels abutting the transit line on either side of the station.” Boundaries for station sectors were chosen to be approximately halfway between stations and to follow logical division lines where applicable, such as at streams, roads, or “notable” parcels.
Tax Map Key Parcel (TMK)	Real property parcel identified by its “tax map key” number.
Viewshed	The view from a historic property or a portion of a historic property to its surroundings.
Workmanship	Aspect of integrity. Workmanship is the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory.

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