

Appendix A

Correspondence

December 7, 2012	HART to NPS submission of Final HAER documentation for Waikele Canal Bridge and Highway Overpass (<i>missed insertion in January 2013 PA Semi-Annual Report</i>)
January 7, 2013	NPS to HART receipt and acceptance of Final HAER documentation for Waikele Canal Bridge and Highway Overpass (<i>missed insertion in January 2013 PA Semi-Annual Report</i>)
February 4, 2013	NPS to HART receipt and acceptance of Final HAER documentation for Waiawa, Waimalu, Kalauao Springs and Kalauao Stream Bridges
February 20, 2013	HART to SHPD submission of Stip V.E Digital Photography
February 20, 2013	HART to Municipal Reference Center submission of Stip V.E. Digital Photography
March 1, 2013	SHPD to HART Airport AISP Addendum Approval
March 1, 2013	SHPD to HART City Center AISP Addendum Approval
March 4, 2013	HART to SHPD first submission of Draft Airport AIS Report
March 29, 2013	SHPD to CSH Draft Airport AIS Report comments
April 8, 2013	HART to SHPD first submission of Draft City Center AIS Report
April 17, 2013	HART to SHPD Draft Mother Waldron Playground NR
April 23, 2013	SHPD to HART Draft Mother Waldron NR Comments
May 13, 2013	HART to NPS submission of Final HAER for Honouliuli Bridge
June 5, 2013	HART to SHPD Revised Draft Airport AIS Report
June 6, 2013	HART to SHPD Kapālama Canal Bridge Rail Replacements/Retrofit
June 18, 2013	SHPD to HART receipt and review of Final HAER documentation for Honouliuli Bridge
June 20, 2013	SHPD to HART Concurrence on Kapālama Canal Bridge Rails
July 12, 2013	CSH to SHPD Revised Draft Airport AIS Report
July 12, 2013	CSH to SHPD Revised Draft City Center AIS Report

Daniel A. Grabauskas
EXECUTIVE DIRECTOR AND CEO

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Wayne Y. Yoshioka

December 7, 2012

Ms. Christine Avery, Project Historian
U.S. Department of the Interior
National Park Service, Cultural Resources
909 First Avenue, Fifth Floor
Seattle, Washington 98104-1060

Dear Ms. Avery:

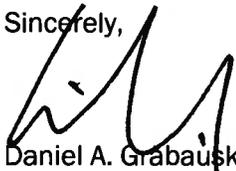
Subject: Waikele Canal Bridge and Highway Overpass (Waikele Canal Bridge)
Historic American Engineering Record (HAER) No. HI-100
Honolulu Rail Transit Project
Honolulu, Island of Oahu, Hawaii

Per Section 106 Programmatic Agreement, Stipulation V. Recordation and Documentation, Section C. Historic American Building Survey/HAER/Historic American Landscape Survey Recordation for the Honolulu High-Capacity Transit Corridor Project, the Honolulu Authority for Rapid Transportation (HART) is transmitting the Waikele Canal Bridge and Highway Overpass (Waikele Canal Bridge) HAER No. HI-100.

This document meets the requirements for HAER documentation and photography of the bridge outlined in Ms. Jackson-Retondo's letters dated June 29, 2011, and July 16, 2012 respectively. The final report has been printed on 25% cotton, lignin-free archival paper. Two archival copies are being submitted; one for the Historic American Engineering Record archive at the Library of Congress and one for the State Department of Land and Natural Resources, Historic Preservation Office. The cover card is being submitted on archival card stock. A CD is being submitted as well as one nonarchival (Xerox) copy of the report.

If you have any questions regarding this matter, please do not hesitate to contact Ms. Dawn Hegger, Environmental Documentation and Compliance Manager, at 808-768-6240 or dhegger@honolulu.gov.

Sincerely,



Daniel A. Grabauskas
Executive Director and CEO

Attachment: Waikele Canal Bridge and Highway Overpass HAER No. HI-100

cc: Department of Land and Natural Resources, State Historic Preservation Division
Department of Transportation, State of Hawaii

AP00ENV
CMS-00322



United States Department of the Interior



NATIONAL PARK SERVICE

Pacific West Region
333 Bush Street, Suite 500
San Francisco, CA 94104-2828

'13 JAN 11 P1 :48

H38 (PWR-CR)

January 7, 2013

Daniel A. Grabauskas
Executive Director and CEO
Honolulu Authority for Rapid Transportation
Alii Place, Suite 1700
1099 Alakea Street
Honolulu, Hawaii 96813

Re: Final HAER documentation, Waikele Canal Bridge and Highway Overpass (Waikele Canal Bridge),
Honolulu County, Hawaii (HAER No. HI-100)

Dear Mr. Grabauskas:

The National Park Service acknowledges the receipt of and accepts the final Historic American Engineering Record (HAER) documentation for the Waikele Canal Bridge and Highway Overpass, HAER No. HI-100.

The completed documentation will be transmitted to the Prints and Photographs Division of the Library of Congress. The records are in the public domain and will be accessible through the library. We will also transmit a copy of the documentation to the Hawaii State Historic Preservation Division.

Sincerely,

Elaine Jackson-Retondo, Ph.D.
Acting History Program Manager
Pacific West Region

TAKE PRIDE
IN AMERICA 

AP00ENV
CMS-00335



United States Department of the Interior

NATIONAL PARK SERVICE

Pacific West Region
333 Bush Street, Suite 500
San Francisco, CA 94104-2828

HART



'13 FEB 14 P1:05

H40 (PWR-CR)

February 4, 2013

Daniel Grabauskas
Honolulu Authority for Rapid Transportation
1099 Alakea Street, Suite 1700
Honolulu, HI 96813

Re: Final HAER documentation for four Oahu Bridges (HAER Nos. HI-101, HI-115, HI-116, HI-117)

Dear Mr. Grabauskas:

The National Park Service acknowledges the receipt of and accepts the final Historic American Engineering Record (HAER) documentation for the following properties:

HAER No. HI-101, Waiawa Bridge
HAER No. HI-115, Waimalu Bridge
HAER No. HI-116, Kalauao Springs Bridge
HAER No. HI-117, Kalauao Stream Bridge

The completed documentation will be transmitted to the Prints and Photographs Division of the Library of Congress. The records are in the public domain and will be accessible through the library. We also will transmit copies of the documentation to the Hawaii State Historic Preservation Officer.

Sincerely,

A handwritten signature in black ink, appearing to read "Elaine Jackson-Retondo".

Elaine Jackson-Retondo, Ph.D.
Acting History Program Manager
Pacific West Region

TAKE PRIDE[®]
IN AMERICA A graphic of the American flag, showing the stars and stripes, positioned to the right of the text "IN AMERICA".



IN REPLY REFER TO:
CMS-ENV00203

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
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Jiro Sumada

February 20, 2013

The Honorable William J. Aila, Jr.
State Historic Preservation Officer and Chairperson
Department of Land and Natural Resources
State Historic Preservation Division
Kakuhihewa Building, Room 555
601 Kamokila Boulevard
Kapolei, Hawaii 96707

Attention: Dr. Pua Aiu, SHPD Administrator

Dear Mr. Aila:

Subject: Honolulu Rail Transit Project, Section 106 Programmatic Agreement (PA),
Stipulation V. Recordation and Documentation, Section E. Digital Photography

The Honolulu Authority for Rapid Transportation is submitting two (2) copies (on CD) of the digital photographs of select resources and view sheds within the area of potential effect (APE), per the above referenced PA Stipulation (237 labeled images). These photographs were taken by a professional photographer in conjunction with a supervising architectural historian. These photographs focus on National Register of Historic Places (NRHP)-eligible resources and unique landscape features. Three (3) copies (on CD) of these photographs are also being submitted to the Municipal Reference Center (referred to as the City Municipal Library in PA Stipulation V.E.) as required by the PA.

If you have any questions regarding this matter, please do not hesitate to contact Joanna Morsicato, Deputy Planning Director, at 808-768-6181 or jmorsicato@honolulu.gov.

Sincerely,

Daniel A. Grabauskas
Executive Director and CEO

Enclosures

cc: Mr. Ted Matley, Federal Transit Administration, Region IX (w/out enclosures)



IN REPLY REFER TO:
CMS-ENV00204

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
EXECUTIVE DIRECTOR AND CEO

February 20, 2013

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Jiro Sumada

MEMORANDUM

TO: MUNICIPAL REFERENCE CENTER
DEPARTMENT OF CUSTOMER SERVICES

ATTENTION: ROBIN BALLENTYNE

FROM: DANIEL A. GRABAUSKAS, EXECUTIVE DIRECTOR AND CEO

SUBJECT: HONOLULU RAIL TRANSIT PROJECT, SECTION 106 PROGRAMMATIC AGREEMENT
(PA), STIPULATION V. RECORDATION AND DOCUMENTATION, SECTION E. DIGITAL
PHOTOGRAPHY

The Honolulu Authority for Rapid Transportation is submitting three (3) copies (on CD) of the digital photographs of select resources and view sheds (237 labeled images) within the area of potential effect (APE), per the above referenced PA Stipulation (referred to as the City Municipal Library in PA Stipulation V.E). These photographs focus on National Register of Historic Places (NRHP)-eligible resources and unique landscape features.

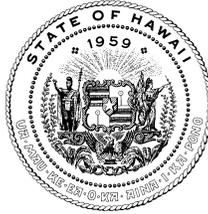
If you have any questions regarding this matter, please do not hesitate to contact Joanna Morsicato, Deputy Planning Director, at Local 86181 or jmorsicato@honolulu.gov.

DANIEL A. GRABAUSKAS
Executive Director and CEO

Enclosures

cc: Mr. Ted Matley, Federal Transit Administration, Region IX (w/out enclosures)
Mr. William J. Aila, Jr., State Historic Preservation Officer (w/out enclosures)

NEIL ABERCROMBIE
GOVERNOR OF HAWAII



WILLIAM J. AILA, JR.
CHAIRPERSON
BOARD OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT

ESTHER KIA'AINA
FIRST DEPUTY

WILLIAM M. TAM
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES
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KAHOOLAWE ISLAND RESERVE COMMISSION
LAND
STATE PARKS

**HISTORIC PRESERVATION DIVISION
DEPARTMENT OF LAND AND NATURAL RESOURCES**

601 Kamokila Boulevard, Suite 555
Kapolei, HI 96806

March 1, 2013

Mr. Matt McDermott, Principal Investigator
Cultural Surveys Hawai'i, Inc.
P.O. Box 1114
Kailua, Hawaii 96734
mmcdermott@culturalsurveys.com

LOG NO: 2013.1957
DOC NO: 1302SL29
Archaeology

Dear Mr. McDermott:

**SUBJECT: Chapter 6E-8 and National Historic Preservation Act (NHPA) Section 106 Review –
Addendum to an Archaeological Inventory Survey Plan for the Airport (Phase 3)
Construction of the Honolulu High-Capacity Transit Corridor Project
Hālawa and Moanalua Ahupua'a, 'Ewa and Honolulu District, O'ahu Island
TMK Sections (1) 1-1 and 9-9**

Thank you for the opportunity to review this revised report titled *Addendum to an Archaeological Inventory Survey Plan for the Airport (Phase 3) Construction of the Honolulu High-Capacity Transit Corridor Project Hālawa and Moanalua Ahupua'a, 'Ewa and Honolulu District, O'ahu Island TMK Sections (1) 1-1 and 9-9* (Hammatt and Shideler, February 2013). Our Kapolei office received this submittal on February 20, 2013.

The study area for this AISP Addendum extends from Kamehameha Highway at Kaloloa Drive (just northwest of Hālawa Stream) in the west to Kamehameha Highway at Middle Street (just west of Kalihi Stream) in the east. The AISP Addendum study area differs from the study area defined in the AISP approved for the Airport (Phase 3) HART Corridor project (December 2, 2011; Log No. 2011.2167, Doc. No. 1211NN01). The AISP Addendum involves re-location of the airport footprint about 60 m south of the original footprint and re-location of the corridor on either side and through the new airport footprint.

The revisions made to this document adequately address most of the concerns raised in our prior correspondence (January 31, 2013; Log No. 2012.3655, Doc. No. 1301SL36). This archaeological inventory survey plan meets the requirements of Hawaii Administrative Rule (HAR) §13-284-5. It is accepted by SHPD. Please send one hardcopy of the document, clearly marked FINAL, along with a copy of this review letter and a text-searchable PDF version on CD to the Kapolei SHPD office, attention SHPD Library.

Please contact Susan A. Lebo at (808) 692-8019 or at Susan.A.Lebo@hawaii.gov if you have any questions regarding this letter.

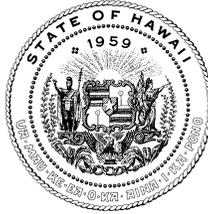
Aloha,

A handwritten signature in black ink, appearing to read "Theresa K. Donham".

Theresa K. Donham
Deputy State Historic Preservation Officer

cc: Paul Cleghorn, Kako'o, cleghorn@pacificlegacy.com

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WILLIAM J. AILA, JR.
CHAIRPERSON
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KAHOOLAWE ISLAND RESERVE COMMISSION
LAND
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**HISTORIC PRESERVATION DIVISION
DEPARTMENT OF LAND AND NATURAL RESOURCES**

601 Kamokila Boulevard, Suite 555
Kapolei, HI 96806

March 1, 2013

Mr. Matt McDermott, Principal Investigator
Cultural Surveys Hawai'i, Inc.
P.O. Box 1114
Kailua, Hawaii 96734
mmcdermott@culturalsurveys.com

LOG NO: 2013.1958
DOC NO: 1302SL28
Archaeology

Dear Mr. McDermott:

**SUBJECT: Chapter 6E-8 and National Historic Preservation Act (NHPA) Section 106 Review –
Addendum to an Archaeological Inventory Survey Plan for the City
Center (Phase 4) of the Honolulu High-Capacity Transit Corridor Project
Kalihi, Kapālama, and Honolulu Ahupua‘a, Honolulu District, Island of O‘ahu
TMK Sections (1) 2-1, 2-3 (various)**

Thank you for the opportunity to review this revised report titled *Addendum to an Archaeological Inventory Survey Plan for the City Center (Phase 4) of the Honolulu High-Capacity Transit Corridor Project Kalihi, Kapālama, and Honolulu Ahupua‘a, Honolulu District, Island of O‘ahu TMK: (1) 2-1, 2-3 (Various Plats and Parcels) Addressing Changes from the Vicinity of Ward Avenue and Halekauwila Street to the Vicinity of Queen and Kamakee Streets* (Hammatt et al., February 2013). Our Kapolei office received this submittal on February 20, 2013.

The entire study area for the Honolulu High-Capacity Transit Corridor Project (HHCTCP) extends about 23 miles from Kapolei in the west to Ala Moana Center in the east. The City Center (Phase 4) study area represents the eastern 4.3 mile terminus of the HHCTCP. The AISP Addendum study area differs from the study area defined in the City Center (Phase 4) AISP approved by SHPD on October 25, 2011 (Log No. 2011.2379, Doc. No. 1110NN08). The AISP Addendum involves re-location of the City Center's Kaka'ako Station footprint and corridor beginning about 30 m 'Ewa (northwest) of Ward Avenue and rejoining the original alignment in the middle of Queen Street about 100 m Diamond Head (southeast) of Kamake'e Street.

The revisions made to this document adequately address the concerns raised in our prior correspondence (January 31, 2013; Log No. 2012.3654, Doc. No. 1301SL34). This archaeological inventory survey plan meets all of the requirements of Hawaii Administrative Rule (HAR) §13-284-5. It is accepted by SHPD. Please send one hardcopy of the document, clearly marked FINAL, along with a copy of this review letter and a text-searchable PDF version on CD to the Kapolei SHPD office, attention SHPD Library.

Please contact Susan A. Lebo at (808) 692-8019 or at Susan.A.Lebo@hawaii.gov if you have any questions regarding this letter.

Aloha,

A handwritten signature in black ink, appearing to read 'Theresa K. Donham'.

Theresa K. Donham
Deputy State Historic Preservation Officer

cc: Paul Cleghorn, Kako‘o, cleghorn@pacificlegacy.com



IN REPLY REFER TO:
CMS-ENVO0213

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
EXECUTIVE DIRECTOR AND CEO

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March 4, 2013

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Dr. Pua Aiu, Administrator
State Historic Preservation Division
Department of Land and Natural Resources
Kakuhihewa Building, Room 555
601 Kamōkila Boulevard
Kapolei, Hawai'i 96707

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Damien T.K. Kim
Glenn M. Okimoto, Ph.D.

Attention: Dr. Susan A. Lebo

Dear Dr. Aiu:

Subject: Request for State Historic Preservation Division (SHPD) Review of the Draft Archaeological Inventory Survey (AIS) Report for Construction Section 3 (Airport) of the Honolulu High-Capacity Transit Corridor Project (HHCTCP), Hālawā and Moanalua Ahupua'a, 'Ewa and Honolulu Districts, O'ahu Island TMK Sections [1] 1-1 and 9-9. (Hammatt, Hallett H., David Shideler, and Matt McDermott 2013)

The enclosed draft AIS report (Hammatt, Hallett H., David Shideler, and Matt McDermott 2013) is submitted for SHPD review on behalf of the Federal Transit Administration (FTA) and the Honolulu Authority for Rapid Transportation (HART). SHPD's review will be under Section 106 of the National Historic Preservation Act, Hawaii Revised Statutes Chapter 6E-8, and Hawaii Administrative Rules Chapter 13-275. The draft AIS report was prepared in accordance with the HHCTCP's Final Programmatic Agreement (PA) dated January 18, 2011. Included with the hard copy of the AIS report are the SHPD review submittal form and the AIS review fee check for \$450.00.

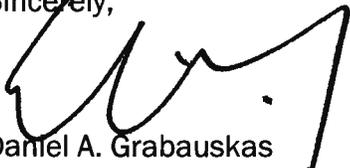
On December 2, 2011, SHPD approved the Archaeological Inventory Survey Plan (AISP) for the Airport (Phase 3) Construction of the HHCTCP (SHPD correspondence LOG NO: 2011.2167, DOC NO: 1211NN01). A revised AISP Addendum for the Airport (Phase 3) AISP was submitted to SHPD for final approval on February 20, 2013. The AIS investigation was carried out in fulfillment of this SHPD-accepted AISP.

Under Federal Section 106 historic preservation review guidelines (36 CFR 800.4), SHPD has 30 days to review and comment on the draft AIS report. If additional time is required for review, please inform HART prior to April 1, 2013. Please contact Ms. Faith Miyamoto [(808) 768-8350] at HART if you have any questions or we can help facilitate your review in any way.

Dr. Pua Aiu, Administrator
Page 2
March 4, 2013

Thank you for your continued help with this project.

Sincerely,

A handwritten signature in black ink, appearing to read 'D. Grabauskas', written over a horizontal line.

Daniel A. Grabauskas
Executive Director and CEO

Enclosures

cc: Dr. Theresa Donham, SHPD
Mr. Ted Matley, FTA
Ms. Barbara Gilliland, PB
Ms. Anna Mallon, HART



Corr sent #003500

IN REPLY REFER TO:
CMS-ENVO0213

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
EXECUTIVE DIRECTOR AND CEO

BOARD OF DIRECTORS

March 4, 2013

Carrie K.S. Okinaga, Esq.
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Dr. Pua Aiu, Administrator
State Historic Preservation Division
Department of Land and Natural Resources
Kakuhihewa Building, Room 555
601 Kamōkila Boulevard
Kapolei, Hawai'i 96707

George I. Atta, FAICP, LEED AP, CEI
Robert Bunda
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William "Buzz" Hong
Donald G. Horner
Kestie W.K. Hui
Damien T.K. Kim
Glenn M. Okimoto, Ph.D.

Attention: Dr. Susan A. Lebo

Dear Dr. Aiu:

Subject: Request for State Historic Preservation Division (SHPD) Review of the Draft Archaeological Inventory Survey (AIS) Report for Construction Section 3 (Airport) of the Honolulu High-Capacity Transit Corridor Project (HHCTCP), Hālawā and Moanalua Ahupua'a, 'Ewa and Honolulu Districts, O'ahu Island TMK Sections [1] 1-1 and 9-9. (Hammatt, Hallett H., David Shideler, and Matt McDermott 2013)

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Dr. Pua Aiu, Administrator
Page 2
March 4, 2013

Thank you for your continued help with this project.

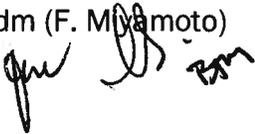
Sincerely,

Daniel A. Grabauskas
Executive Director and CEO

Enclosures

cc: Dr. Theresa Donham, SHPD
Mr. Ted Matley, FTA
Ms. Barbara Gilliland, PB
Ms. Anna Mallon, HART

dm (F. Miyamoto)



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STATE PARKS

**HISTORIC PRESERVATION DIVISION
DEPARTMENT OF LAND AND NATURAL RESOURCES**

601 Kamokila Boulevard, Suite 555
Kapolei, HI 96806

March 29, 2013

Mr. Matt McDermott
Cultural Surveys Hawai'i, Inc.
P.O. Box 1114
Kailua, Hawaii 96734
mmcdermott@culturalsurveys.com

LOG NO: 2013.2279
DOC NO: 1303SL24
Archaeology

Dear Mr. McDermott:

**SUBJECT: Chapter 6E-8 and National Historic Preservation (NHPA) Section 106 Review –
Archaeological Inventory Survey for the Airport Section (Construction Section 3) of the
Honolulu High-Capacity Transit Corridor Project
Hālawā and Moanalua Ahupua'a, 'Ewa and Honolulu Districts, O'ahu Island
TMK: (1) 1-1 and 9-9 (Various Plats and Parcels)**

Thank you for the opportunity to review this draft report titled *Archaeological Inventory Survey for the Airport Section (Construction Section 3) of the Honolulu High-Capacity Transit Corridor Project Hālawā and Moanalua Ahupua'a, 'Ewa and Honolulu Districts, O'ahu Island TMK Sections [1] 1-1 and 9-9 (Various Plats and Parcels)* (Hammatt et al., March 2013). We received this submittal on March 4, 2013.

The Honolulu High-Capacity Transit Corridor Project (HHCTCP) includes the use of federal funds and involves lands under several jurisdictions, including Federal, State, City and County of Honolulu, and private. Pursuant to 36 CFR §800.3(a), the proposed project constitutes an undertaking subject to review under Section 106. To mitigate the undertaking's potential adverse effect on historic properties, a Programmatic Agreement (PA) was excavated on January 18, 2011 with the Federal Transportation Authority (FTA), the Hawaii State Historic Preservation Officer (SHPO), the U.S. Navy, and the Advisory Council on Historic Preservation (ACHP) as signatories, and the City and County of Honolulu (C&C Honolulu) as an invited signatory.

The Airport Section 3 archaeological inventory survey (AIS) comprises the area of potential effect (APE) between Station 994+00 and Station 1248+00, a distance of 7.74 kilometers (4.8 miles) and about 9.06 acres of direct ground disturbance, including three stations. An archaeological inventory survey plan (AISP) was prepared by Cultural Surveys Hawaii, Inc. (Hammatt and Shideler 2011). The AISP was reviewed and approved by SHPD on December 2, 2011 (Log No. 2011.2167, Doc. No. 1211NN01). An Addendum AISP was subsequently prepared to address a possible alternative site (Alternative A) for the Honolulu International Airport Station (Hammatt and Shideler 2013). It was reviewed and approved by SHPD on March 1, 2013 (Log No. 2013.1957, Doc. No. 1302SL29).

The AIS involved a 100 percent surface survey of a heavily developed urban corridor which yielded no surface archaeological cultural resources (historic properties). Subsurface testing involved 47 trench test excavations, of which 40 were proposed in the AISP. The additional 7 were included to address the proposed Alternative A location for the Honolulu Airport Station. The proposed and final locations of some of the trench test excavations differ slightly, reflecting decision changes required to address impediments (e.g., utility line locations) and/or safety issues.

Two cultural resources or historic properties were identified. They consist of Site 50-80-13-7420 (buried asphalt roadway) and Site 50-80-13-7421 (buried concrete slabs, coral pavement, and base course sections related to ca. 1942-1943 military infrastructure). Both sites are assessed as National- and Hawaii- Register eligible under Criterion D (information content). The undertaking effect is identified as "effect, with proposed mitigation commitments." The recommended mitigation measure identified for this undertaking involves an archaeological monitoring program.

Mr. McDermott
March 29, 2013
Page 2

We have identified the issues and concerns directly on the text pages of the AIS report that are in need of revision prior to the acceptance of this report pursuant to the *Secretary of Interior Standards for Archaeological Documentation* and Hawaii Administrative Rule §13-279-4. Please contact me at (808) 692-8019 or at Susan.A.Lebo@hawaii.gov if you have any questions or concerns regarding this letter.

Aloha,



Susan A. Lebo, PhD
O'ahu Lead Archaeologist

cc: Faith Miyamoto, fmiyamoto@honolulu.gov
Barbara Gilliland, Gilliland@pbworld.com



IN REPLY REFER TO:
CMS-ENV00224

HONOLULU AUTHORITY for RAPID TRANSPORTATION

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April 8, 2013

Dr. Pua Aiu, SHPD Administrator
State Historic Preservation Division
Department of Land and Natural Resources
Kakuhihewa Building, Room 555
601 Kamokila Boulevard
Kapolei, Hawaii 96707

Attention: Dr. Susan Lebo

Dear Dr. Aiu:

Subject: Request for State Historic Preservation Division (SHPD) Review of the Draft Archaeological Inventory Survey (AIS) Report for City Center (Section 4) of the Honolulu High-Capacity Transit Corridor Project (HHCTCP), Kalihi, Kapālama, Honolulu, and Waikīkī Ahupuaʻa, Honolulu District, Island of Oʻahu, TMK: [1] 1-2, 1-5, 1-7, 2-1, 2-3 (Various Plats and Parcels)

The enclosed DVD of the Draft AIS report (Hammatt 2013) is submitted for SHPD review on behalf of the Federal Transit Administration (FTA) and the Honolulu Authority for Rapid Transportation (HART). SHPD's review will be under Section 106 of the National Historic Preservation Act, Hawaii Revised Statutes Chapter 6E-8, and Hawaii Administrative Rules Chapter 13-275. The draft AIS report was prepared in accordance with the HHCTCP's Final Programmatic Agreement (PA) dated January 18, 2011. Included with the DVD of the AIS report are the SHPD review submittal form and the AIS review fee check for \$450.00. Please let us know which volumes you would like in hard copy and we will provide them under a separate cover.

On October 25, 2011, SHPD approved the Archaeological Inventory Survey Plan (AISP) for the City Center (Section 4) of the HHCTCP (SHPD correspondence LOG NO: 2011.2379, DOC NO: 1110NN08). An Addendum to this AISP was accepted by SHPD on March 1, 2013 (SHPD CORRESPONDENCE LOG NO: 2013.1958, DOC NO: 1302SL28).

Under Federal Section 106 historic preservation review guidelines (36 CFR 800.4), SHPD has 30 days to review and comment on the draft AIS report. If additional time is required for review, please inform HART prior to May 1, 2013. Please contact Ms. Faith Miyamoto [(808) 768-8350] at HART if you have any questions or if we can help facilitate your review in any way.

Dr. Pua Aiu, Administrator
Page 2
April 8, 2013

Thank you for your continued support and review of this project.

Sincerely,

A handwritten signature in black ink, appearing to read 'D. Grabauskas', written over the word 'Sincerely,'.

Daniel A. Grabauskas
Executive Director and CEO

Enclosures

cc: Dr. Theresa Donham, SHPD
Mr. Ted Matley, FTA
Ms. Barbara Gilliland, PB
Ms. Joanna Morsicato, HART

Corr sent # 00372



IN REPLY REFER TO:
CMS-APO0ENV-00238

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
EXECUTIVE DIRECTOR AND CEO

April 17, 2013

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State Historic Preservation Division
Department of Land and Natural Resources
Kakuhihewa Building
601 Kamokila Boulevard, Suite 555
Kapolei, Hawaii 96707

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Glenn M. Okimoto, Ph.D.

Dear Dr. Aiu:

Subject: National Register of Historic Places (NRHP) Registration Form for Mother Waldron
Playground, Honolulu Rail Transit Project (H RTP)

Please find enclosed a draft NRHP Registration Form for Mother Waldron Playground for your review and comment. Per Stipulation VI.C.2 of the Section 106 of the National Historic Preservation Act Programmatic Agreement for the H RTP, SHPD has 30 days to review and comment on NRHP Registration Forms.

Since Mother Waldron Playground was already listed on the Hawaii Register of Historic Places on June 9, 1988 as an element of the thematic group, "City and County of Honolulu Art Deco Parks," no additional coordination with your office is required regarding Stipulation VI.C, 3.

Please contact Mr. Stanley Solamillo of HART at (808) 768-6187 if you have any questions or if we can help facilitate your review in any way. Thank you for your continued support and review of this project.

Sincerely,

for
Daniel A. Grabauskas
Executive Director and CEO

Enclosure

cc: Ms. Angie Westfall, SHPD
Ms. Faith Miyamoto, HART
Ms. Joanna Morsicato, HART



HART

'13 JUL -3 P2 :22

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DEPARTMENT OF LAND AND NATURAL RESOURCES

HISTORIC PRESERVATION DIVISION
KAKUHIHEWA BUILDING
601 KAMOKILA BLVD STE 555
KAPOLEI HI 96707

DATE: April 23, 2013

LOG: 2013.2853

DOC: 1304RS71

TO: Daniel A. Grabauskas
Executive Director and CEO
Honolulu Authority for Rapid Transportation (HART)
1099 Alakea Street, Unit 1700
Honolulu, HI 96813

SUBJECT: **National Historic Preservation Act Section 106 Review**
Project: Mother Waldron Park National Register Nomination Update
Owner Name: City & County of Honolulu, HCDA, State of Hawaii
Address: Kakaako, Oahu
Tax Map Key: (1) 2-1-051:003,:005, :006 and Coral Street Right of Way between Pohukaina and Halekauwila Streets
Date Received by SHPD: April 23, 2013

SHPD has the following comments regarding the National Register nomination draft update:

Overall:

1. No footnotes to denote where specific information was taken from.
2. Define common local directions (mauka, makai, Ewa, and Diamond Head) at the beginning of the article both as to their immediate meaning (toward the mountains, etc.) and compass directions.
3. Please place appropriate pronunciation guides in parenthesis for Hawaiian language terms (such as O'ahu, Hawai'i, etc.) when first using these words.

Section 1: Name of Property: Please add the Tax Map Key (TMK): (1) (1) 2-1-051:003,:005, :006 and Coral Street Right of Way between Pohukaina and Halekauwila Streets as this is the way that SHPD files records.

Section 5: Classification: Number of Resources within Property: Please identify Contributing and Non-contributing Buildings and Sites at the bottom of this section.

Section 7: Description: Summary Paragraph:

1. Redundant discussion on alterations to park.
2. Are any further details available on physical changes within building?
3. Nothing is included on present condition of facilities (i.e. maintenance).
4. Did the concrete employed in the building employ the methods used at Ala Moana and other parks to use less cement by partially filling the molds with rubble?

Section 8: Statement of Significance:

1. There are no historic photographs included. Photographs should, for example, include the site, Mother Waldron herself, Atkinson Park, and Pohukaina School.

Section 8: Historical Narrative: Hawaii History:

1. Early History through Annexation has little or nothing to do with this park.

Overall: Background materials on the Kakaako neighborhoods are badly lacking. Maps showing the development of the neighborhood should be included. Coral Street, for example, is off grid because it was the original road to the low lying Leper Hospital. Successive Immigration Station buildings were erected along Ala Moana with the Hawaii Sugar Planters' Association's labor assignment office even closer into the center of this district. The potential residential use of this area was compromised early by the large coal piles stacked at the waterfront for both commercial shipping and military warships before the switch to oil-powered marine locomotion and the move by the US Navy to Pearl Harbor. Pohukaina School was originally where the State Library is, but was moved by Governor Frear to the property Ewa of what became Mother Waldron Park in order to take advantage of a Carnegie Grant. Kewalo (Kakaako is the Ewa side of Kewalo) was mostly poorly drained land and was the location of the second large landfill project (after the Honolulu Harbor waterfront). Other municipal and Territorial infrastructure projects included parks makai of Ala Moana and Ewa of Kewalo Basin, the development of two large waste material "crematoria" a ban on open burning of trash along the coast, and construction of the Kakaako Sewage Pump Station, Vocational School, etc. Atkinson and Mother Waldron Parks were where the children of the different ethnic camps (mark these on map) learned to co-exist and cooperate together (the foundation of our multi-cultural society). Nearby were a Japanese Buddhist Hongwanji temple, Portuguese Holy Ghost festivals, Congregationalist Mission, Catholic Church, "mom and pop" stores, etc. Source materials include Marsha Gibson's *Kakaako As We Knew It and Remembering Kaakako 1910-1950* from the University of Hawaii Center for Oral History.

Insert and expand a section on Education. The Territory made a considerable investment in a two story, fire proof building at Pohukaina School. This was done to "Americanize" the youth of this area, but also to raise educational standards and as part of a program of civic improvements. The park and school had an intimate relationship that is not discussed in this document. Add to the biography of Mother Waldron. How was Mother Waldron so successful in turning youth toward creative endeavors?

Insert a section on the Works Progress Administration (WPA) and its relationship to projects such as Mother Waldron Park.

Last paragraph: Even with the changes on the mauka side, the Park would still be recognizable to its designer, Bent. Under these circumstances, it retains integrity of materials, design, and workmanship.

Please resubmit the nomination to SHPD when these changes have been completed. Any questions should be addressed to Ross W. Stephenson, SHPD Historian, at (808) 692-8028 (office) or ross.w.stephenson@hawaii.gov.

Mahalo for the opportunity to comment.



Angie Westfall
Architecture Branch Chief, Hawaii Historic Preservation Division



IN REPLY REFER TO:
CMS-AP00ENV-00240

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
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May 13, 2013

Ms. Christine Avery, Project Historian
National Park Service, Cultural Resources
U.S. Department of the Interior
909 First Avenue, Fifth Floor
Seattle, Washington 98104-1060

Dear Ms. Avery:

Subject: Honouliuli Bridge
Historic American Engineering Record (HAER) No. HI-99
Honolulu Rail Transit Project
Honolulu, Island of O'ahu, Hawai'i

Per the Section 106 Programmatic Agreement, Stipulation V, Recordation and Documentation, Section C, Historic American Building Survey/Historic American Engineering Record/Historic American Landscape Survey (HABS/HAER/HALS) Recordation for the Honolulu High-Capacity Transit Corridor Project, the Honolulu Authority for Rapid Transportation (HART) is transmitting the final document for Honouliuli Bridge (HAER No. HI-99).

This document meets the requirements for HAER documentation and photography of the bridges outlined in Ms. Elaine Jackson-Retondo's letters dated June 29, 2011, and July 2, 2012, respectively. The final report has been printed on 25% cotton, lignin-free archival paper. Two archival copies are being transmitted: one for the Historic American Engineering Record archive at the Library of Congress and one for the Hawaii Department of Land and Natural Resources, State Historic Preservation Division. The cover card is being submitted on archival card stock. A CD is being included as well as one nonarchival (Xerox) copy of the report.

If you have any questions regarding this matter, please do not hesitate to contact Mr. Stanley Solamillo, Architectural Historian, at (808) 768-6187 or ssolamillo@honolulu.gov.

Sincerely,

Daniel A. Grabauskas
Executive Director and CEO

Enclosure: Honouliuli Bridge (HAER No. HI-99)
2 archive copies, 1 nonarchival copy, and one CD



IN REPLY REFER TO:
CMS-AP00ENV-00261

HONOLULU AUTHORITY for RAPID TRANSPORTATION

Daniel A. Grabauskas
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June 5, 2013

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Department of Land and Natural Resources
Kakuhihewa Building, Suite 555
601 Kamokila Boulevard
Kapolei, Hawaii 96707

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Glenn M. Okimoto, Ph.D.

Attention: Ms. Susan A. Lebo, PhD

Subject: Honolulu Authority for Rapid Transportation's (HART) Request for State Historic Preservation Division (SHPD) Review and Approval of the Revised Draft Archaeological Inventory Survey (AIS) Report for Construction Section 3 (Airport) of the Honolulu High-Capacity Transit Corridor Project (HHCTCP) (Hammatt et al. 2013)

Dear Dr. Aiu:

The enclosed revised Draft AIS Report (Hammatt et al. 2013) is submitted for SHPD review on behalf of the Federal Transit Administration (FTA) and HART. SHPD's review will be under Section 106 of the National Historic Preservation Act, Hawaii Revised Statutes Chapter 6E-8, and Hawaii Administrative Rules Chapter 13-275. The revised Draft AIS Report was prepared in accordance with the HHCTCP's Final Programmatic Agreement (PA) dated January 18, 2011.

Thank you for your March 29, 2013 review letter for the earlier Draft Airport AIS Report (SHPD correspondence LOG NO: 2013.2279 DOC NO: 1303SL24). In the letter you described how the requested revisions to the Draft Airport AIS Report were made directly on the hard copy that was reviewed. This hard copy of the requested revisions was provided to HART's archeological consultant and the requested revisions were made. The enclosed revised Draft AIS Report was reviewed by HART's Kako'o (PA compliance officer) and his staff to check that all SHPD requested revisions were made.

Included with this transmittal letter are the following:

1. The two-sided bound revised Draft AIS Report: two volumes—Volume I is the AIS Report and Volume II is the Supporting Appendices—incorporating SHPD's requested revisions.
2. A copy of SHPD's hand edited earlier draft report (Volume I and Appendices A and C from Volume II only) that show SHPD's requested revisions and comments. This copy includes the highlighted marking of the Kako'o's review of how these requested revisions were addressed and incorporated into the revised draft.

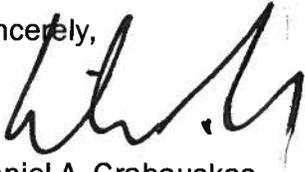
Dr. Pua Aiu, Administrator
Page 2
June 5, 2013

3. Copies of the Kako'o-generated list of SHPD-requested revisions that still needed to be checked by SHPD staff to ensure satisfactory handling after the Kako'o review. These lists were annotated by HART's archaeological consultant documenting how the requested revisions were addressed in the enclosed revised draft report.
4. Copies of a draft March 15, 2013 SHPD review letter (forwarded as an email) that addresses needed changes to the Volume II Introduction and Appendices D, and E, and F, along with a list prepared by HART's archaeological consultant on how these changes to Volume II were made.
5. The standard SHPD review submittal form.

Thank you once again for SHPD's continued diligence in working through the HHCTCP's historic preservation review.

Should you have any questions on the matter, please contact Ms. Faith Miyamoto, Chief Planner, at 768-8350.

Sincerely,



Daniel A. Grabauskas
Executive Director and CEO

Enclosures



HONOLULU AUTHORITY for RAPID TRANSPORTATION

IN REPLY REFER TO:
CMS-AP00ENV-00254

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June 6, 2013

Ms. Pua Aiu, Ph.D., Administrator
State Historic Preservation Division
Department of Land and Natural Resources
Kakuhihewa Building, Room 555
601 Kamokila Boulevard
Kapolei, Hawaii 96707

Dear Dr. Aiu:

Subject: Request for Review of Proposed Bridge Rail Replacement for
Kapālama Canal Bridge Per Programmatic Agreement Stipulation VIII.B.

The Honolulu Authority for Rapid Transportation (HART) is submitting the attached information for your review in compliance with the Programmatic Agreement Stipulation VIII.B which states:

The bridge rails on the Kapālama Canal Bridge must be replaced or retrofitted to meet current safety standards. The City will maintain or replace the rails to match the appearance of the historic rails and to maintain existing views to and from the bridge. The City shall consider *The Secretary of the Interior's Standards for the Treatment of Historic Properties*, 36 C.F.R. pt. 68, in developing draft plans to provide to SHPD for review per Stipulation IV.

The existing bridge structure on Dillingham Boulevard over the Kapālama Canal was constructed in 1930 as a five-span reinforced concrete T-girder bridge. The bridge is approximately 113 feet long, 79.5 feet wide, and currently carries six (6) lanes of bi-directional traffic. In 2005, the existing structure was seismically retrofitted.

The bridge was determined to be eligible for the National Register of Historic Places under Criterion A for its association with the transportation history of the area and the extension of the Dillingham Boulevard as well as under Criterion C as an example of concrete bridge engineering and design in Hawai'i. Since the Honolulu Rail Transit Project proposed widening and installation of a future raised median are considered substantial bridge modifications, improvements must comply with current design and safety standards. Therefore the existing bridge rails on both sides of the bridge are required to be improved. See attached concrete railing drawing.

Rebuilding balustrades (rails) includes the following: duplicating the original design (identified locally as "paddleboard," similar to Texas Classic Railing Type C411 [modified opening to 4"] for base, wall, and cap; reducing width of the openings to meet code requirements without the need for an insert down the middle of each opening; reproducing the solid-to-void appearance of the original span

Dr. Pua Aiu, Administrator

Page 2

June 6, 2013

although the number of openings will increase; rebuilding corner piers (columns) and duplicating the existing design including base, raised panel, and cap; duplicating the river rock perimeter wall design and construction on the *makai* side of the bridge by building with river rock matching existing in size, shape, and color; and tinting concrete mortar to match existing mortar color. (Note the perimeter wall design on the *mauka* side does not match the *makai* side. Only the *makai* side will be disturbed by the bridge widening activities.)

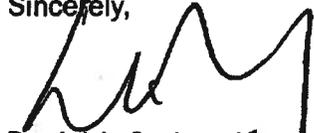
Texas Classic Railing Type C411 (modified opening to 4") has been selected to match the appearance of the historic rails and maintain the existing views to and from the bridge. The attached KAPĀLAMA CANAL BRIDGE WIDENING TYPE SELECTION MEMORANDUM for WIDENING DILLINGHAM BLVD BRIDGE OVER KAPĀLAMA CANAL (Bridge No. 147) (May 2013) provides detailed information on the railing selection.

HART is in the process of mitigating the adverse effect to the bridge through Historic American Engineering Record (HAER) Level II documentation (including 4x5 black and white photographs of side elevations and obliques; 8x10 negatives from 4x5 photographs of original plan and section drawings [1930] and narrative report). This is in compliance with PA Stipulation V.C. These requirements are clearly identified in the National Park Service (NPS) letter of June 29, 2011. Completion of this documentation and acceptance by NPS will be accomplished prior to commencement of construction activities outlined above that will impact this historic property and affect its NRHP integrity under Criterion C (PA Stipulation V.C.3).

Please indicate your concurrence that the action proposed is consistent with PA Stipulation VIII.B. This will enable continuation of design activities for this location.

If you have any questions regarding this matter, please do not hesitate to contact Mr. Stanley Solamillo, HART Architectural Historian, at (808)768-6187 or ssolamillo@honolulu.gov.

Sincerely,



Daniel A. Grabauskas
Executive Director and CEO

Enclosures

KAPĀLAMA CANAL BRIDGE WIDENING

TYPE SELECTION MEMORANDUM
for
**WIDENING DILLINGHAM BLVD BRIDGE
OVER KAPĀLAMA CANAL**
(Bridge No. 147)

FOR THE

**DESIGN OF THE HONOLULU RAIL TRANSIT
CITY CENTER SECTION
UTILITIES AND GUIDEWAY CONTRACT**

Revision:

**A
B**

Date:

**March - 2013
May - 2013**

Description:

**Draft Submittal
Final Submittal**

AECOM

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 PARTIAL BRIDGE DEMOLITION
 TEXAS CLASSIC RAILING TYPE C411 (MOD)

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C. PRELIMINARY FOUNDATION REPORT C1

1-1 EXISTING STRUCTURE TYPE

The existing bridge structure on Dillingham Boulevard over the Kapālama Canal was constructed in 1930 as a five-span reinforced concrete T-girder bridge. The bridge is approximately 113 ft long, 79.5 ft wide and currently carries 6 lanes of bi-directional traffic. Three foot (3'-0") high concrete railings and 10 ft wide concrete sidewalks flank both sides of the bridge. The deck structure consists of a 5" thick reinforced concrete deck under the sidewalk and a 9" thick reinforced concrete deck under the roadway travel lanes with a 1½" asphalt overlay.

The superstructure consists of fourteen (14) concrete T-beams with concrete diaphragms within five spans, each approximately 21'-9" in length.

The substructure is comprised of concrete abutments and pile bents. The abutments are supported on a continuous concrete pile cap with 10 concrete piles. A 5'-0" deep reinforced concrete backwall exists below the abutment cap. Each of the four pile bents is founded on 14 concrete piles. All piles are 14" square precast concrete piles with a typical length of 30 feet.

Several utilities are carried on the existing bridge or encased in the canal below. Located in the first interior cell just inside the mauka side of the bridge are two waterlines (12" and 18" in diameter). Near the center of the bridge and located in the canal bottom is an encased sewer line (36" diameter). Near the downstream side of the bridge and located in a cell are cable TV and telephone conduits. Located in the canal bottom just outside the makai edge of the bridge is an encased 42" diameter waterline. Existing overhead electrical lines/poles on the makai side of Dillingham must also be relocated to accommodate the widened bridge.

In 2005 the existing structure was seismically retrofitted including the addition of concrete shear keys and carbon composite laminate on each side of the existing pier caps.

1-2 HISTORIC EFFECTS REPORT FINDINGS:

The bridge was determined to be eligible for the NRHP under Criterion A for its association with the transportation history of the area and the extension of the Dillingham Blvd and under Criterion C as an example of concrete bridge engineering and design in Hawai'i. Since the proposed HRTTP widening and installation of a future raised median are considered substantial bridge modifications, improvements must use current design and safety standards. Therefore the existing bridge rails on both sides of the bridge are required to be improved.

2-1 WIDENING TYPE SELECTION RECOMMENDATIONS

I. Introduction

The Kapālama canal Bridge (Widen) is located on Dillingham Blvd in the City and County of Honolulu and serves as a primary link between Kalihi and downtown Honolulu.

The proposed future Honolulu Rail Transit Guideway alignment and profile above Dillingham Blvd will require placement of support columns within a raised median occupying one of the existing lanes of traffic on the Kapālama Canal Bridge. The City and County of Honolulu proposes to widen the Dillingham Blvd corridor including this structure approximately 12 feet on the makai side (downstream side) to accommodate the aerial guideway improvements.



Figure 1: Kapālama Canal Bridge

II. Design Criteria

The design criterion for this structure includes:

- AASHTO LRFD 2010 Bridge Design Specifications
- HDOT Design Criteria for Bridges and Structures, October 2010.

III. Design Alternatives

Typically it is desirable to widen the bridge by matching the existing structure type and configuration. Due to the need to match the 2% cross slope of the roadway, match the minimum vertical clearance, and minimize the construction time and staging impacts associated with the widening, the use of precast girders is preferable over cast-in-place reinforced concrete Tee girders.

While several precast girder alternatives were considered and compared (i.e. precast prestressed concrete slab, precast prestressed double tee girder) the preferred economical alternative for the widening is a **precast prestressed double-tee girder** for the following reasons:

Applicability

Based on depth from the existing structure soffit to the top of existing pier cap, a structure depth of 1'-6" precast prestressed double-tee girder with an 8" (max and varies) reinforced concrete deck slab will meet the vertical clearance requirement.

Pros and Cons

Construction of a precast concrete bridge is typically faster because girders may be fabricated off-site while the substructure is under construction. The precast concrete girder option therefore reduces traffic impacts and enhances safety during construction. The structure depth of the precast double-tee girder is similar to that of the existing CIP reinforced concrete girder, so there will be less visual impact, i.e. the finished widening will have a similar look to the existing historical structure.

Texas Classic Barrier

To preserve the historic look of the existing concrete bridge railing with its 6" wide x 14" height openings, meet the current bridge railing height requirement of 42 inches, and meet the minimum FHWA-TL-2 crash test rating requirements, the Texas Classic Barrier Type C411 was selected as a replacement barrier. This barrier meets FHWA crash test requirements for a low speed roadway (less than 45 mph) and includes barrier openings of similar size and shape to those of the existing structure. It also provides the height increase necessary to meet current safety requirements. The width of the barrier opening was reduced to 4" to meet current HDOT requirements.

IV. Maintenance Issues

No major maintenance is required for precast prestressed concrete structures. To minimize seismic design issues and future conflicts when replacement of the existing bridge is deemed necessary, AECOM proposes to support the entire widening on new stand-alone pile caps separated and isolated from the existing structure with a longitudinal deck joint. No special maintenance is required for the longitudinal joint.

V. Construction Issues

Construction at the deck level will be isolated from the Dillingham Blvd road traffic by temporary K-railing located at the edge of the travel lane. Utilizing staged construction, the existing barriers and deck overhangs will be removed and the new deck and barrier rails constructed while maintaining existing traffic lanes. The new double tee girders will be placed from the existing bridge during several night shifts. The contractor may opt to use the trestle (see discussion of drilled shaft construction below) for girder placement.

During the bridge widening (Construction Stage 1), the makai sidewalk will be closed to pedestrian traffic while the mauka sidewalk remains open. During the mauka barrier replacement (Construction Stage 2), only the mauka sidewalk will be closed to pedestrian traffic. It is anticipated that shoring (sheet piling) will be required to extend the abutments and walls during Stage 1 widening. Based on consultation with the City and SHPD, the wall extension below the bridge is required to match the existing rock wall. A form liner with colored concrete was originally considered, however SHPD is requiring re-use of the existing stone.

To facilitate construction of the new foundations in the water, 24" diameter cased drilled shaft foundations are proposed for the widening structure. The existing piles will be

extracted. Should removal of an existing pile prove impossible, it will be cut below mudline and left in place. The proposed new pile spacing avoids the existing pile locations. Consideration was given to many different foundation types. Drilled shafts were selected based on the age and condition of the existing structure to reduce the potential for settlement of the existing foundations during installation of displacement piles. Our geotechnical consultant, Geolabs, provided conceptual foundation recommendations for this report. The final foundation recommendations will be provided as part of the future submittal of the City Center Geotechnical Report. A work trestle will be used to construct the drilled shafts across the canal.

3-1 SEISMIC DESIGN AND REHABILITATION STRATEGY

In 2005 the existing Kapālama Canal Bridge was reviewed and retrofitted by others. To avoid any new seismic issues between the proposed widening and the existing Kapālama Canal Bridge, AECOM proposes to support the new superstructure widening on an all new substructure. A simple open longitudinal joint will be placed in the deck between the widening and existing structure, and the new pier caps will stand alone and be separated by a gap instead of being connected to the existing piers.

Based on the Bridge Inspection Report dated April 2012, the bridge deck, superstructure, and substructure are rated as being in fair condition. The bridge's primary structural elements are sound but exhibit minor section loss such as cracking and/or spalling concrete.

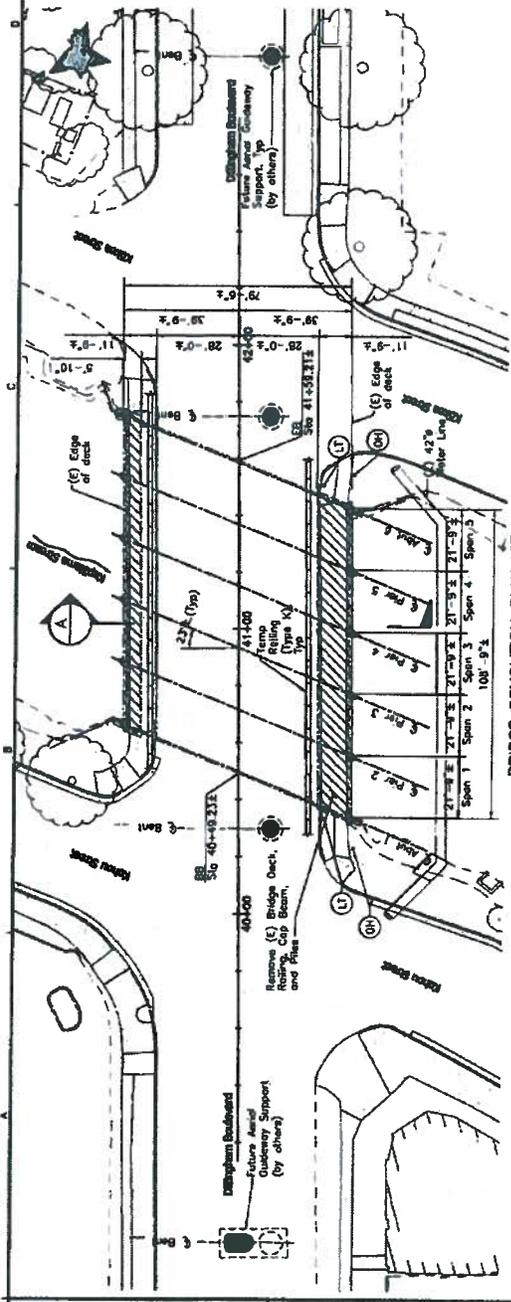
In summary, AECOM proposes no new seismic retrofit or rehabilitation of the existing structure beyond the required replacement of the existing barriers as previously noted.

APPENDIX A

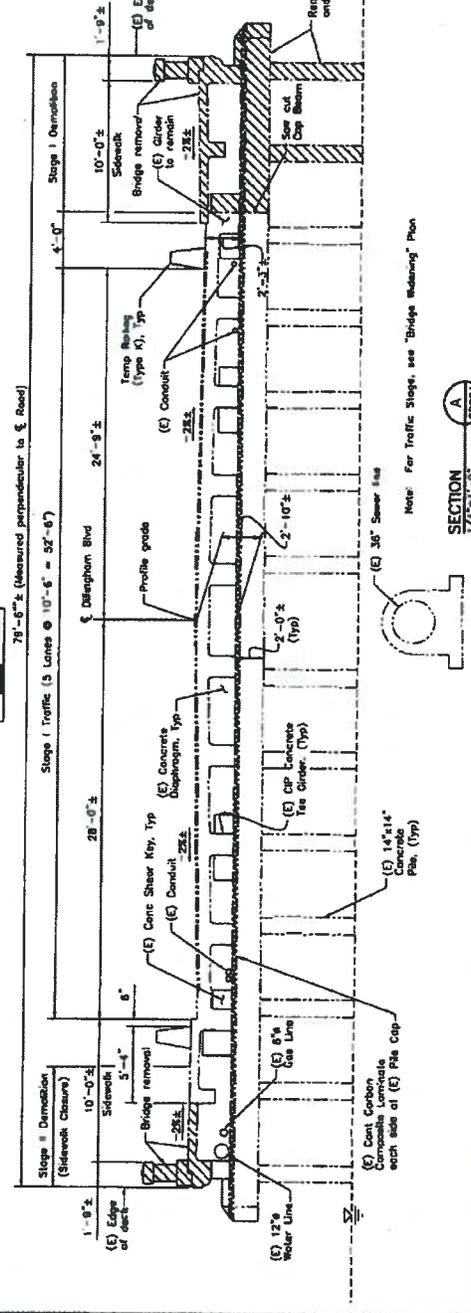
GENERAL PLANS PARTIAL BRIDGE DEMOLITION TEXAS CLASSIC RAILING TYPE C411

- NOTES:**
1. The contractor shall be responsible for planning and executing all procedures necessary to partially demolish and remove the bridge in a safe and controlled manner while preventing damage to the existing bridge to remain.
 2. The contractor shall submit a complete bridge removal plan to the City & County of Honolulu that includes detail, procedures and sequence of the bridge demolition.
 3. The remaining structure shall be stable during all stages of the removal operations.
 4. Demolition operations shall be planned such that the safety of the remaining structure is maintained. The utility lines shall be protected if affected by demolition operations. All work associated with utility lines shall be coordinated by the contractor with the respective utility companies.
 5. The contractor shall contain, collect and dispose of construction debris, including concrete dust and debris, in compliance with applicable regulations.

- LEGEND:**
- ▨ Bridge Removal (partial)
 - ⊖ OH Electrical Line Pole to be relocated
 - ⊖ Street Light Pole to be relocated
 - ⊖ Existing

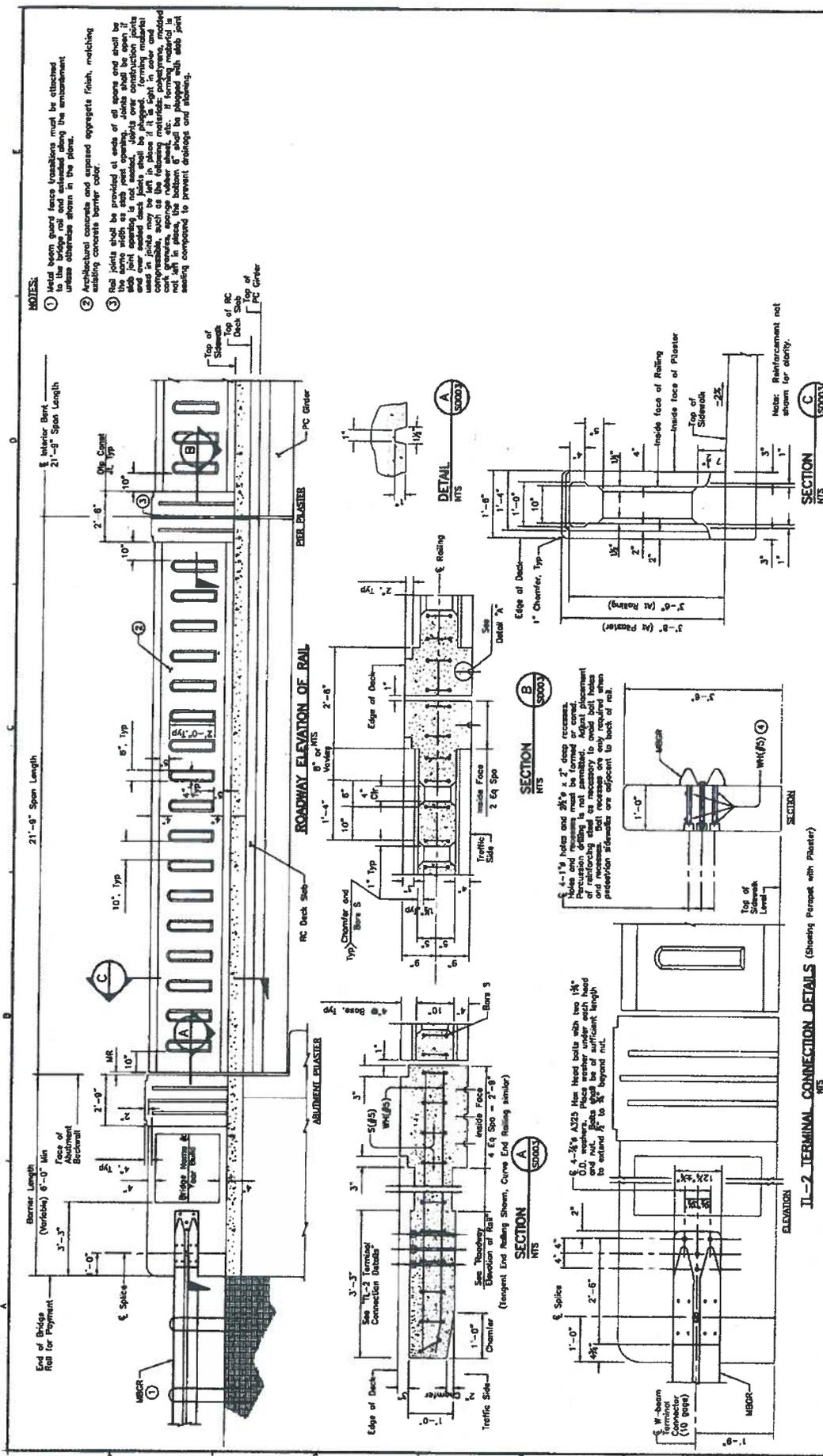


BRIDGE DEMOLITION PLAN VIEW



SECTION 1/8"=1'-0" A (300')

PRELIMINARY ENGINEERING UPDATE		HONOLULU RAIL TRANSIT PROJECT HONOLULU AUTHORITY FOR RAIL TRANSPORTATION		CITY CENTER GUIDEWAY AND UTILITIES KAPALAMA CANAL BRIDGE PARTIAL BRIDGE DEMOLITION	
Project No. SC-1411-1-200149	Drawn By D. Nguyen	Checked By C. Tu	Project Consultant AECOM	Sheet No. 50001	Scale As Noted
Drawn By W. Jordan	Checked By W. Jordan	Approved By W. Jordan	1001 Alie Street, Suite 1600 - Honolulu, HI 96813	Project No. 50001	Sheet No. A
Date 03-18-13	Scale 0.5"=1'-13"	For Technical Details, Engineer Stamp, Refer to Volume 3			



- NOTES:**
- ① Metal beam guard fence (详示) must be attached to the reinforcement using the attachment unless otherwise shown in the plans.
 - ② Architectural concrete and exposed aggregate finish, matching existing concrete border curb.
 - ③ Rail joints shall be provided at ends of all spans and shall be staggered. The joint spacing shall be staggered. All joints shall be provided with construction joints and new sealed deck joints shall be provided. Forming materials shall be used for the joints. The joints shall be made compressible, such as the following materials: cork granules, sponge rubber sheets, etc. If forming material is used in place, the bottom of it shall be plugged with steel joint casting compound to prevent drainage and staining.

<p>PRELIMINARY ENGINEERING UPDATE</p>		<p>HONOLULU RAIL TRANSIT PROJECT HONOLULU AUTHORITY FOR RAIL TRANSPORTATION</p>		<p>CITY CENTER GUIDEWAY AND UTILITIES KAPALAMA CANAL BRIDGE TEXAS CLASSIC RAILING TYPE C411(MOD)</p>	
<p>Project No. 05-24-13 Revision No. 03-19-13 Date 03-19-13</p>	<p>Prepared By W. Jordan Checked By W. Jordan Approved By W. Jordan Date 05-24-13</p>	<p>Project No. 05-24-13 Revision No. 03-19-13 Date 03-19-13</p>	<p>Project No. 05-24-13 Revision No. 03-19-13 Date 03-19-13</p>	<p>Project No. 05-24-13 Revision No. 03-19-13 Date 03-19-13</p>	<p>Project No. 05-24-13 Revision No. 03-19-13 Date 03-19-13</p>
<p>Scale: As Shown</p>		<p>Scale: As Shown</p>		<p>Scale: As Shown</p>	

APPENDIX B

GENERAL PLAN QUANTITIES

City and County of Honolulu

BRIDGE GENERAL PLAN QUANTITIES

STRUCTURE:	KAPALAMA CANAL BRIDGE (WIDEN)				
TYPE:	PC P/S Double-Tee Girder on CISS Concrete Piling				
LENGTH	113	x WIDTH	36.5	= AREA	4,125 SQ FT

	<u>CONTRACT ITEMS</u>	<u>UNIT</u>	<u>QUANTITY</u>
1	STRUCTURE EXCAVATION (BRIDGE)	CY	54
2	STRUCTURE BACKFILL (BRIDGE)	CY	14
3	DRIVE 24" DIA CAST-IN-STEEL-SHELL CONCRETE PILING	EA	24
4	FURNISH 24" DIA CAST-IN-STEEL-SHELL CONCRETE PILING	LF	1,488
5	STRUCTURAL CONCRETE, (BRIDGE)	CY	210
6	STRUCTURAL CONCRETE, (BRIDGE FOOTING)	CY	27
7	BAR REINFORCING STEEL (BRIDGE)	LBS	53,100
8	CONCRETE BARRIER TYPE TEXAS CLASSIC C411 (MOD)	LF	301
9	FURNISH PRECAST/ PRESTRESSED CONC DOUBLE-TEE GIRDER	EA	30
10	ERECT PRECAST/ PRESTRESSED CONC DOUBLE-TEE GIRDER	EA	30
11	JOINT SEAL (MR=1/2")	LF	219
12	MISCELLANEOUS METAL (BRIDGE)	LBS	539
13	TEMPORARY RAILING (TYPE K)	LF	240
14	RETAINING WALLS/ WINGWALLS	SF	450
15	ROCK AESTHETICS TREATMENT	SF	300
16	BRIDGE REMOVAL (cont. incl.)	LS	1

APPENDIX C

PRELIMINARY FOUNDATION REPORT



GEOLABS, INC.

Geotechnical Engineering and Drilling Services

TECHNICAL MEMORANDUM

DATE:	February 26, 2013	TIME:	9:49 AM
TO:	AECOM Technical Services, Inc.	FROM:	Robin M. Lim
ATTN:	Mr. Wally Jordan	W.O. No.:	6730-30
SUBJECT:	H RTP – City Center Section Kapalama Stream Bridge Widening Preliminary Drilled Shaft Recommendations	NO. OF PAGES:	2 (Text)
E-MAIL:	Wally.Jordan@aecom.com		
COPY TO:	File		

This technical memorandum presents our preliminary drilled shaft recommendations for the foundation design of the Kapalama Stream Bridge Widening project for the Honolulu Rail Transit Project (H RTP) – City Center Section Utilities and Guideway Contract. As you are aware, we have not been able to commence the geotechnical exploration program for the project. Therefore, the preliminary geotechnical recommendations for foundation design presented herein are based on the widely spaced available subsurface information along the alignment and our experience in the vicinity only.

The preliminary geotechnical recommendations for the bridge widening are subject to change based on the upcoming geotechnical exploration efforts. A detailed summary of our findings and recommendations will be contained in the geotechnical engineering report. The final report should be consulted when it becomes available.

Subsurface Conditions

Based on available information and our previous experience in the area, the Kapalama Stream Bridge site (at the abutment locations) is underlain by thin surface fills over recent alluvium (including lagoonal deposits) consisting of medium stiff to soft clayey silts and silty clays extending to depths of about 20 feet below the existing bridge deck. The recent alluvium is underlain by medium dense coralline detritus materials and older alluvial deposits, consisting of very stiff to hard clayey silts.

Drilled Shaft Foundations

Based on the available subsurface information and foundation loads provided, we recommend using deep foundations consisting of drilled shafts for foundation support of the proposed Kapalama Stream Bridge Widening. The drilled shafts foundations would derive support principally from adhesion between the drilled shaft and the medium dense coralline detritus and the very stiff to hard clayey silts. Based on the structural loads imposed on the foundations, we recommend using a minimum drilled shaft diameter of 18 inches with an embedment depth of 62 feet below the planned bottom of bent cap at about Elevation +4 feet MSL. The drilled shaft length is designed based on a strength limit state compression load capacity of up to 150 kips per drilled

shaft. For the extreme event limit state, a compression load capacity of 230 kips may be used in the drilled shaft design.

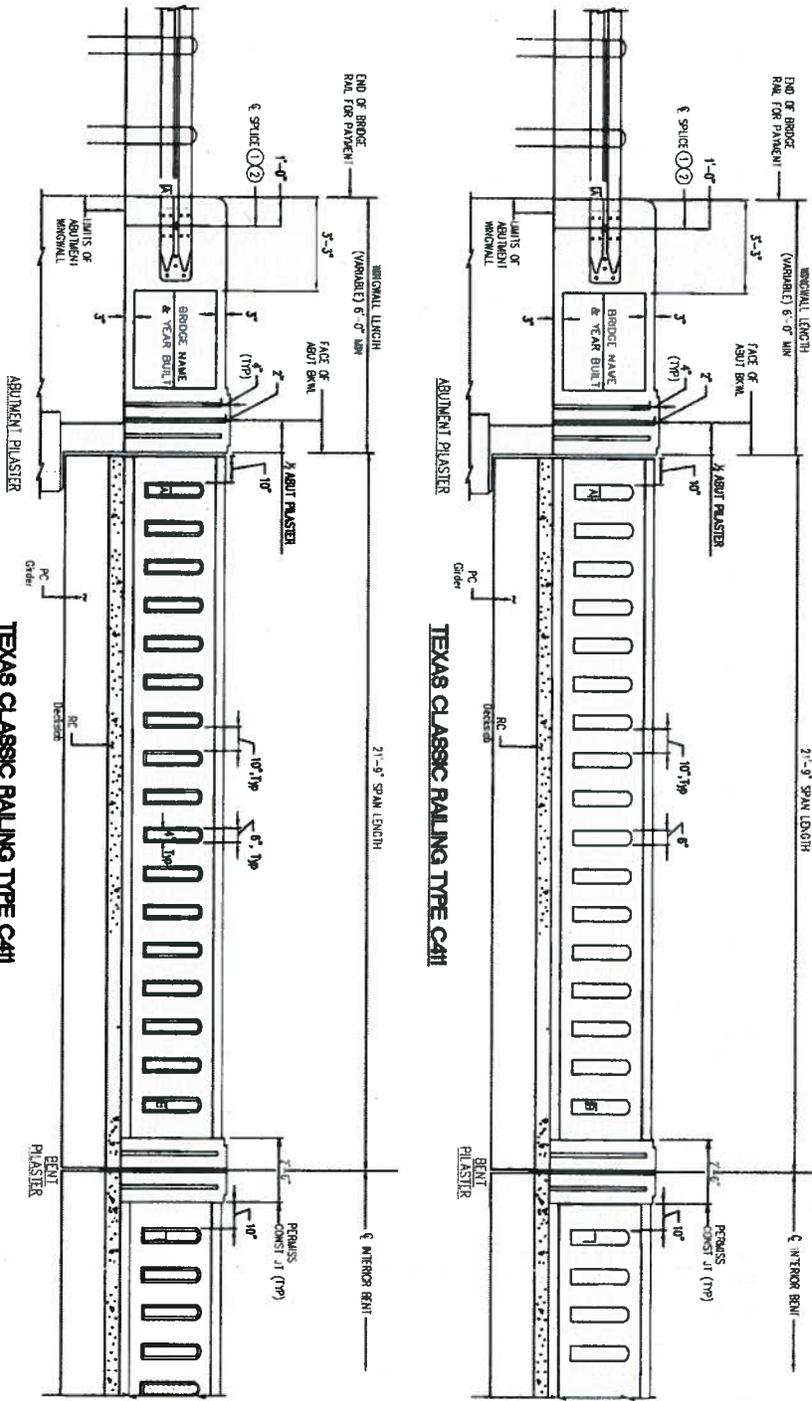
To facilitate construction of the drilled shafts, we recommend using permanent steel casing for the drilled shafts extending through the stream water to serve as pier columns. The permanent steel casing for the drilled shafts should have a minimum outside diameter (OD) of about 24 inches. The permanent casing (unbonded zone) should extend through the soft soil deposit (20 feet) and be embedded to a minimum casing tip elevation of about -20 feet MSL. The permanent steel casing should have a minimum wall thickness of 0.75 inches and shall be reduced for design purposes by a presumptive value of 0.25 inches for corrosion.

Our recommendations pertaining to the drilled shafts compression load capacity and length are presented in the following table.

COMPRESSION LOAD CAPACITY OF DRILLED SHAFT FOUNDATION					
Strength Limit State Compression Load Capacity (kips)	Drilled Shaft Diameter (inches)	Permanent Steel Casing Outside Diameter (inches)	Cased Length (feet)	Preliminary Drilled Shaft Length (feet)	Preliminary Drilled Shaft Tip Elevation (feet MSL)
150	18	24	24	60	-58

Closure

If you have questions regarding the contents of this technical memorandum or need additional information, please contact our office.



TEXAS CLASSIC RAILING TYPE C4H1
 (Modified Opening to 4')

TEXAS CLASSIC RAILING TYPE C4H1

NEIL ABERCROMBIE
GOVERNOR OF HAWAII



HART

'13 JUL -3 P2:22

WILLIAM J. AILA, JR.
CHAIRPERSON
BOARD OF LAND AND NATURAL RESOURCES
COMMISSION ON WATER RESOURCE MANAGEMENT

ESTHER KIA'AINA
FIRST DEPUTY

WILLIAM M. TAM
DEPUTY DIRECTOR - WATER

AQUATIC RESOURCES
BOATING AND OCEAN RECREATION
BUREAU OF CONVEYANCES
COMMISSION ON WATER RESOURCE MANAGEMENT
CONSERVATION AND COASTAL LANDS
CONSERVATION AND RESOURCES ENFORCEMENT
ENGINEERING

FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
KAHOOLAWE ISLAND RESERVE COMMISSION
LAND
STATE PARKS

STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES

HISTORIC PRESERVATION DIVISION
KAKUHIHEWA BUILDING
601 KAMOKILA BLVD STE 555
KAPOLEI HI 96707

DATE: June 18, 2013

LOG: 2013.3891

DOC: 1306RS49

TO: Daniel A. Grabauskas
Executive Director and CEO
Honolulu Authority for Rapid Transportation (HART)
1099 Alakea Street, Unit 1700
Honolulu, HI 96813

SUBJECT: Section 6E-08 Historic Preservation Review
Project: Historic American Engineering Record (HAER) Report
Owner: Highways Division, Department of Transportation, State of Hawaii
Location: Farrington Highway over Honouliuli Stream
Tax Map Key: ROW adjacent to (1) 9-1-017:016

Date Received by SHPD: May 15, 2013

Description of Project/Undertaking: Documentation of bridge for Honolulu Authority for Rapid Transportation (HART)

Area of Potential Effect (APE): Bridge footprint and approaches

Description of Resource: Single span, concrete tee-beam, reinforced concrete bridge erected in 1939. Constructed by prolific local engineer William Bartels.

Eligibility: Eligible under Criteria A (Events – post 1925 Federal Aid Project, when Hawaii given federal funds for highway construction; development and improvement of around the island road system) and C (Architecture -period bridge design).

Documentation Received: HAER HI-99. Honouliuli Bridge (Honouliuli Stream Bridge)

SHPD Determination: The report should further expand on how technology, entrepreneurship, and capital investment changed the use and population carrying capacity of the land, which in turn required expansion of the road system between both the major employers (Ewa, Waianae, and Oahu Sugar) and the plantations and Honolulu. This would include the sites of ranch operations, specific decisions to where to locate mills, consolidation of operations within each plantation and lessening dependence upon both intra-plantation railroads and the OR&L. All of these actions affected both the placement and development of transportation infrastructure.

Any questions should be addressed to Ross W. Stephenson, SHPD Historian, at (808) 692-8028 (office) or ross.w.stephenson@hawaii.gov.

Mahalo for the opportunity to comment.

A handwritten signature in black ink, appearing to read "Angie Westfall".

Angie Westfall
Architecture Branch Chief, Hawaii Historic Preservation Division

NEIL ABERCROMBIE
GOVERNOR OF HAWAII



STATE OF HAWAII
DEPARTMENT OF LAND AND NATURAL RESOURCES

HISTORIC PRESERVATION DIVISION
KAKUHIHEWA BUILDING
601 KAMOKILA BLVD STE 555
KAPOLEI HI 96707

WILLIAM J. AILA, JR.
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COMMISSION ON WATER RESOURCE MANAGEMENT

ESTHER KIA'AINA
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AQUATIC RESOURCES
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CONSERVATION AND COASTAL LANDS
CONSERVATION AND RESOURCES ENFORCEMENT
ENGINEERING
FORESTRY AND WILDLIFE
HISTORIC PRESERVATION
KAHOOLAWE ISLAND RESERVE COMMISSION
LAND
STATE PARKS

DATE: June 20, 2013

LOG: 2013.3815

DOC: 1303RS47

TO: Daniel A. Grabaukas
Executive Director and CEO
Honolulu Authority for Rapid Transportation (HART)
1099 Alakea Street, Unit 1700
Honolulu, HI 96813

SUBJECT: **National Historic Preservation Act (NHPA) Section 106 Review**
Project: Bridge railing replacement as part of reconstruction and extension of vehicular bridge
Owner: City and County of Honolulu
Location: Dillingham Boulevard Bridge over Kapalama Canal
Tax Map Key: (1) 1-5-020:011 Right of Way over Bridge

Date Received by SHPD: June 12, 2013

Description of Project/Undertaking: Rebuilding balustrades including duplicating the original design; identified locally as "paddleboard", similar to Texas Classic Railing Type C411 (modified by opening to four inches) for base, wall, and cap; reducing the width of the openings to meet code requirements without the need for an insert down the middle of each opening; reproducing the solid-to-void appearance of the original span (although the number of openings will increase); rebuilding the corner piers (columns) and the existing design including base, raised panel, and cap; duplicating the river rock perimeter wall design and construction on the makai side of the bridge with river rock matching existing in size, shape, and color (SHPD would prefer reusing existing rock rather than using a mold); and tinting of concrete mortar to match existing mortar color.

Area of Potential Effect (APE): Existing bridge footprint, approaches, areas immediately mauka and makai

Description of Resource: Five span 1930s concrete T-girder bridge, approximately 113 feet long, 79.5 feet wide, carrying six lanes of traffic.

Eligibility: Eligible under Criterion A (Events – transportation history) and C (Architecture – bridge design).

Documentation Received: *Kapalama Canal Bridge Widening Type Selection Memorandum for Widening Dillingham Blvd Bridge Over Kapalama Canal (Bridge No. 147)*, AECOM, 2013.

SHPD Determination: HART is in the process of mitigating the adverse effect to the Dillingham Boulevard Bridge through Historic American Building Record (HAER) Level II documentation as part of the Programmatic Agreement (PA) between the U. S. Department of Transportation Federal Transit Administration, the Hawaii State Historic Preservation Officer, the United States Navy and the Advisory Council on Historic Preservation dated January 2011.

SHPD concurs with the actions above (reconstruction and documentation) are consistent with the Programmatic Agreement Sections V.C.3 and VIII.B of the PA.

Any questions should be addressed to Ross W. Stephenson, SHPD Historian, at (808) 692-8028 (office) or ross.w.stephenson@hawaii.gov.

Mahalo for the opportunity to comment.

A handwritten signature in black ink, appearing to read "Angie Westfall".

Angie Westfall, Architecture Branch Chief, Hawaii Historic Preservation Division

Date: 07/12/2013

CSH Job Code: HALAWA 13

Submittal Sheet for Historic Preservation Review Filing Fees

State Historic Preservation Division
Department of Land and Natural Resources

Agency/Firm (Requesting Review): Cultural Surveys Hawai'i Inc.

Contact: Matt McDermott

Phone: (808) 262-9972 Fax: (808) 262-4950

Address: P.O. Box 1114 Kailua, HI 96734

Email: mmcdermott@culturalsurveys.com

Title of Report/Plan:

Archaeological Inventory Survey for the Airport Section (Construction Section 3) of the Honolulu High-Capacity Transit Corridor Project, Hālawā and Moanalua Ahupua'a, Ewa and Honolulu Districts, Island of O'ahu, TMK (1) 1-1 and 9-9 (Various Plats and Parcels)		
Island O'ahu	District Ewa and Honolulu	Ahupua'a Hālawā and Moanalua
TMK (1) 1-1 and 9-9 (Various Plats and Parcels)		
Acres Inventoried		Number of new sites inventoried
9.06 ac		2

Submittal Plan/Report Fee & Type: (All reports or plans submitted to the SHPD for review shall be accompanied by the appropriate fee in accordance with HAR§13-275-4 and §284-4).

- Check if Report is a Re-Submittal (no fee charged)
- Final Report with CD
- \$25 Literature Review & Field Check
- \$50 Archaeological Assessment
- \$150 Archaeological Inventory Survey Plan
- \$450 Archaeological, Architectural or Ethnographic Survey Report
- \$150 Preservation Plan
- \$25 Monitoring Plan
- \$150 Archaeological Data Recovery Plan
- \$250 Burial Treatment Plan
- \$100 Archaeological Monitoring Report, if resources reported
- \$450 Archaeological Data Recovery Report
- \$450 Ethnographic Documentation Report
- \$25 Burial Disinterment Report
- \$50 Osteological Analysis Report

RECEIVED
 HISTORIC PRES. DIV.
 DEPT. OF LAND &
 NATURAL RESOURCES
 2013 JUL 12 PM 4:08
 Will be logged in
 July 15, 2013

Fee Total: 0.00 (make checks payable to "Hawaii Historic Preservation Special Fund")
For Office Use Only:

Date Received	Payment Method
	Cash \$
	Check Check No:
Log No.	Receipt Issued:

Date: 7/12/13

CSH Job Code: KALIHI 23

Submittal Sheet for Historic Preservation Review Filing Fees

State Historic Preservation Division

Department of Land and Natural Resources

Agency/Firm (Requesting Review): Cultural Surveys Hawai'i Inc.

Contact: David Shideler

Phone: 808 262-9972 Fax: 808 262-4950

Address: P.O. Box 1114 Kailua, HI 96734

Email: dshideler@culturalsurveys.com

Title of Report/Plan:

REVISED Archaeological Inventory Survey Report For City Center (Section 4) of the Honolulu High-Capacity Transit Corridor Project, Kahihi, Kapālama, Honolulu, and Waikiki Ahupua'a, Honolulu (Kona) District, Island of O'ahu, TMK [1] 1-2, 1-5, 1-7, 2-1, 2-3 (Various Plats and Parcels): Volume I		
Island	O'ahu	District Honolulu
TMK		Ahupua'a Kahihi, Kapālama, Honolulu, Waikiki
TMK: [1] 1-2, 1-5, 1-7, 2-1, 2-3 (Various Plats and Parcels)		
Acreege Inventoried		Number of new sites inventoried
13.87		7

Submittal Plan/Report Fee & Type: (All reports or plans submitted to the SHPD for review shall be accompanied by the appropriate fee in accordance with HAR§13-275-4 and §284-4).

- Check if Report is a Re-Submittal (no fee charged)
- Final Report with CD
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- \$150 Preservation Plan
- \$25 Monitoring Plan
- \$150 Archaeological Data Recovery Plan
- \$250 Burial Treatment Plan
- \$100 Archaeological Monitoring Report, if resources reported
- \$0 Archaeological Monitoring Report, no finds
- \$450 Archaeological Data Recovery Report
- \$450 Ethnographic Documentation Report
- \$25 Burial Disinterment Report
- \$50 Osteological Analysis Report

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 HISTORIC PRES. DIV.
 DEPT. OF LAND &
 NATURAL RESOURCES
 2013 JUL 12 PM 01
 will be logged in
 July 15, 2013

Fee Total: 0 (make checks payable to "Hawaii Historic Preservation Special Fund")
For Office Use Only:

Date Received	Payment Method
	Cash \$
	Check Check No:
Log No.	Receipt Issued: